

LEISURE TRAIL MASTER PLAN UPDATE

New Albany, Ohio



JULY 2019



COMMUNITY PILLARS

New Albany's leisure trails are important to the community for multiple reasons, one of which is that it represents the four Community Pillars that embody the values that are important to the New Albany community. These pillars are:



LIFELONG LEARNING



ENVIRONMENTAL SUSTAINABILITY



HEALTHY LIVING



ARTS AND CULTURE

The integration of leisure trails into the fabric of the city and the community will create numerous benefits, including encouraging residents to be active and healthier, advancing sustainability, and connecting residents to community amenities and resources like schools, the Village Center, and parks.

ACKNOWLEDGEMENTS

STEERING COMMITTEE

Chip Fellows, *City Council*
Abbey Brooks, *Parks & Trails Advisory Board*
Bill Resch, *Parks & Trails Advisory Board*
George Stribick, *Parks & Trails Advisory Board*
Andrea Wiltrout, *Planning Commission*
Neil Kirby, *Planning Commission*
Kasey Kist, *City Council*
Hans Schell, *Planning Commission*
Dave Wallace, *Planning Commission*
Susie Banchefsky, *Resident*
Phil Heit, *Healthy New Albany*
Tom Rubey, *New Albany Company*

CITY OF NEW ALBANY STAFF

Joe Stefanov, *City Manager*
Stephen Mayer, *Development Services Manager*
Adrienne Joly, *Director of Administrative Services*
Jennifer Chrysler, *Director of Community Development*
Jackie Russell, *Development Services Coordinator*
Christopher Christian, *Planner*

PLANNING TEAM

Chris Hermann, *Principal, AICP*
Aron Fraizer, *Associate*
Chris Will, *Planner*
Juliana Silveira, *Planner*
Arin Blair, *Planner*

CONTENTS

SECTION 1:
INTRODUCTION 6

SECTION 2:
COMMUNITY GUIDANCE 12

SECTION 3:
EXISTING CONDITIONS 24

SECTION 4:
RECOMMENDATIONS 36

SECTION 5:
IMPLEMENTATION 64

SECTION 1: **INTRODUCTION**

PLAN OVERVIEW

Introduction

The New Albany leisure trail system is a beloved and valued asset of the community, consisting of paved trails throughout many parts of the city. In order to continue to improve the system, the city is embarking upon a Leisure Trail Master Plan Update. This plan process kicked-off in September 2018 and concluded in early Spring 2019.

A significant task of the planning effort was to examine existing conditions, assess needs, share best practices, identify critical linkages, and create a plan for the continued development and improvement of the City of New Albany leisure trail system. This plan builds upon the foundation established by the 2006 Leisure Trail Master Plan. This study provides a city-wide vision for future corridor trail improvements, greenway trails, and regional connections within the context of an overall low stress bicycle and pedestrian network. It also includes recommended strategies for implementing the plan.

GOAL STATEMENT

Update the 2006 Leisure Trail Master Plan to guide the provision of safe, inviting, and usable off-street mobility options that meet the needs of people living and working in New Albany.

PLAN OBJECTIVES

- 1 *Establish recommendations to guide the continued development and improvement of the leisure trail network through private development, public projects, and grant funding.*
- 2 *Review and develop standard leisure trail designs (typologies) for various typical corridor conditions.*
- 3 *Develop standards for leisure trail transitions between sidewalks, on-street bike lanes, other path systems, street crossings, and places with no paths.*
- 4 *Develop and prioritize a network of routes and loops throughout the city.*
- 5 *Create evaluation criteria to aid in the prioritization of leisure trail improvements.*
- 6 *Develop strategies for connecting and extending the New Albany leisure trail network to the greater Central Ohio and state-wide bike and trail network.*
- 7 *Consider potential wayfinding and branding, as well as systems for encouraging leisure trail use.*
- 8 *Establish if and when special conditions exist in which installation of a leisure trail on only one side of the street could be permitted..*
- 9 *Identify and prioritize gaps in the trail system, particularly in loops and routes.*
- 10 *Identify and prioritize trail amenities.*

PLAN PROCESS

Introduction

WHY AN UPDATE?

Since the 2006 *Leisure Trail Master Plan* was adopted, a lot has changed in New Albany, Northeastern Franklin County, and Western Licking County. The city population has increased over 60% from roughly 6,500 people in 2006 to over 10,700 today - and the city has expanded geographically. Since 2006, physical changes include new neighborhoods, new commercial development, new and improved parks, a growing school campus, and an expanded Village Center. Additionally, the city has actively worked to add leisure trail segments per the 2006 plan as part of capital improvement projects, through grant projects like Safe Routes to School, and as required parts of new development. With so much progress and change, it was time to revisit and update the Master Plan.

PLANNING PROCESS

Beginning in September 2018, the City undertook a six-month planning process guided by a selected Steering Committee of volunteers. The City was assisted by the private planning and design firm MKSK. The process included four Steering Committee meetings, online website engagement, two community walks, and a project team bike ride.

PLANNING AREA

The planning area for the update to the Leisure Trail Master Plan consists of the existing corporate boundary and the city's planned expansion area as defined in the *New Albany Strategic Plan*. The planning area is shown on the map on the following page.

PLANNING PROCESS

1. Project Launch

Establish Project Goals and Objectives;
Complete Existing Conditions Inventory

2. Community Input

Engage The Community; Community and Team Bike Rides And Walks

3. Needs Assessment

Determine Future Trail Connection Needs, Identify Key Loops and Routes, and Identify Key Connections to the Region

4. Strategic Recommendations

Develop Recommendations; Draft Plan; Adoption Process



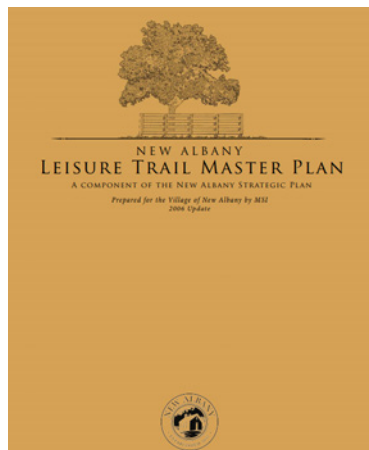
PLANNING AREA

PRIOR PLANNING

Related to Leisure Trails

PRIOR NEW ALBANY PLANS

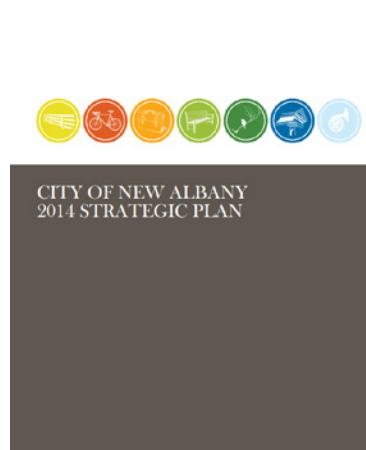
LEISURE TRAIL MASTER PLAN (2006)



Key Findings & Recommendations

1. Identified missing links in trail system.
2. Identified segments where development should construct missing links.
3. Recommended completing more leisure trail loops.
4. Recommended connecting to parks and planned parks.

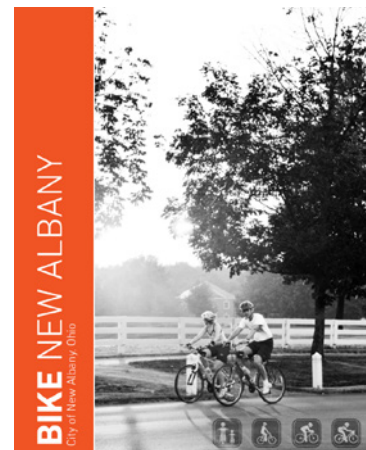
STRATEGIC PLAN (2014)



Key Findings & Recommendations

1. Create connections between parks, businesses, and neighborhoods.
2. Establish a system to monitor trail use.
3. Promote use and safety for walkers, runners and cyclists by building facilities, amenities and supporting educational opportunities.
4. Expand the leisure trail system to follow natural features, streams, and green corridors.

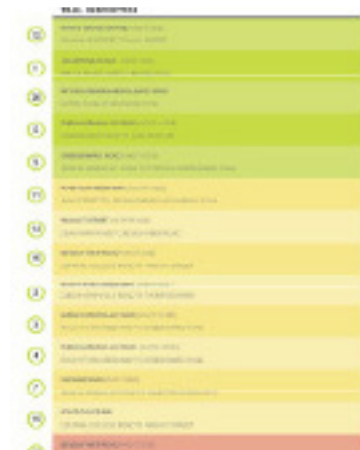
BIKE NEW ALBANY (2015)



Key Findings & Recommendations

1. Construct a 5-mile New Albany velo loop.
2. Develop cycling hubs & amenities.
3. Add signage & wayfinding.
4. Organize bicycle education & policy advocacy.
5. Make New Albany a premier cycling destination.
6. Address connectivity between north and south sides of SR-161.

LEISURE TRAILS PRIORITIZATION (2016)



Key Findings & Recommendations

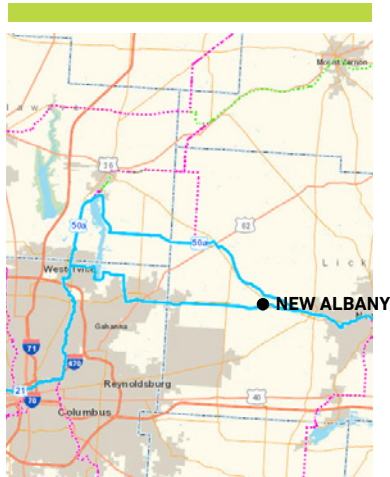
1. Prioritized 20 trail connections into tiers 1-3 (1 = highest priority) and Safe Routes to School.
2. Tier 1 Priorities: Ryan's Grove Ravine, Johnstown Rd., Reynoldsburg-New Albany Rd., Dublin-Granville Rd., and Greensward Rd.
3. Integrate Safe Routes to School and leisure trails.

PRIOR PLANNING

Related to Leisure Trails

PRIOR REGIONAL PLANS

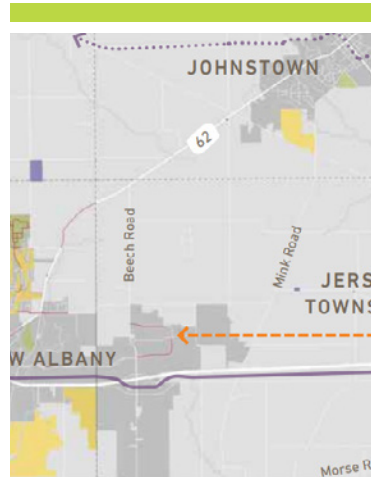
STATEWIDE BIKE PLAN



Key Findings & Recommendations

1. US Bike Route 50 corridor along SR-161 between Newark and Columbus is a major piece of an identified statewide and nationwide cycling network.

WESTERN LICKING COUNTY ACCORD (2017)



Key Findings & Recommendations

1. Connect to the TJ Evans Trail.
2. Proposed Beaver Trail from New Albany, parallel to SR-161.
3. Create and link trails to facilitate the US Bike Route 50/50A.

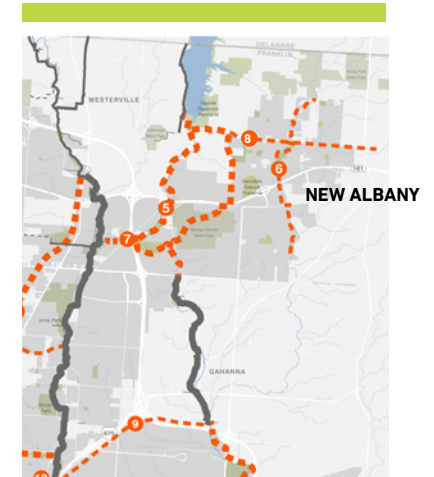
CENTRAL OHIO GREENWAYS TRAIL VISION (2018)



Key Findings & Recommendations

1. Connect New Albany and Central Ohio Greenways to TJ Evans Trail and Licking County's trail system via Blacklick Creek and Johnstown Road.
2. Continue Rocky Fork Greenway Trail.
3. Connect Johnstown Road to the Hoover Reservoir.

COLUMBUS GREENWAYS IMPROVEMENTS (2018-2022)



Key Findings & Recommendations

1. Rocky Fork Trail to Rocky Fork Metro Park.
2. New Albany/Alum Creek Connector via Hamilton Road and Blendon Woods Metro Park.
3. New Albany/Hoover Reservoir Connector via Central College Road.

SECTION 2: **COMMUNITY GUIDANCE**

COMMUNITY GUIDANCE

Community Engagement

Community guidance was integrated throughout the planning process. The project was guided by a Steering Committee of volunteer community members selected and appointed by the City of New Albany. Four Steering Committee meetings were held during the planning process. The consulting team also provided regular updates and received direction from a Working Group comprised of City staff members.

To engage the general public and additional community members, a community walk was held where participants filled out surveys and pointed out challenges and opportunities along the walk. The same survey was also advertised and promoted to the community to complete online for people who could not attend the community walk. More than 260 surveys were submitted. In addition, the planning team went on a 3.5-mile bike ride around the city that was open to the community at the start of the project to experience firsthand cycling through the city on the leisure trails. The city also provided access to further community survey data from the League of American Bicyclists, which is complementary to the survey conducted as part of this planning process.

Team Ride: Rocky Fork Metro Park

Community Walk 1: Johnstown Rd.

Community Walk 2: Swickard Woods

COMMUNITY ENGAGEMENT

2018

ONLINE ENGAGEMENT

A community survey hosted on SurveyMonkey was made available to residents and employees in the city from September to early November. The survey asked 13 questions about who uses the trail, how they use it, and which type of improvements they would like to be made.

STEERING COMMITTEE #1

STEERING COMMITTEE #2

STEERING COMMITTEE #3

STEERING COMMITTEE REVIEW

PARKS & TRAILS BOARD REVIEW

2019

FINAL PLAN + CITY COUNCIL ADOPTION

PLANNING TEAM RIDE

Community Guidance

The planning team ride was hosted on September 29th and began and finished at the Rocky Fork Metro Park. City staff, MKSK consultants, a Franklin County Metro Park ranger, and a City of New Albany police officer all took part in the ride. The route took the team 3.5 miles and provided varying experiences highlighting the different types of trail and transition environments.

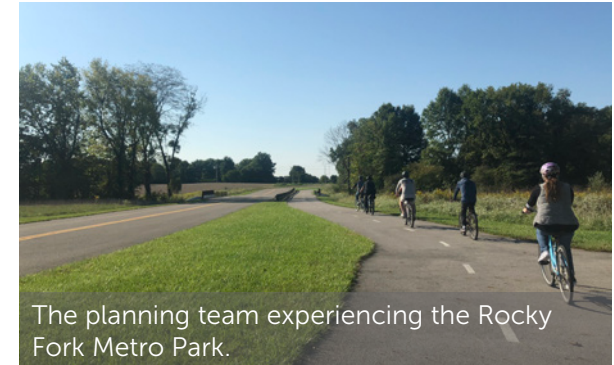
Objectives:

- + Experience and discuss the leisure trail network, including different types of trails (off-road, on-road, trails through parks, etc.) and the connectivity on streets and sidewalks through neighborhoods as well as to regional amenities like the Metro Park.
- + Envision the future of the leisure trail network and how to improve it.

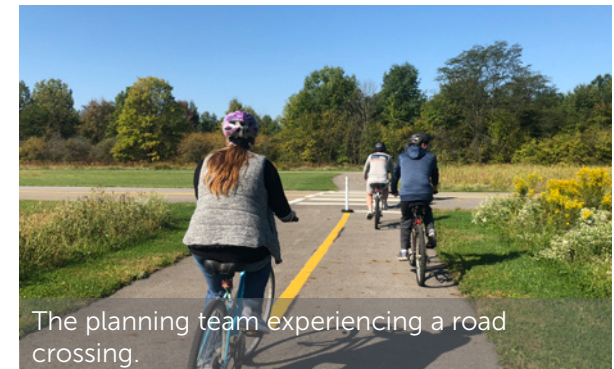
PLANNING TEAM RIDE



TEAM RIDE PHOTOS



The planning team experiencing the Rocky Fork Metro Park.



The planning team experiencing a road crossing.



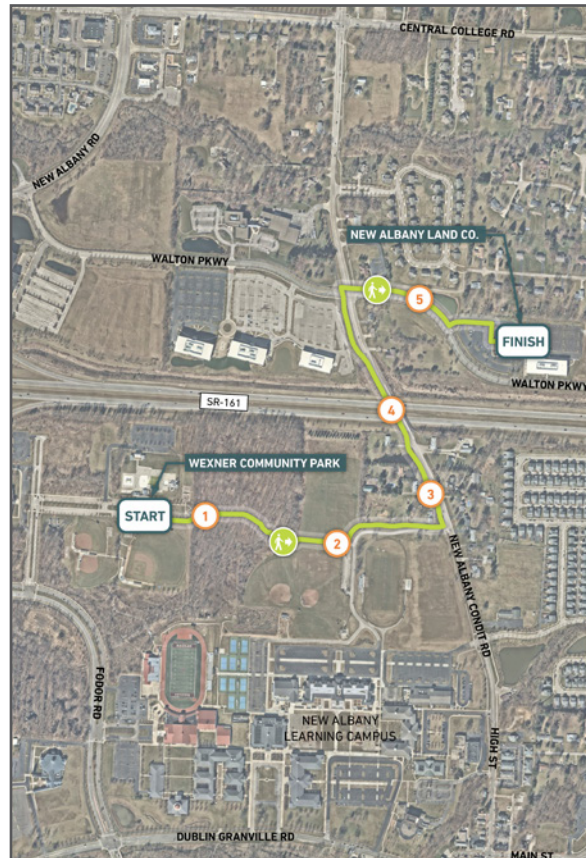
The planning team experiencing riding on a neighborhood street.

COMMUNITY WALKS

Community Guidance

Two community walks were organized for October. A small group of community members participated in the walk held on October 24th. The community walk began at the Wexner Community Park, traveled through Swickard Woods, over SR-161 via the protected trail on New Albany-Condit Road, and finished at the New Albany Land Company's office on Walton Parkway. This route was specifically selected so that walkers could experience numerous trail conditions including trails through park and natural areas, trails along roadways, and trails over SR-161. At the end of the community walk, participants were asked to fill out a survey based on their experience. These surveys were recorded and helped to advise the plan recommendations. Another community walk planned for October 23rd along US 62 to Morse Road was canceled for lack of attendance.

COMMUNITY WALK ROUTE



COMMUNITY WALK PHOTOS



Walkers experiencing one of the city's many leisure trails.



Walkers experiencing existing natural trails in the city (Swickard Woods).



Walkers filling out project survey.

ONLINE ENGAGEMENT SURVEY

Community Guidance

SURVEY OVERVIEW

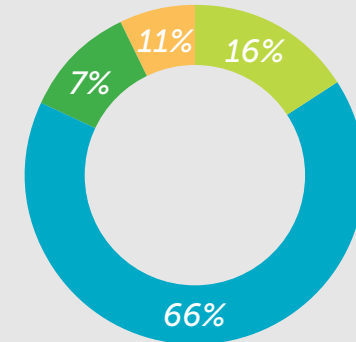
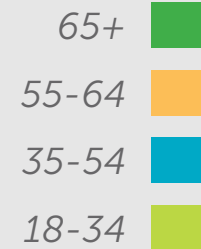
The community survey, hosted on SurveyMonkey, was launched in September and closed in early November. The survey asked questions about demographics to better understand the composition of participants, participants' experience on trails and how they use the trails, and ideas to improve and expand the trails. The survey was posted on the City web site, and advertised in a City E-blast and promoted in a Business Campus newsletter. Over 260 surveys were completed. It should be noted that although the level of participation was substantial, the survey is not free from bias as participants were not randomly selected to represent the composition of the community population.

PARTICIPANT DEMOGRAPHICS

Women aged 35-54 represent the most significant user group who completed the survey. The survey asked four questions to understand how the public is using the leisure trail system. Of those who completed these questions, 1 in 4 answered that they use the trail every day, while 68% answered that they use the trails at least once a week. Fifteen percent said they rarely or never use the trails. The leisure trails are primarily used for exercise (walking, biking, and jogging) at over 65% and pleasure at over 50%.

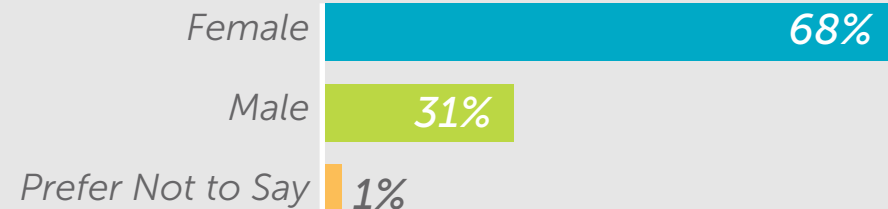
WHO IS USING THE TRAILS

WHICH AGE GROUP ARE YOU?

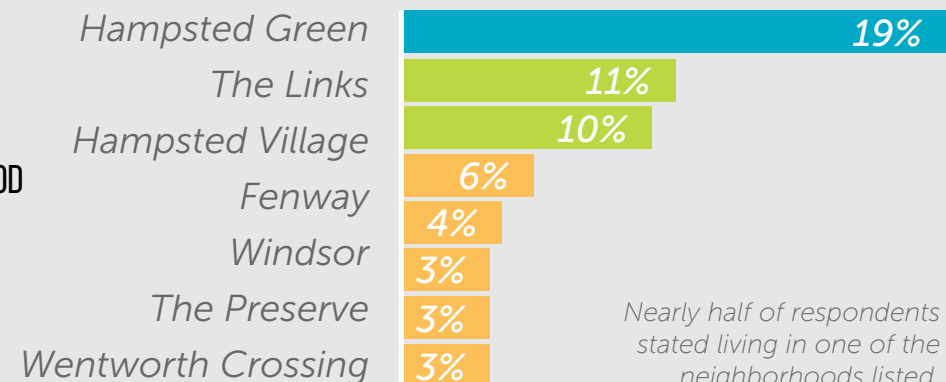


Though no respondents answered that they were younger than 18, it is assumed that children are using trails with adult family members.

WHAT BEST DESCRIBES YOUR GENDER?



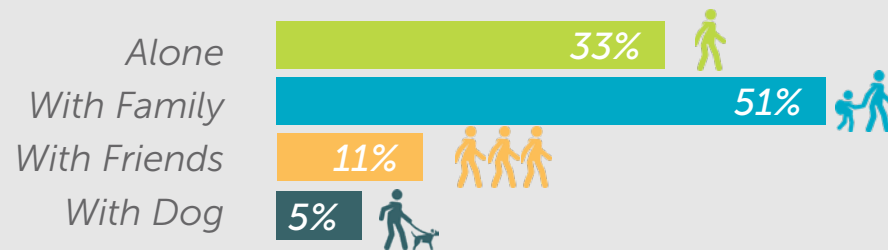
WHAT NEIGHBORHOOD DO YOU LIVE IN?



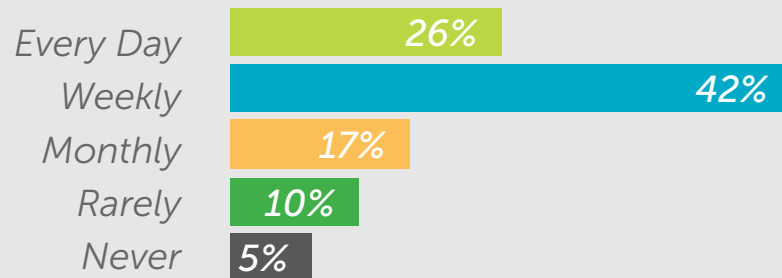
Nearly half of respondents stated living in one of the neighborhoods listed.

HOW THE TRAILS AREA USED

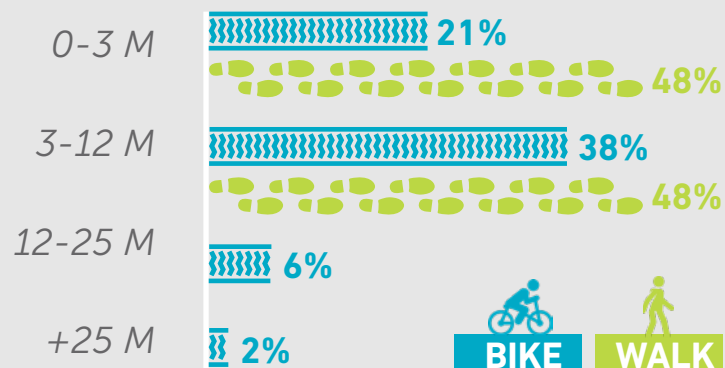
WHO ARE YOU WITH WHEN USING THE TRAILS?



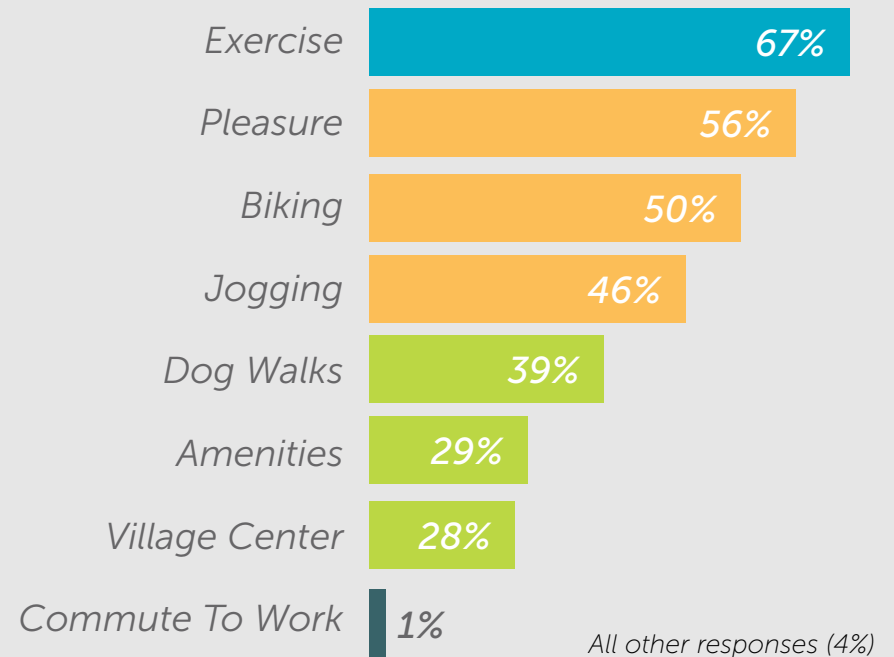
HOW OFTEN DO YOU USE THE TRAILS?



HOW FAR DO YOU USUALLY TRAVEL?



WHAT ACTIVITIES DO YOU TYPICALLY USE THE TRAILS FOR?



Figures may not equal 100% as some participants responded that they do not use the leisure trails.

ONLINE ENGAGEMENT SURVEY

Community Guidance

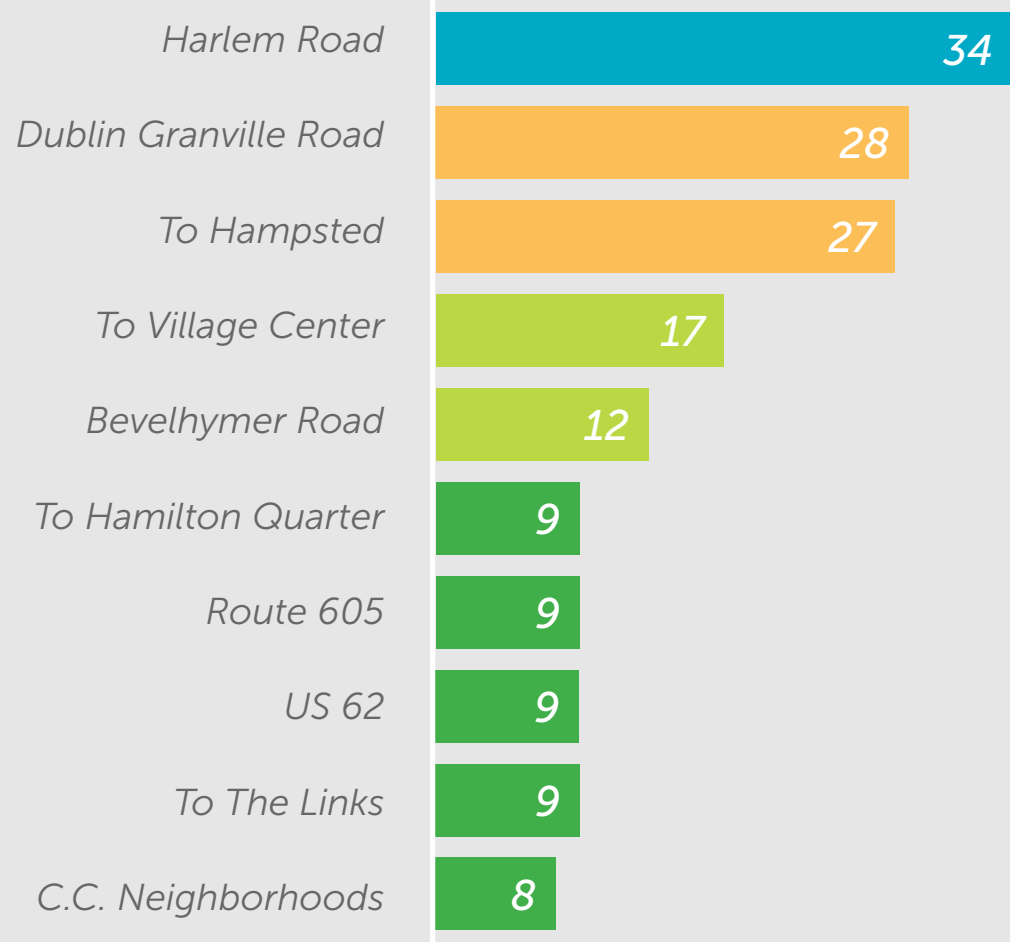
RECOMMENDATIONS FOR NEW TRAILS

The survey asked participants to recommend places where trails should be constructed in the city. Many recommendations from the community focused on improvements in the area of Dublin-Granville Road and Harlem Road around Hampsted Village. Roughly half of the participants agree the city should focus on investing in improvements to connect the Village Center with the rest of the city as well as connecting neighborhoods to one another.

PRIORITIZATION

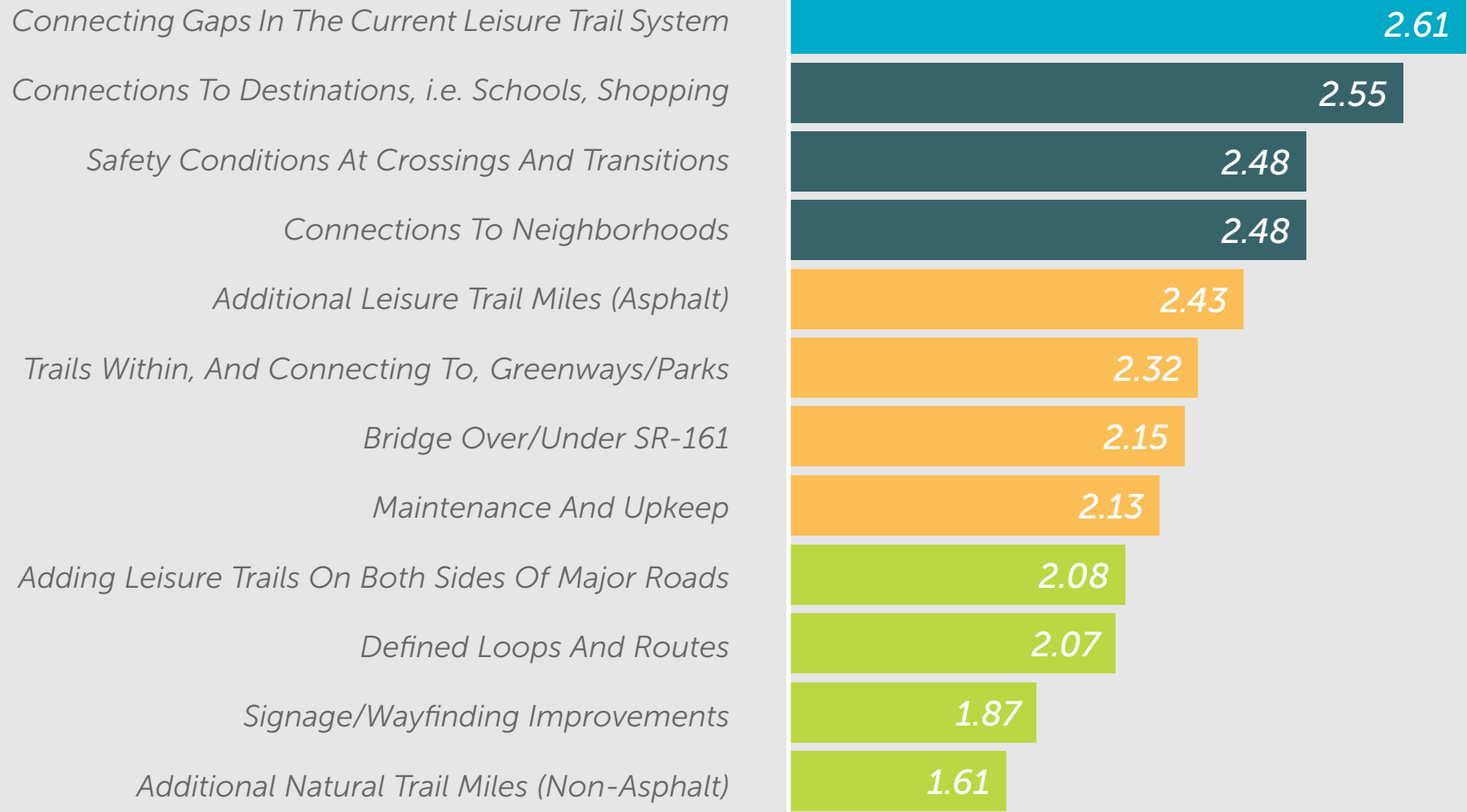
The survey asked participants to prioritize improvements as low, medium, or high. The graph opposite this page created an aggregate score of these priorities to rank improvements from higher priority to lower priority. It should be noted that three (3) is the highest possible aggregate "score" while one (1) is the lowest. Results show that connecting gaps in the current leisure trail system ranked as the highest priority. Connecting neighborhoods and destinations as well as addressing safety concerns were other highly ranked improvements.

WHERE WOULD YOU LIKE TO SEE NEW TRAILS?



Note: Only recommendations with 8 or more votes are listed.

WHICH IMPROVEMENTS SHOULD WE PRIORITIZE?



STEERING COMMITTEE

Community Guidance

The process was guided by a Steering Committee representing residents and board and commission members of the city.

During the second Steering Committee meeting, the committee participated in a prioritization exercise. The prioritization questions were as follows:

- + Which strategic improvements should be prioritized?
- + Which trail corridors should be prioritized?
- + Which regional connections should be prioritized?

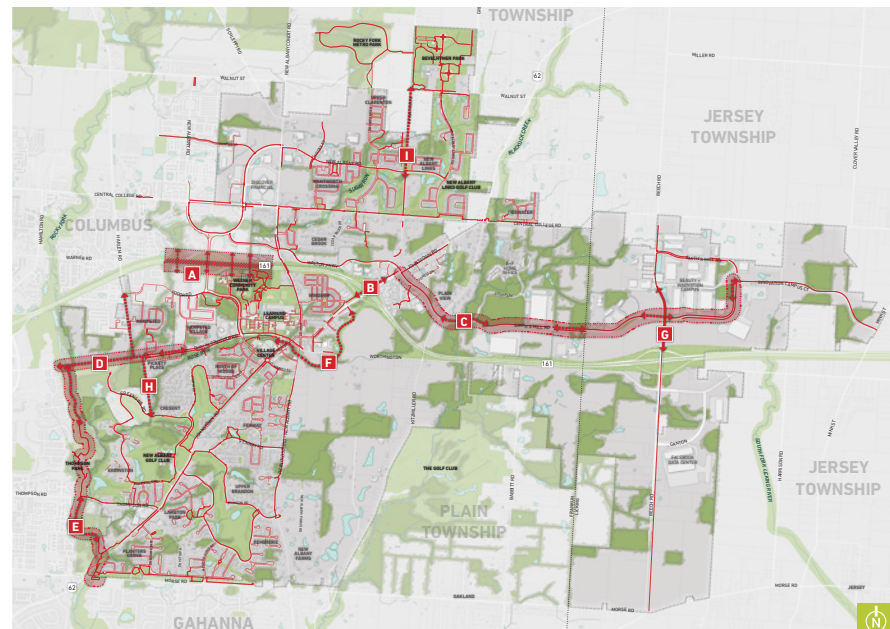
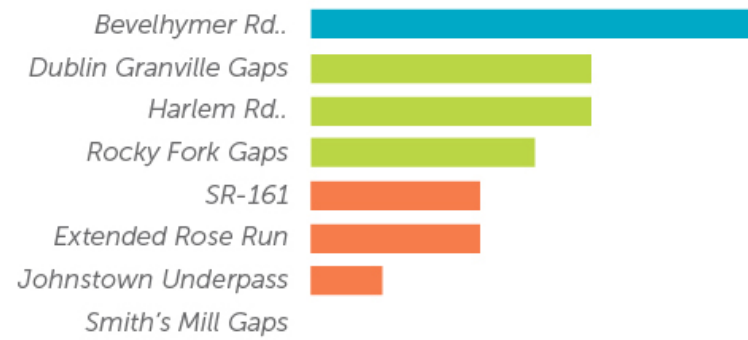
While not an official prioritization for the City, this provides an additional level of input for City leaders and decision makers.



Steering Committee prioritization exercise.

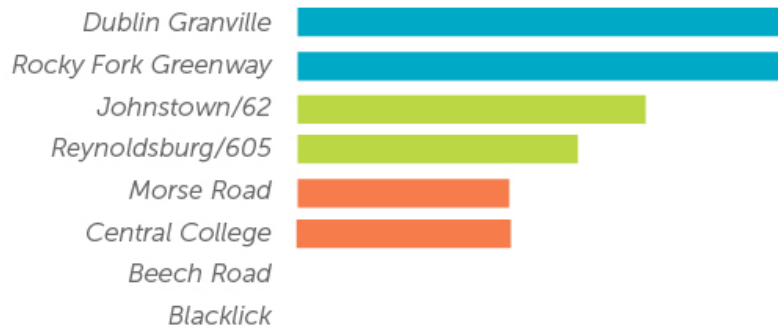
WHICH STRATEGIC IMPROVEMENTS SHOULD BE PRIORITIZED?

8 of 10 members think the city should prioritize improvements to Bevelhymer Road.



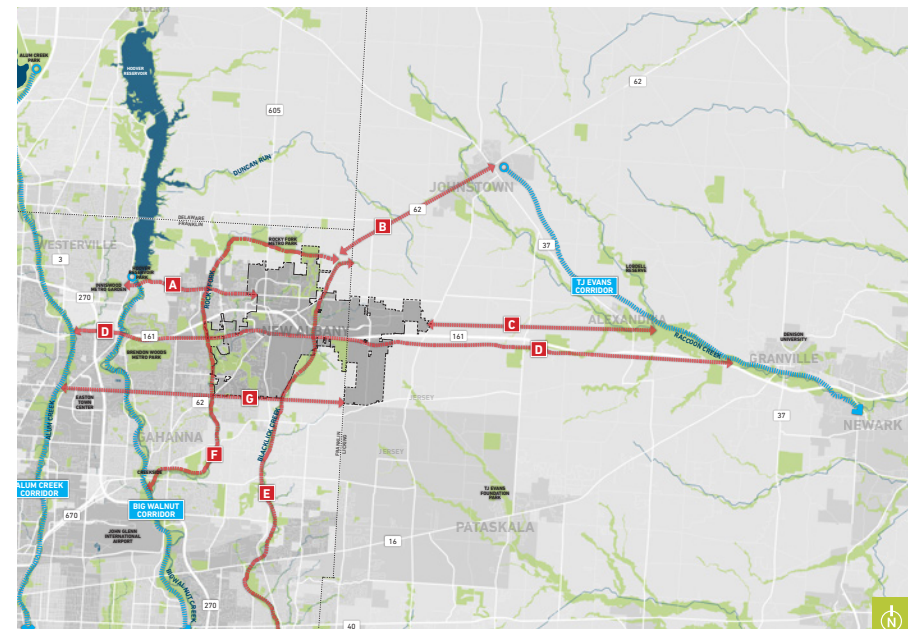
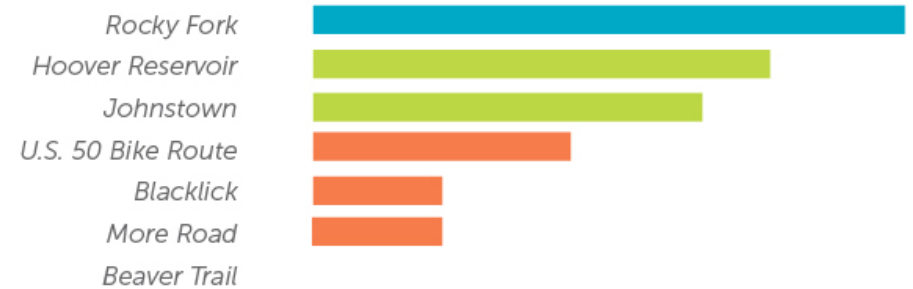
WHICH TRAIL CORRIDORS SHOULD BE PRIORITIZED?

Dublin-Granville Road and the Rocky Fork Greenway are the committee's highest prioritized corridors



WHICH REGIONAL CONNECTIONS SHOULD BE PRIORITIZED?

9 of 10 members think we should prioritize the Rocky Fork Greenway for regional connections.



BICYCLE FRIENDLY COMMUNITY SURVEY

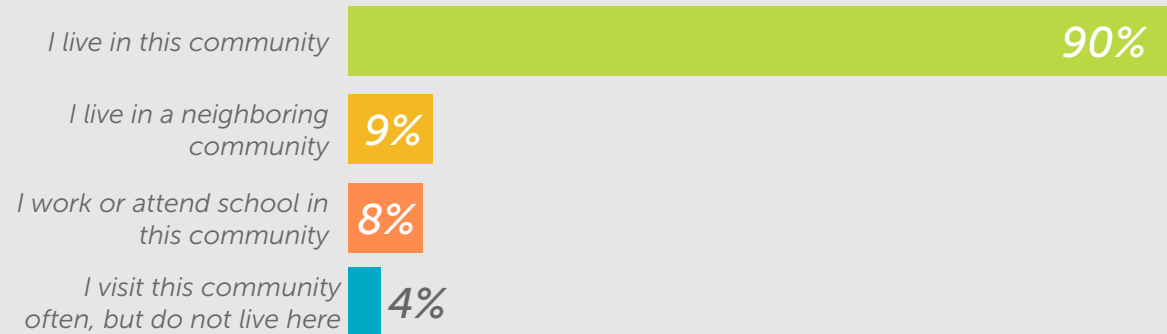
Community Guidance

In the fall of 2018, the City applied to be a Certified Bike Friendly Community (BFC). The City did not qualify but did receive an honorable mention. The application response from The League of American Bicyclists included a BFC Report Card and steps needed to become a full-fledged bike friendly community. As part of the BFC review process, The League gathered input from residents, local bicyclists, and bike advocates to better understand the bicycling experience in New Albany. Those results are summarized to the right.

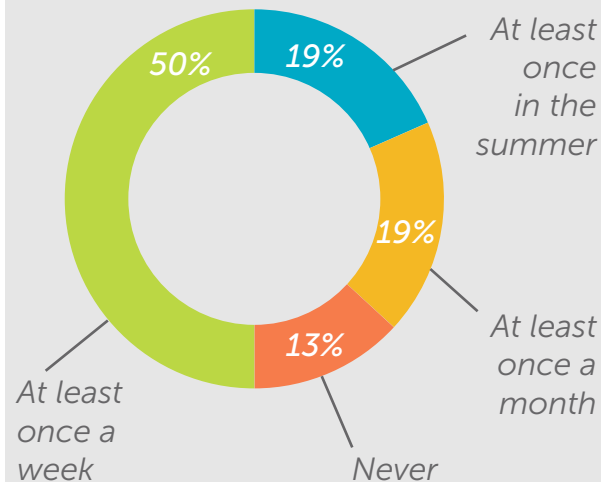
ABOUT THE ORGANIZATION: The League of American Bicyclists is a non-profit organization that “represents bicyclists in the movement to create safer roads, stronger communities, and a bicycle-friendly America”. Through information, advocacy, and promotion, they work to “celebrate and preserve the freedom cycling brings to their members everywhere.”



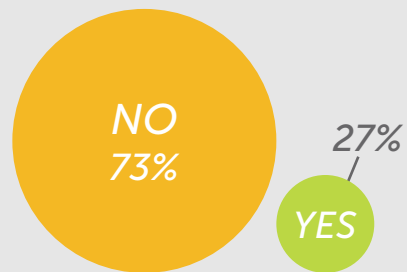
WHICH OF THE FOLLOWING OPTIONS BEST DESCRIBE YOUR CONNECTION TO THIS COMMUNITY?



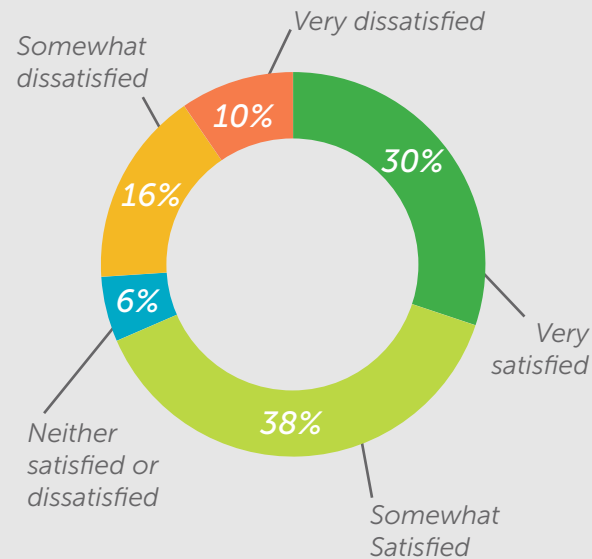
ON AVERAGE DURING THE SUMMER MONTHS, HOW OFTEN DO YOU USE A BICYCLE?



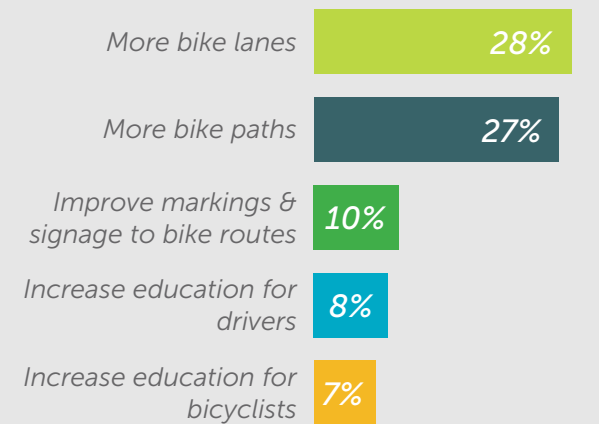
IN THE PAST FIVE YEARS, HAVE YOU RECEIVED ANY TRAINING IN BICYCLING SAFETY IN THIS COMMUNITY?



HOW SATISFIED ARE YOU WITH HOW THIS COMMUNITY IS DESIGNED FOR MAKING BIKE RIDING SAFE?



WHAT IS THE NUMBER ONE CHANGE YOU WOULD MOST LIKE TO SEE THE LOCAL GOVERNMENT MAKE IN THIS COMMUNITY FOR BICYCLISTS?



Only top 5 answers shown above.

SECTION 3: **EXISTING CONDITIONS**

USER PROFILES

Existing Conditions

To successfully plan for the needs of the leisure trail system, it is imperative to understand that there is no single type of trail user. Incorporating user profiles identified by the 2015 Bike New Albany Plan, the leisure trail planning process also identified non-cyclist users and developed six categories of pedestrians and cyclists to represent the spectrum of trail users in the city. These represent the different comfort levels of users in New Albany, as well as the different reasons people use the leisure trails in the city. These user categories help to identify needs and recommendations of this plan.

In terms of cyclists, different users prefer or are comfortable using different types of bicycle facilities. Some prefer to ride on the road, others prefer to ride or walk on trails, some will use both, and some may wish to ride on the road but may be intimidated. In terms of joggers, walkers, and strollers, providing enjoyable circuits (loops) and routes in an attractive environment with connectivity to their neighborhoods and equipped with desired amenities help to attract users. By utilizing both on-street and off-street infrastructure, and supporting this system with trail amenities, signage and wayfinding, and policies to increase awareness of the trails, the City can create a more cycling- and pedestrian-friendly community.

USER PROFILES



Advanced Cyclist

Experienced riders that primarily ride on the city's roads, covering many miles in one ride.



Enthusiasts/Leisure Riders

Cyclists that like to take longer, faster rides; however may not be as comfortable riding on the streets.



Commuters

Users who wish to bike or walk to work or for errands, and will use either trails or roads to reach their destination.



Kids, Friends & Families

Users who wish to bike or walk on the trails for leisurely enjoyment and shared experience. Cyclists in this category are not interested in riding on the roads and prefer the safety of the trails.



Daily Exercisers

Residents and workers who routinely use the trail for walking, jogging, or other non-bike exercise.



Dog Walking

Residents who walk their dog(s) within the neighborhood.

EXISTING LEISURE TRAILS

Existing Conditions

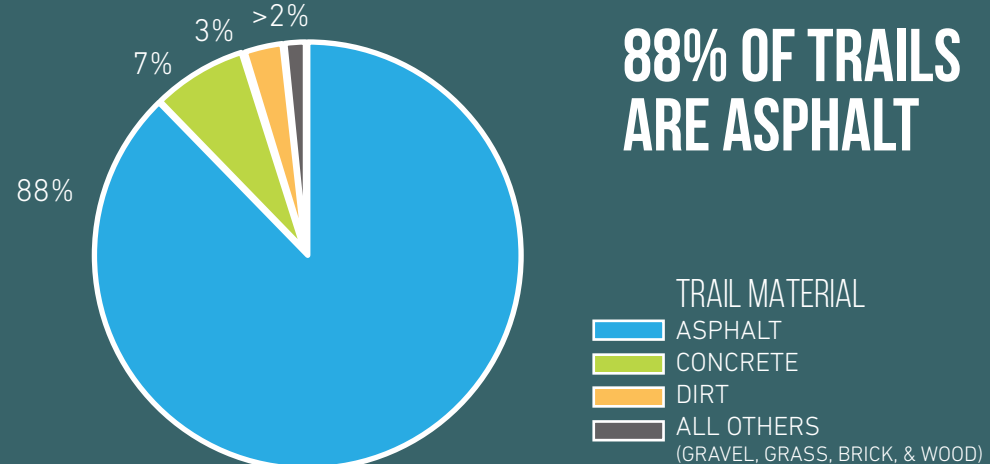
The existing leisure trail system reflects the emphasis on livability in New Albany. Many miles of leisure trails have been built and an impressive foundation laid for a cohesive trail system that connects the population within New Albany and the greater regional network.

The existing trail system includes 54 miles of trails connecting users to the Village Center, the Learning Campus, the business park, parks, and neighborhoods. The strength of this system is the important outdoor leisure and recreational opportunity it provides for residents and employees. Of the 54 miles of trails, 30% of the trails have been constructed since 2010 and close to half the trails have been constructed since the 2006 Leisure Trail Master Plan when there were 24 miles system wide. These figures include trails within the city as well as trails in both the Rocky Fork Metro Park and the New Albany-Plain Local School District.

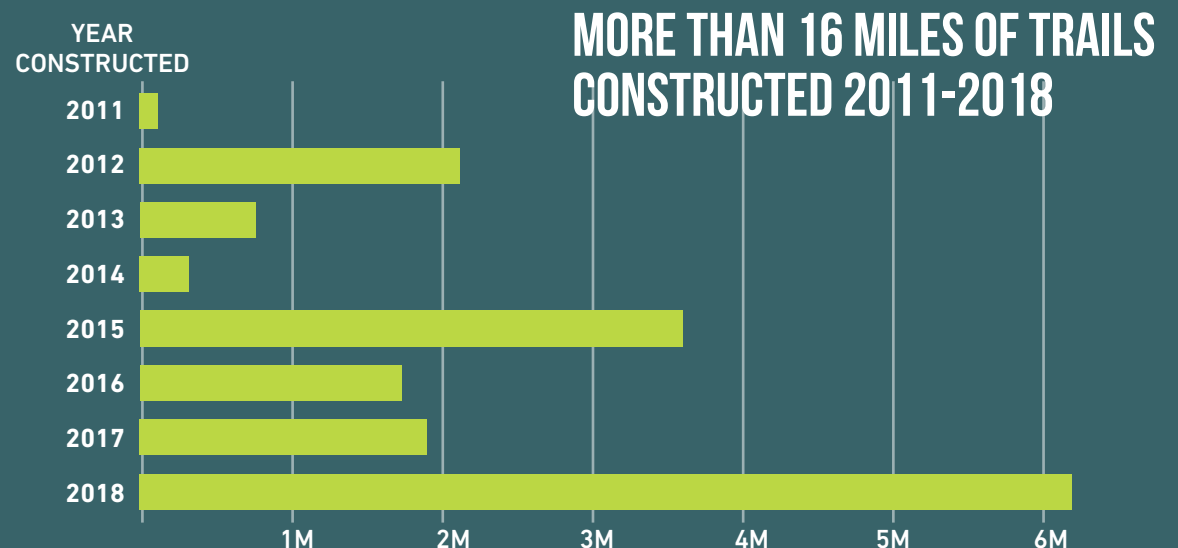
An existing inventory of trails was updated focusing primarily on the asphalt paths in New Albany that make up the leisure trail system. This inventory did not include all of the sidewalks in the city. Although sidewalks provide necessary connections to the leisure trail system, the focus of this plan is primarily on the asphalt leisure trails. In addition to leisure trails and city sidewalks, recently constructed bike lanes on New Albany-Conduit Road add to the inventory of low stress bike and pedestrian facilities that help to make New Albany a walkable community of choice.

54 MILES OF LEISURE TRAILS IN NEW ALBANY

TRAIL NETWORK MATERIALS



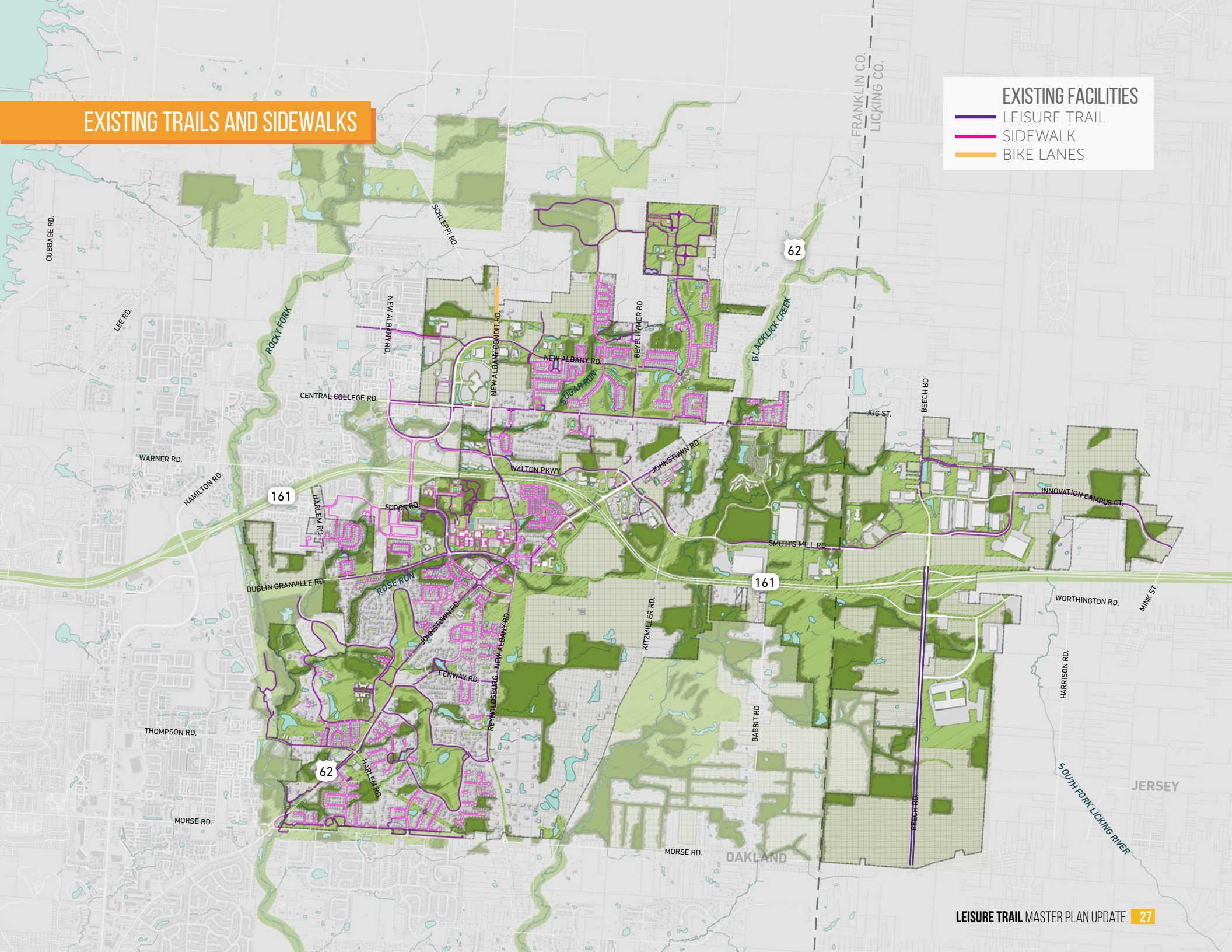
RECENT TRAIL CONSTRUCTION



Note: Figures reflect available data.

EXISTING TRAILS AND SIDEWALKS

- EXISTING FACILITIES**
- LEISURE TRAIL
 - SIDEWALK
 - BIKE LANES



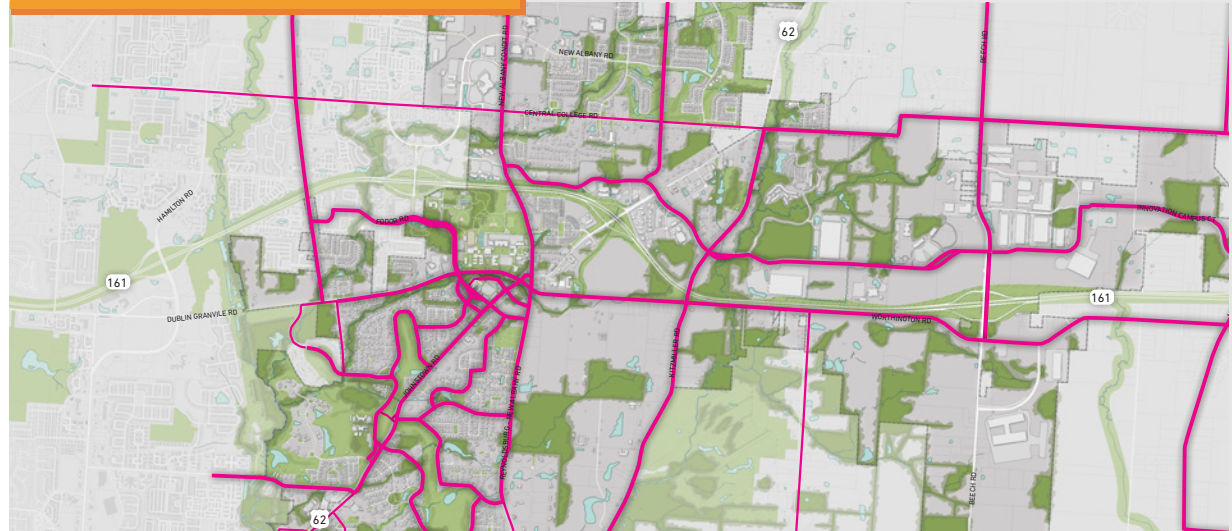
LEISURE TRAIL USAGE

Existing Conditions

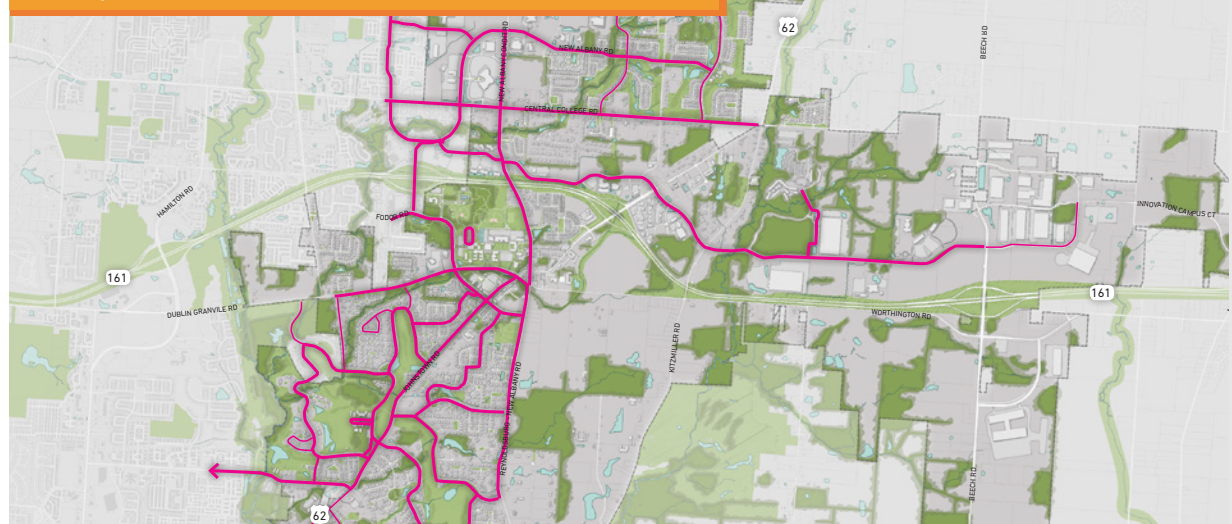
As more people use applications on their phone in everyday life, data is being aggregated and mapped to help planners understand how people interact with their environment. One such example is the fitness/exercise application Strava. Millions of people upload their ride, run, and walk routes to Strava every week via their smartphones or GPS devices. Strava collects both the cycling and running/walking data which allows the analysis of trail and street use by mode. While this does not cover every user of these facilities, the data provides a snapshot of where and at what intensity residents and visitors of the city are using the leisure trail system. The adjacent maps highlight three data sets: one is the running/walking heat map, the second is the biking heat map, and the third is a composite of both.

A quick analysis of the heat maps provides some interesting observations. The running/walking map indicates that pedestrians mostly use the leisure trails along main roads in and around residential neighborhoods, along the main roads through the business campus, the school track, and the trails at Rocky Fork Metro Park. Similar trail and road routes are highlighted for cyclists. One of the primary observations is the heavy use of the rural roads surrounding the city and the divides created by state routes such as SR 161, SR 62, and SR 605 absent leisure trails.

FREQUENTLY USED BIKE ROUTES



FREQUENTLY USED RUNNING/WALKING ROUTES



Source: Strava.

ROUTES

Existing Conditions

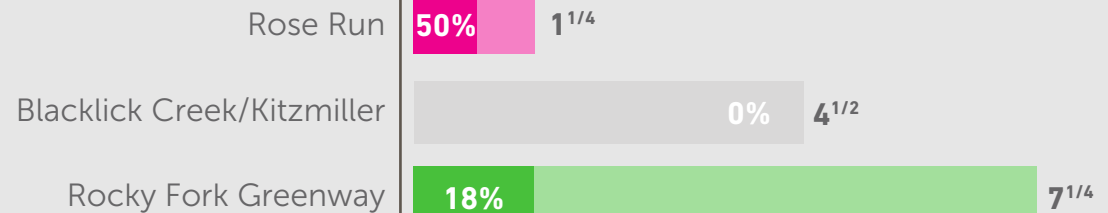
Although leisure trails are envisioned throughout the entirety of the city, there are several significant trail routes that are part of the larger city vision to serve as the spines for the low stress pedestrian and bicycle network, and to connect to the larger planned Central Ohio regional system. These routes were originally identified in the 2006 Leisure Trail Master Plan and divided into two major categories: road corridor trails paralleling roads, and greenway trails paralleling waterways and/or going through parks or other natural areas.

The majority of the existing leisure trail routes, over 70%, are roadway trails. These have been developed primarily by private developers and as part of city street improvement projects over the last 20 years. As a result, corridors adjacent to neighborhood subdivisions and business campus development are more significantly completed - like US 62/Johnstown Road through the Country Club and Walton Parkway/Smith's Mill Road through the Business Campus. Less recently developed sections of the city are often the location of leisure trail gaps.

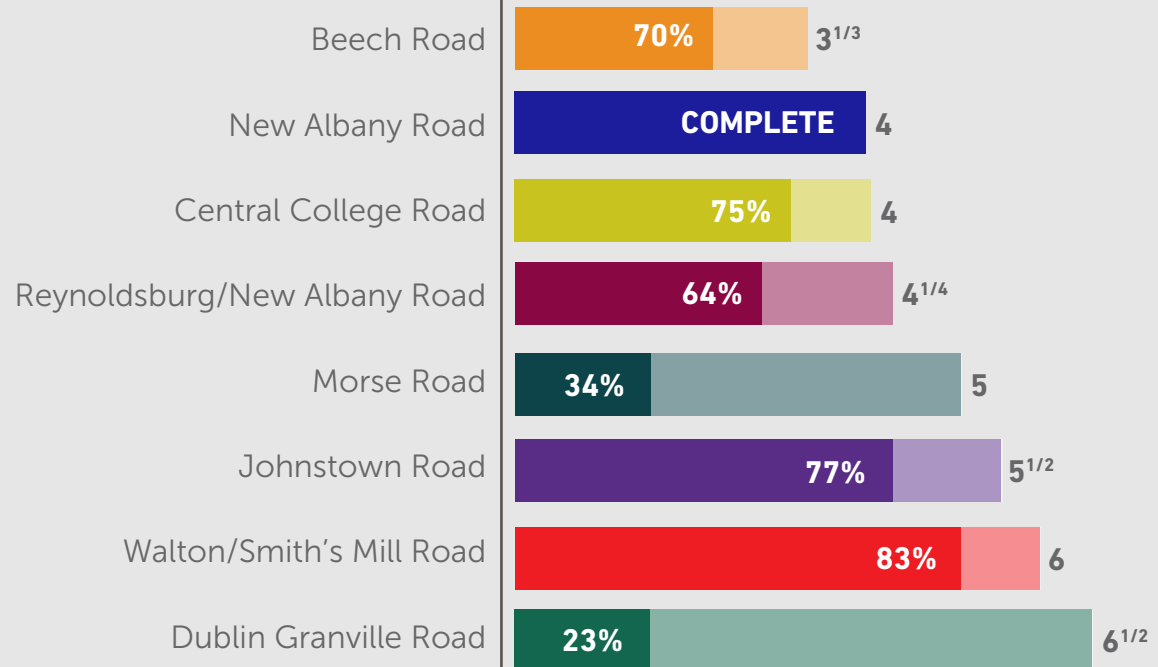
The remaining routes are greenway trails that make up less than 20% of the identified routes. These largely need to be dedicated in segments as part of new development or public park or trail projects.

STATUS OF MAJOR TRAIL ROUTES

Greenway Corridors



Roadway Corridors



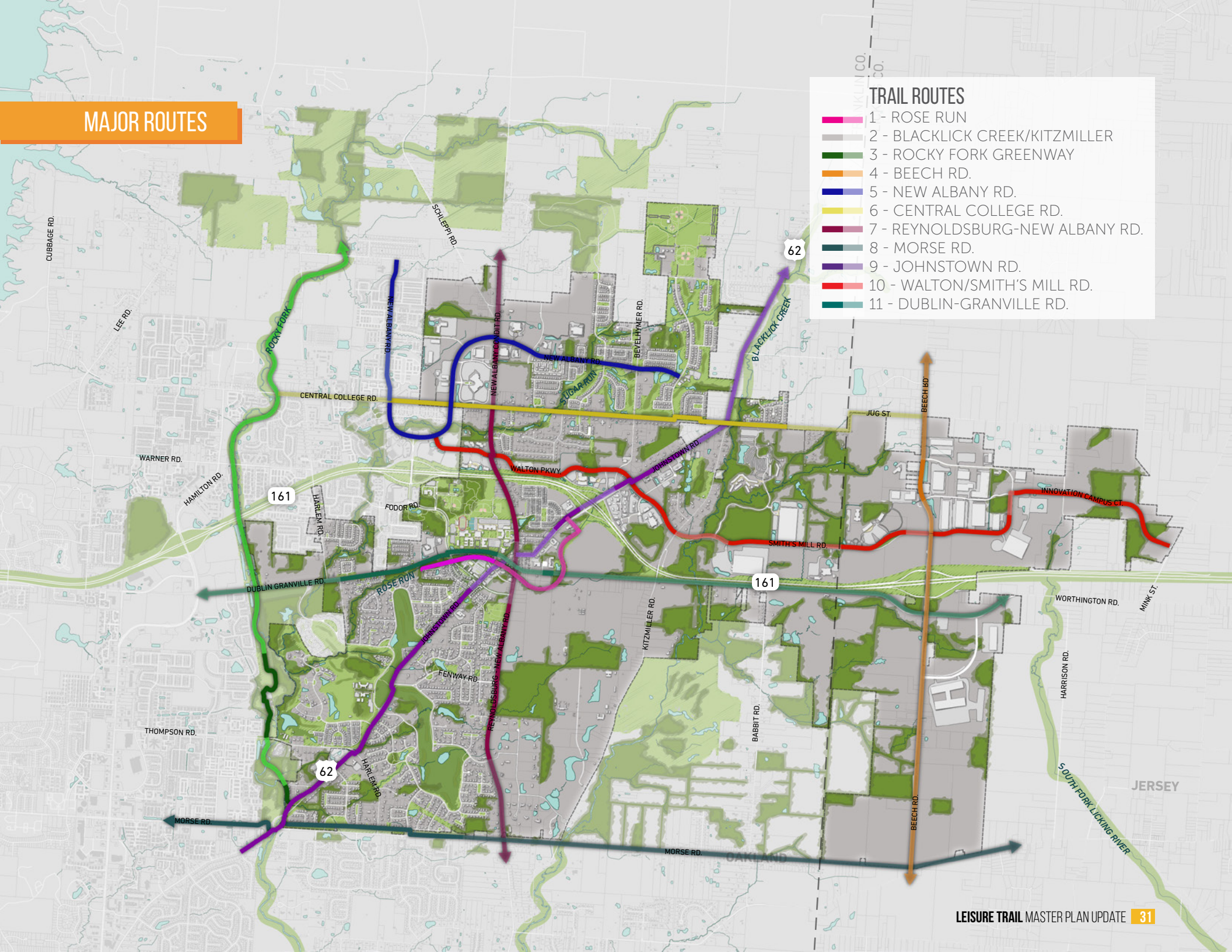
M Total Built/Unbuilt Route Miles
 Portion of Unbuilt
% Percent of Route Complete

Note: Figures reflect available data.

MAJOR ROUTES

TRAIL ROUTES

- 1 - ROSE RUN
- 2 - BLACKLICK CREEK/KITZMILLER
- 3 - ROCKY FORK GREENWAY
- 4 - BEECH RD.
- 5 - NEW ALBANY RD.
- 6 - CENTRAL COLLEGE RD.
- 7 - REYNOLDSBURG-NEW ALBANY RD.
- 8 - MORSE RD.
- 9 - JOHNSTOWN RD.
- 10 - WALTON/SMITH'S MILL RD.
- 11 - DUBLIN-GRANVILLE RD.



LOOPS

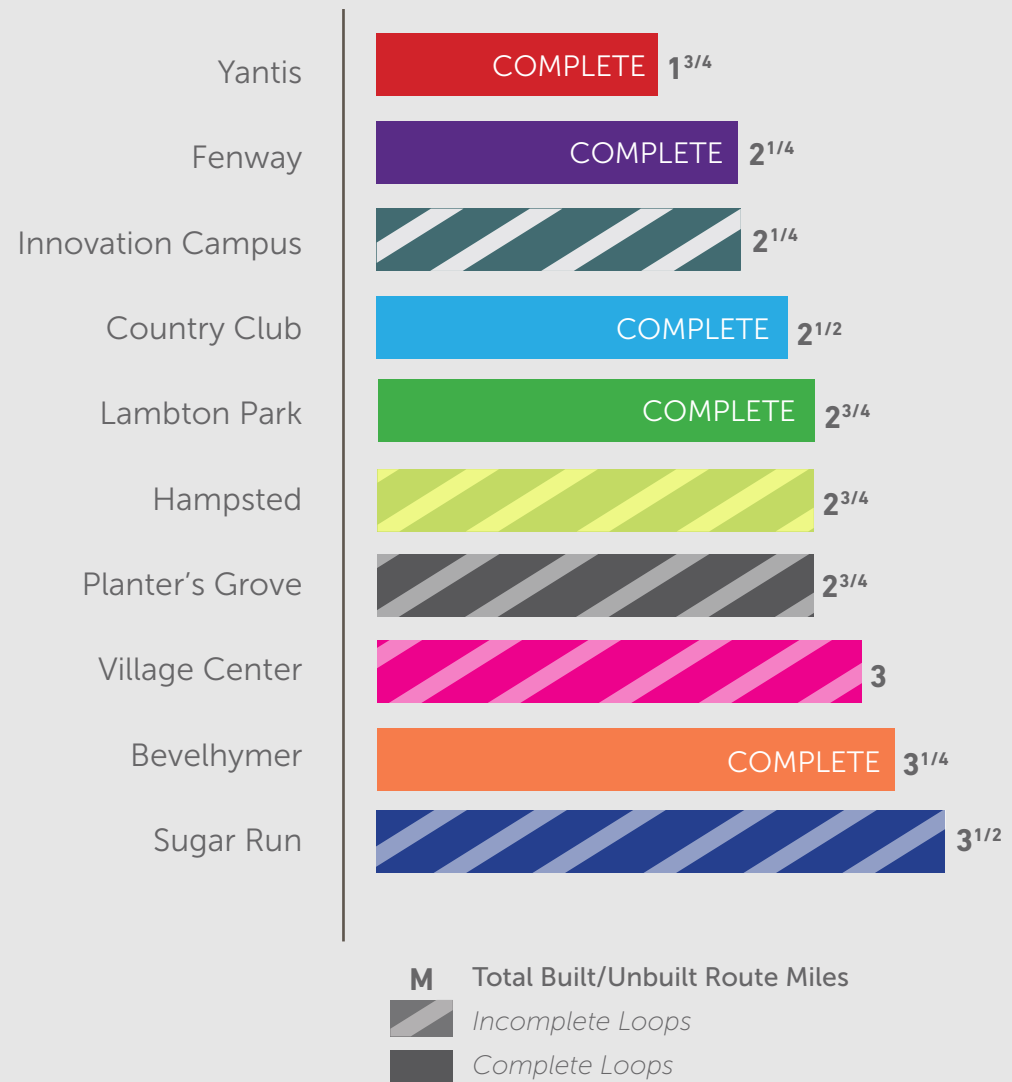
Existing Conditions

To further connect the existing and planned routes and provide more inviting and defined walking circuits, the 2006 Leisure Trail Master Plan also identified six loops. These six loops have been expanded and shaped to include a total of 10 loops. These loops were envisioned to create manageable walking and exercise circuits. They are generally between 2-3 miles in length, which may be walked in 40 minutes to an hour. The loops help connect between routes, better connect neighbors, and connect the business park to the Village Center. Portions of these loops overlap the identified routes, but are primarily along residential neighborhood streets. The loops were defined not only by their lengths but also to minimize the need to cross major arterial roadways — or where they do cross arterials, to do so in a grade-separated or more visible and safe manner.

Currently five of the ten loops are complete. The five additional loops are in various stages of completion.

To this point, little has been done to brand or promote these loop trails. There appears to be marketing and signing opportunities related to these loops and the routes.

STATUS OF MAJOR TRAIL LOOPS

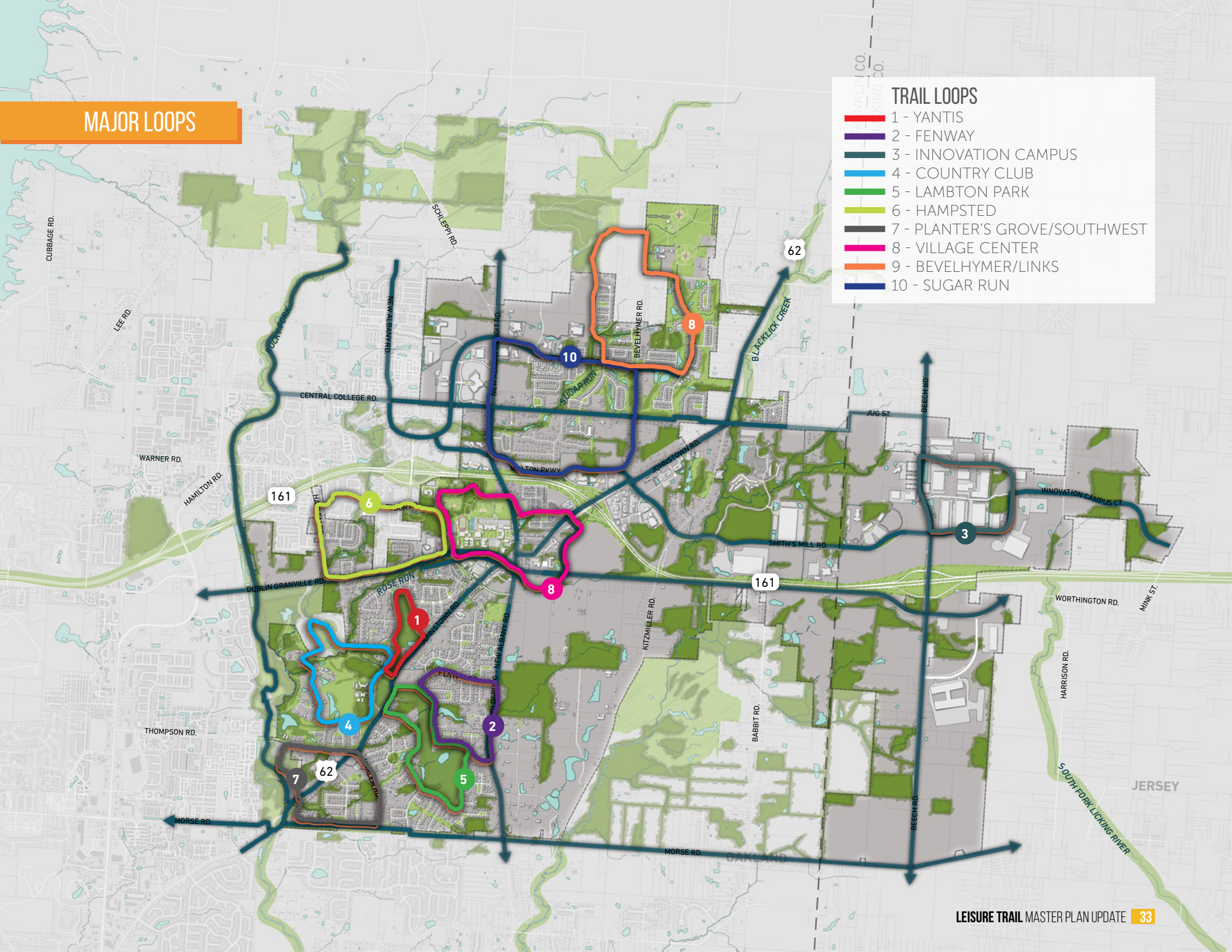


Note: Figures reflect available data.

MAJOR LOOPS

TRAIL LOOPS

- 1 - YANTIS
- 2 - FENWAY
- 3 - INNOVATION CAMPUS
- 4 - COUNTRY CLUB
- 5 - LAMBTON PARK
- 6 - HAMPSTED
- 7 - PLANTER'S GROVE/SOUTHWEST
- 8 - VILLAGE CENTER
- 9 - BEVELHYMER/LINKS
- 10 - SUGAR RUN



REGIONAL SYSTEM

Existing Conditions

Central Ohio Greenways

Over the past 30 years, Central Ohio's municipal governments have been developing the regional trails and greenway network and have made great strides. From only one corridor, Central Ohio now has over 180 miles of dedicated greenway trails that are traveled by residents and visitors whose total annual distance traveled totals 12 million miles.

T.J. Evans Trail

T.J. Evans Trail is a more than 27-mile rail-to-trail in Licking County that runs from Johnstown to Granville to Newark and then east to Hanover ("Panhandle Trail") and the 4-mile Blackhand Gorge Trail and is one of Ohio's signature trails.

Ohio-to-Erie Trail

The Ohio-to-Erie Trail is a 326-mile recreational trail that runs from Lake Erie to the Ohio River across the state of Ohio (connecting Cleveland, Akron, Columbus, & Cincinnati). The paved trail route mostly follows lands formerly occupied by railroads and canals and is currently 83% off roadway.

U.S. Bicycle Route 50

U.S. Bicycle Route 50 is an east-west bike route that runs coast to coast across the US. This route currently is on-road through New Albany connecting between the Alum Creek and T.J. Evans Trails. It is signed.



Part of Designated SR-50 Bike Route



Existing Alum Creek Trail

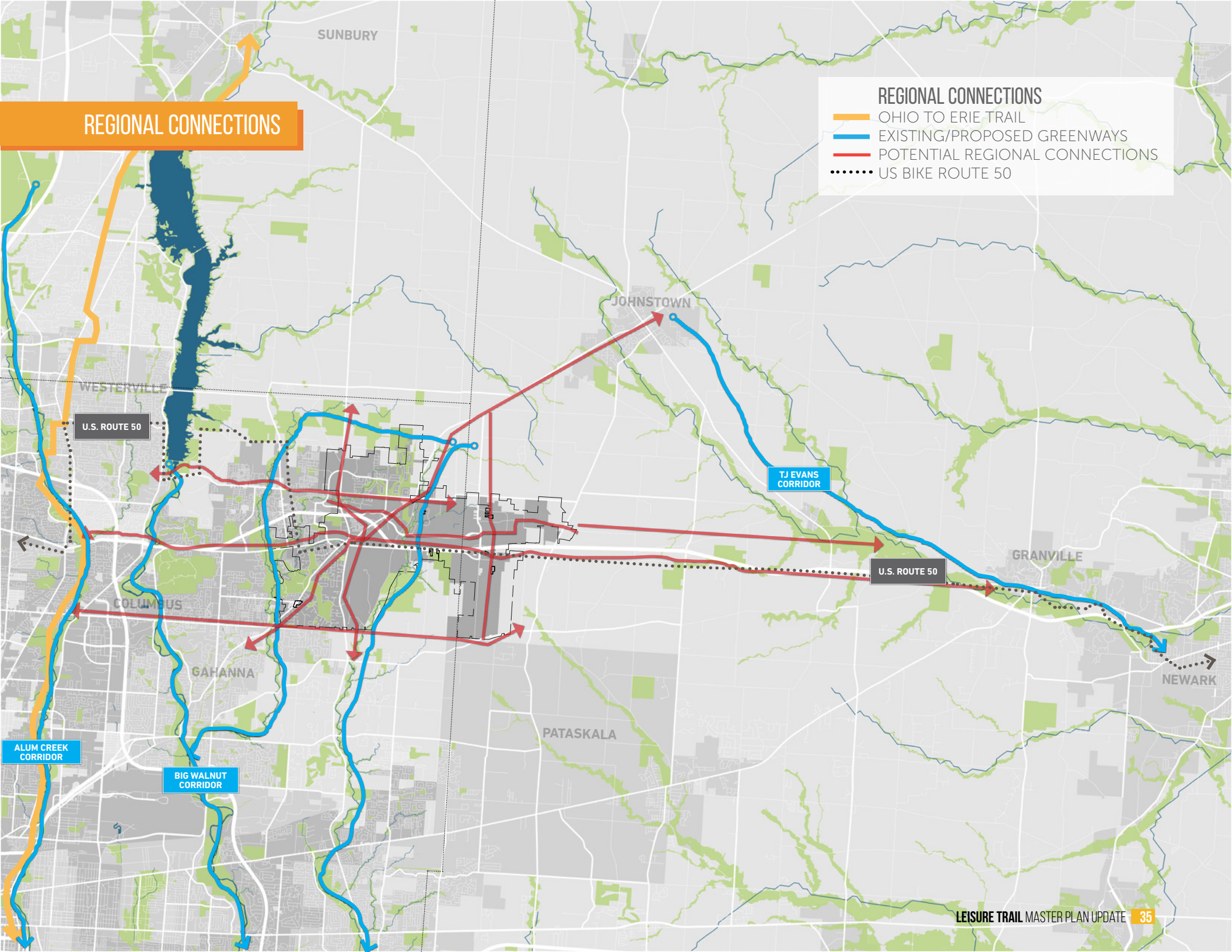


Existing T.J. Evans Trail

REGIONAL CONNECTIONS

REGIONAL CONNECTIONS

- OHIO TO ERIE TRAIL
- EXISTING/PROPOSED GREENWAYS
- POTENTIAL REGIONAL CONNECTIONS
- US BIKE ROUTE 50



SECTION 4: **RECOMMENDATIONS**

OVERVIEW

Recommendations

The purpose of this *Leisure Trail Master Plan Update* is to provide guidance and strategies to continue improving and expanding New Albany's leisure trail system. This plan identifies ten policy areas, each with its own set of specific recommendations to guide trail development citywide. This section identifies and details each of the recommendations.

PLAN RECOMMENDATION POLICY AREAS

- + 10 Priority Corridors
- + System Expansion
- + Design Standards
- + Safety
- + Materials
- + Maintenance
- + Wayfinding And Signage
- + Digital Application
- + Amenities
- + Education and Awareness
- + Velo Loop Improvements
- + Regional Coordination

PRIORITY CORRIDORS

Recommendations

OVERVIEW

Ten priority corridors have been identified (see map on following page) by the planning team and vetted by the Steering Committee and city staff. These corridors were identified as priorities based upon: 1) recommendations of the *2006 Leisure Trail Master Plan*, 2) frequently used trail corridors for pedestrians and cyclists, and 3) corridors that connect the city's neighborhoods and destinations like the Village Center, the Learning Campus, the Business Campus, and city, township and Metro parks. While the city should continue expanding the leisure trail system throughout the community, these ten corridors should receive priority attention.

The *Implementation* section of this plan (beginning on page 69) identifies specific potential improvements for each corridor. These potential improvements should be considered as candidate projects for future iterations of the City's Capital Improvements Program (CIP) as funding is identified and becomes available.

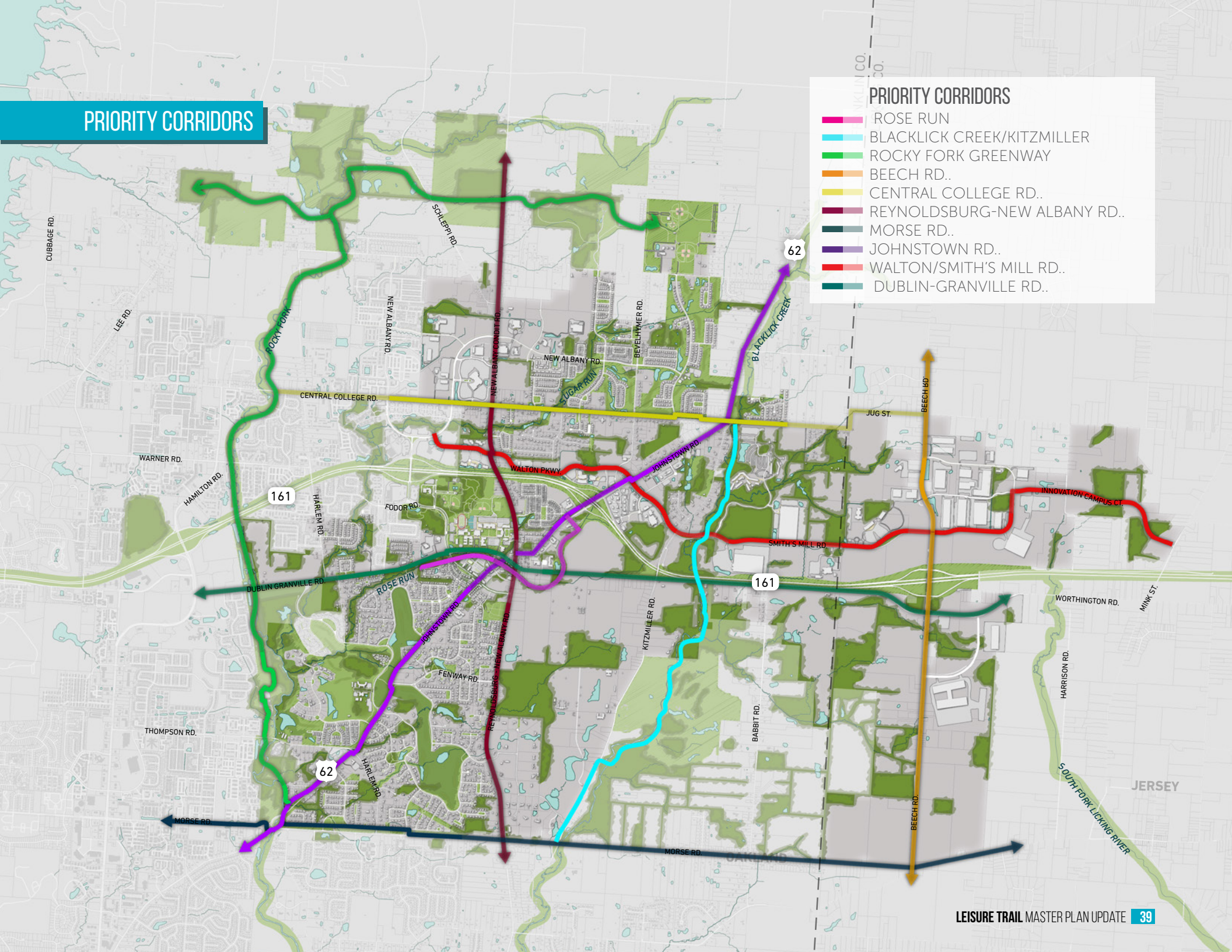
10 PRIORITY CORRIDORS

- + Rose Run Greenway
- + Rocky Fork Greenway
- + Blacklick Creek/Kitzmiller Hybrid
- + Beech Road Route
- + Central College Road Route
- + Reynoldsburg-New Albany Road Route
- + Morse Road Route
- + Johnstown Road Route
- + Walton Parkway/Smith's Mill Road Route
- + Dublin-Granville Road Route

PRIORITY CORRIDORS

PRIORITY CORRIDORS

- ROSE RUN
- BLACKLICK CREEK/KITZMILLER
- ROCKY FORK GREENWAY
- BEECH RD..
- CENTRAL COLLEGE RD..
- REYNOLDSBURG-NEW ALBANY RD..
- MORSE RD..
- JOHNSTOWN RD..
- WALTON/SMITH'S MILL RD..
- DUBLIN-GRANVILLE RD..



SYSTEM EXPANSION

Recommendations

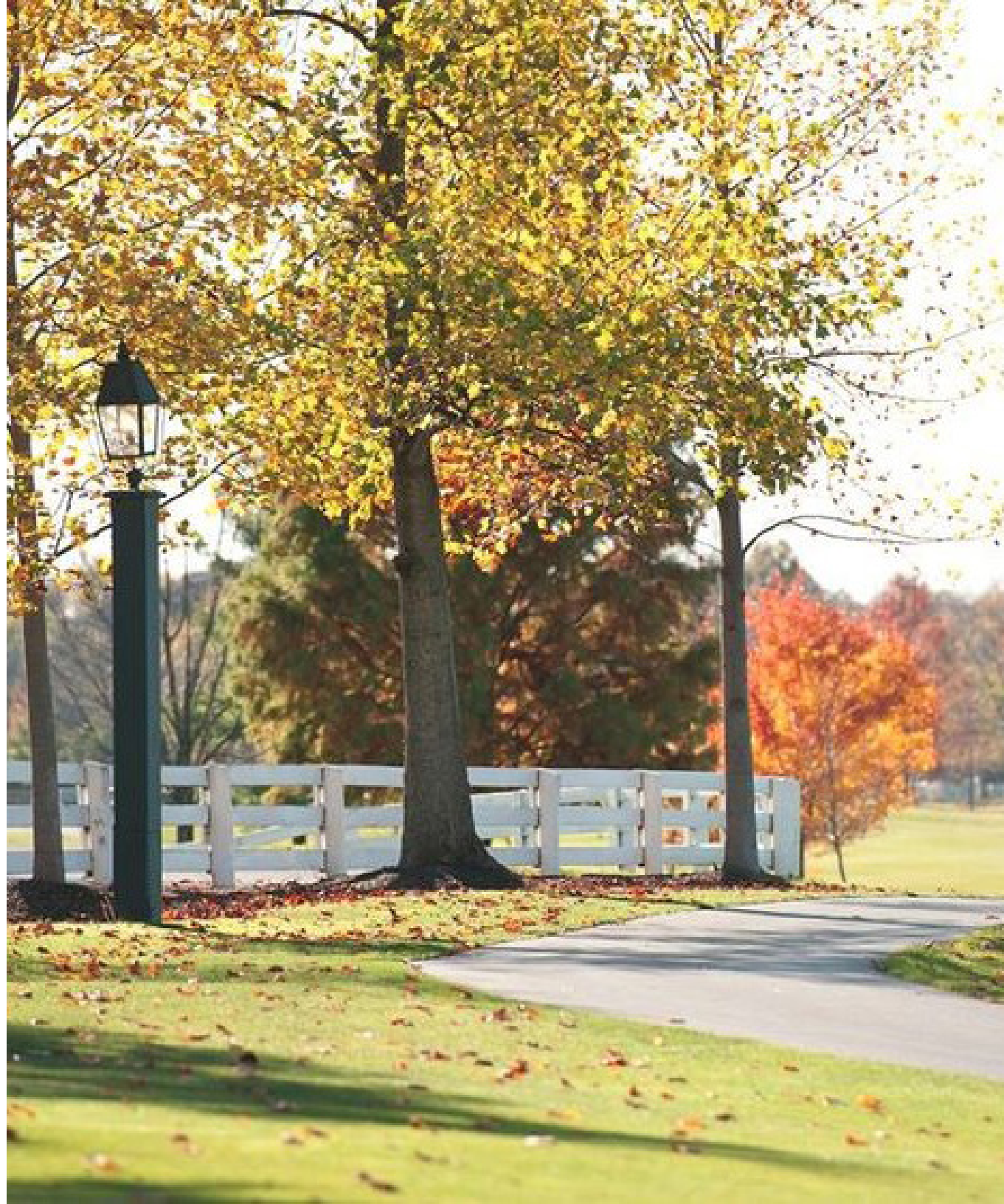
> Continue to expand the leisure trail system.

New Albany should continue to build leisure trails and grow its already established system of trails to serve its residents, students, and workers. This includes expanding along corridors, adding loops, creating greenways, and connecting to local amenities and regional trails. The *Implementation* section (p. 69) provides criteria considerations to assist with the prioritization process.

> Limit the use of fee-in-lieu for leisure trails.

The fee-in-lieu option for leisure trail development requirements should only be permitted in extenuating circumstances. The guidelines below should be followed with regard to leisure trail fee-in-lieu:

- + Fee-in-lieus shall not be used in safety zones (see map page 52) or on priority corridors (see map page 63).
- + If accepted, adequate right-of-way or easements must be dedicated to allow for future leisure trail construction and connection.
- + The amount of fee-in-lieu should match the actual cost of construction of the leisure trail. (Developer must provide construction cost estimate from engineer before development is approved.)



SYSTEM EXPANSION

Recommendations

> Improve the leisure trail connectivity across the major roadways.

The major arterials and collectors are barriers that create breaks in the leisure trail network, interrupting the low stress system and dividing neighborhoods (ex. SR 161, US 62, SR 605). Improve the connectivity by creating safe and effective crossings at strategic locations.

> Fill in leisure trail gaps to complete segments.

Prioritize easily accomplished gaps, gaps where funding can be secured, gaps that create safety issues, and those gaps along priority corridors.

> For new development, require the provision of direct access and connection from neighborhoods and businesses to the adjacent leisure trail, whether existing or planned.

Strategies to accomplish this may include:

- + Development standards/zoning update
- + Development agreements
- + Provision of easements

TRAIL SEGMENT GAPS



TRAIL CONNECTIONS



Example of Neighborhood Trail Connection



Example of Missing Site Connectivity

DESIGN STANDARDS

Recommendations

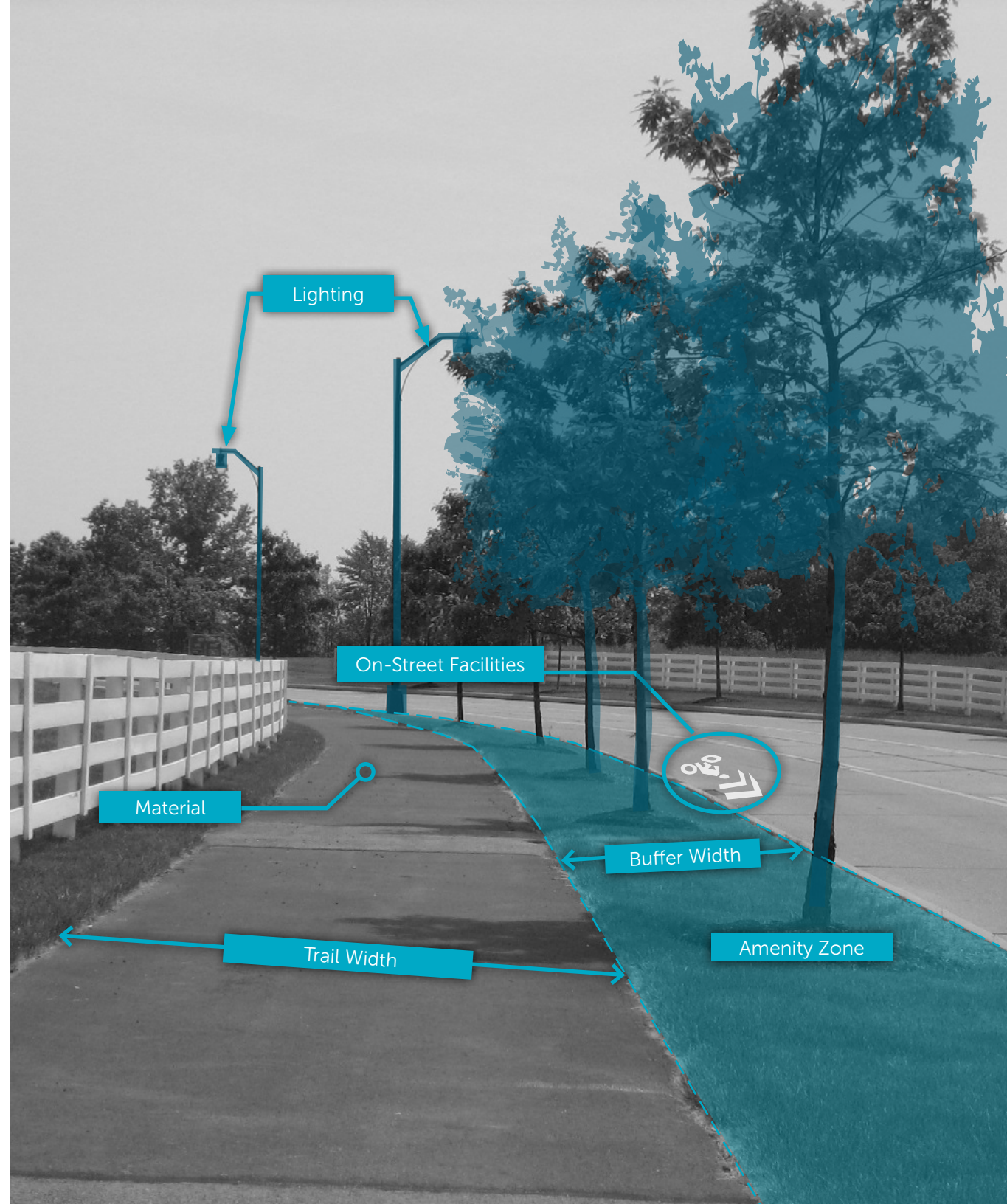
> New trail construction should meet the highest standards of design.

TYPOLOGIES

Leisure trails and streetscapes should be designed to reflect the character typologies originally established by the 2014 *New Albany Strategic Plan*. The plan recommends that all roadways and greenways should include a trail and/or sidewalk facility to serve pedestrians and cyclists. To guide the design of these trail and sidewalk facilities, trail “Section Typologies” provided in the 2006 *Leisure Trail Plan* have been refreshed and new trail “Crossing Typologies” were also developed. The Trail Typologies Map on the opposite page should be referenced to identify the streets, thoroughfares, and greenways to which the typologies apply. The subsequent pages provide specifications and considerations to guide form and functionality.

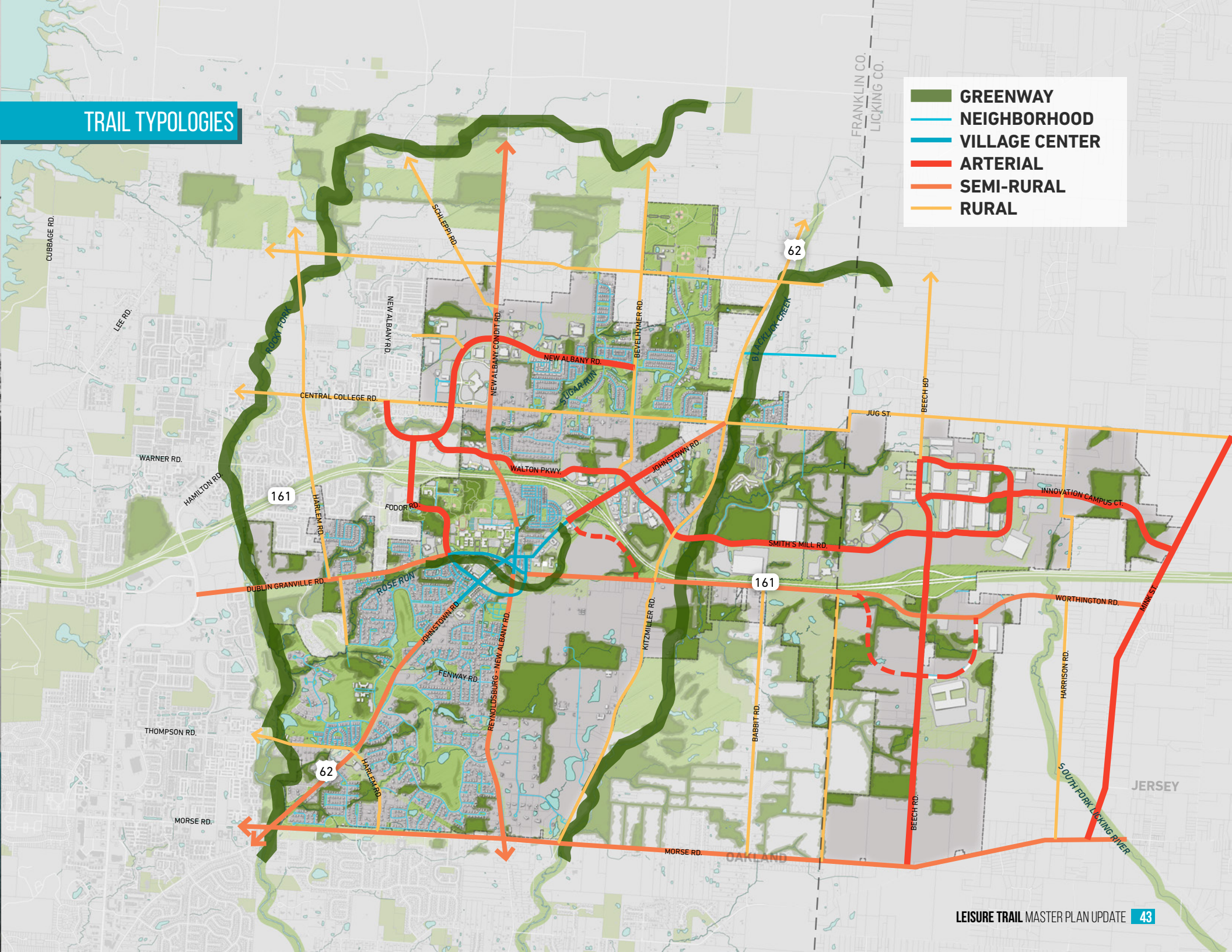
SECTION TYPOLOGIES

The Section Typologies guide design elements including: width, material, the buffer separating the trail or sidewalk from the street, if on-street bike facilities are present, and if trails and sidewalks should be on one or both sides of the street corridor.



TRAIL TYPOLOGIES

- █ GREENWAY
- █ NEIGHBORHOOD
- █ VILLAGE CENTER
- █ ARTERIAL
- █ SEMI-RURAL
- █ RURAL



DESIGN STANDARDS - SECTION TYPOLOGIES

Recommendations

NEIGHBORHOOD



Design Standards

Trail/Sidewalk Width	5' Sidewalk, min.
Material	Concrete
On-Street Bike Facility	Sharrows
Amenity Zone	Tree Lawn
Amenity Zone Width	6' min.
1 or 2 Sides of Road	2 Sides

Note: See special conditions, Page 45



Benchmark: Neiswander Square

VILLAGE CENTER



Design Standards

Trail/Sidewalk Width	6' Sidewalk, min.
Material	Concrete
On-Street Bike Facility	Sharrows
Amenity Zone	Trees/Planters
Amenity Zone Width	8' min.
1 or 2 Sides of Road	2 Sides



Benchmark: Market Street

GREENWAY



Design Standards

Trail/Sidewalk Width	12' Trail, min.
Material	Asphalt
On-Street Bike Facility	N/A
Amenity Zone	N/A
Amenity Zone Width	N/A
1 or 2 Sides of Road	N/A



Benchmark: Rocky Fork Greenway

DESIGN STANDARDS - SECTION TYPOLOGIES

Recommendations

ARTERIAL



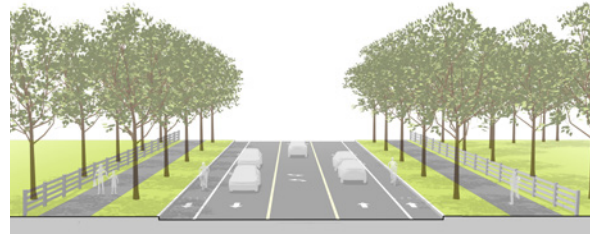
Design Standards

Trail/Sidewalk Width	8' Trail, min.
Material	Asphalt
On-Street Bike Facility	Bike Lanes
Amenity Zone	Tree Lawn
Amenity Zone Width	8'
1 or 2 Sides of Road	2 Sides



Benchmark: Beech Road

SEMI-RURAL



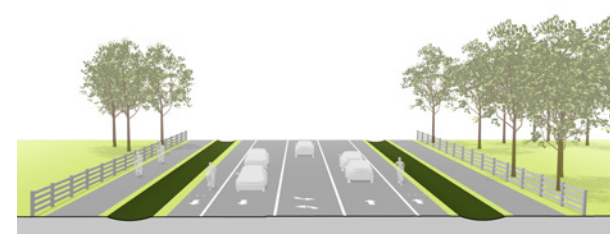
Design Standards

Trail/Sidewalk Width	8' Trail, min.
Material	Asphalt
On-Street Bike Facility	Bike Lanes
Amenity Zone	Tree Lawn
Amenity Zone Width	15'
1 or 2 Sides of Road	2 Sides



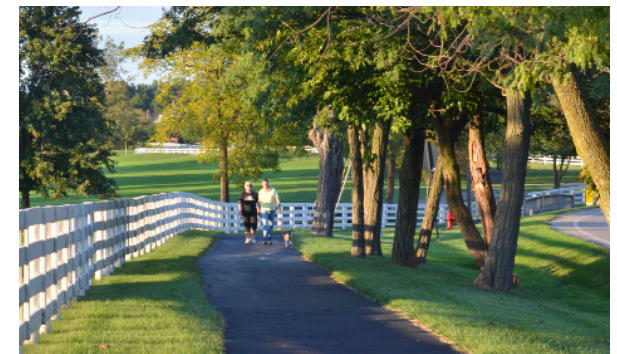
Benchmark: Walton Parkway

RURAL



Design Standards

Trail/Sidewalk Width	8' Trail, min.
Material	Asphalt
On-Street Bike Facility	Bike Lanes
Amenity Zone	Natural Buffer
Amenity Zone Width	20'
1 or 2 Sides of Road	2 Sides



Benchmark: Central College Road

DESIGN STANDARDS - SPECIAL CONDITIONS

Recommendations

ONE-SIDED ROAD ROUTES

> All arterial, semi-rural, and rural streets (see typologies) should have leisure trails on both sides of the corridor.

All non-neighborhood, non-Village Center road routes recommend that leisure trails be constructed on both sides of the roadway (per trail section typologies, page 45). While this should be policy for all new construction areas moving forward, some existing corridors may not need trails on both sides. The guideline is that for road sections directly adjacent to limited access and/or grade separated arterials (ex. SR 161), that side of the road may be considered for exemption. As no development is possible in these locations, it should not be an issue with development applications.

In the case of Morse Road, only the north side of the road corridor is within the City of New Albany. Nothing prevents jurisdictions on the opposite side from guiding the development of trails on their side of the corridor.

The map to the right illustrates corridors where trails may be appropriate on only one side of the road facility.



DESIGN STANDARDS - SPECIAL CONDITIONS

Recommendations

NEIGHBORHOOD PRIMARY ENTRY STREETS

> New subdivisions should include a leisure trail on one side of the primary entry street.

The Design Standards Section Typologies (page 44) illustrate sidewalks on both sides of *Neighborhood* streets. However, certain conditions — particularly the primary subdivision street into and/or through a neighborhood or district — warrant consideration of the use of a leisure trail or widened sidewalk on one side of the street.

For neighborhood streets which are also an entry street connecting the neighborhood to a city thoroughfare, a leisure trail should be constructed on one side of the entry street and a sidewalk on the opposite side. Particularly for larger subdivisions, this provides for bicycle connectivity to the leisure trail network.

The leisure trail should be eight (8) feet in width with a six-foot amenity buffer zone tree lawn. Alternatively eight-foot wide concrete can be substituted for the asphalt, provided the joints and scoring are farther apart to help materially designate this as part of the leisure trail system.

NEIGHBORHOOD STREET WITH LEISURE TRAIL



The leisure trail must connect to the city thoroughfare and continue along the entry street until the street meets its terminus at a cross street or destination such as a park or green. It is recommended the leisure trail connect through the subdivision to the city thoroughfare on the opposite side.

DESIGN STANDARDS - CROSSING TYPOLOGIES

Recommendations

CROSSING TYPOLOGIES

> Improve the leisure trail crossings, connections, and transitions per the Design Standards.

The preceding leisure trail section typologies guide the design of trails and sidewalks alongside streets, thoroughfares, and waterways. This section of the Design Standards provides a complementary collection of crossing section typologies to guide the design of leisure trail (and sidewalk) crossings of those same streets, thoroughfares, and waterways.

These crossing sections reference the same map (see Trail Typologies map, page 41) as the Section Typologies and should consider other contextual information, including but not limited to: annual average daily traffic (AADT) of the streets and thoroughfares, frequency of bike and pedestrian usage on trails and sidewalks at crossings, visibility, and the built and natural landscape of the area.

It should be noted that the Steering Committee preferred not to light the leisure trail system except at crossings points such as these.

NEIGHBORHOOD



Design Considerations

- Vehicular stop bar
- ADA compliant
- Special leisure trail striping

When to Use

- This should be used when leisure trails cross at an intersection with a neighborhood street (see map page 41).



Benchmark: Oxford Loop/Lewis Link Drive

VILLAGE CENTER



Design Considerations

- Vehicular stop bar
- Brick crosswalks
- Pedestrian activated signal
- "Bump-outs" at crossing
- ADA compliant

When to Use

- This should be used when leisure trails cross a street in the Village Center (see map page 41).



Benchmark: Market Street, Village Center

DESIGN STANDARDS - CROSSING TYPOLOGIES

Recommendations

CREEKSIDE UNDERPASS



Design Considerations

- Comfortable clearance height
- Open and well lighted
- ADA compliant

When to Use

- This standard should be used where appropriate topography exists.
- Use with high AADT roadways or highly used leisure trail routes.



Benchmark: US 62/Rocky Fork Bridge

CREEK BRIDGE

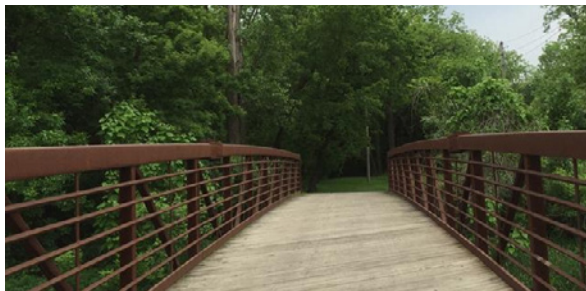


Design Considerations

- Wide enough to allow bike riders to pass abreast
- Scenic qualities of waterway
- ADA compliant

When to Use

- Use when necessary to cross creek along greenway corridor.
- Use where physical limits don't provide space roadside across a creek.



Benchmark: Indian Run Trail, Dublin

DITCH AND CHANNEL BRIDGE



Design Considerations

- Wide enough to allow bike riders to pass abreast
- ADA compliant

When to Use

- This standard should be used when a leisure trail crosses a ditch or small watercourse.



Benchmark: Leisure Trail in Oxford

DESIGN STANDARDS - CROSSING TYPOLOGIES

Recommendations

ROUND-A-BOUT



Design Considerations

- Crosswalk, brick in Village Center
- Protected median refuge
- Pedestrian signage
- Vehicular signage
- ADA compliant

When to Use

- This should be used when leisure trails cross at a roundabout.



Benchmark: Market/Main Street Roundabout

ARTERIAL



Design Considerations

- Vehicular stop bar
- Well delineated crosswalk markings
- Pedestrian activated signal
- ADA compliant
- Special leisure trail striping

When to Use

- This should be used when leisure trails cross at an intersection with an arterial street (see map page 41).



Benchmark: Fodor/Swickard Woods Blvd.

MID-BLOCK



Design Considerations

- Well delineated crosswalk markings
- Pedestrian stop sign
- Rectangular Rapid Flashing Beacons (RRFB) vehicle signage
- ADA compliant

When to Use

- Use with low AADT roadways or highly-used leisure trails routes.
- May be used for greenway corridors.
- May be used when intersection spacings are greater than 1,500'.

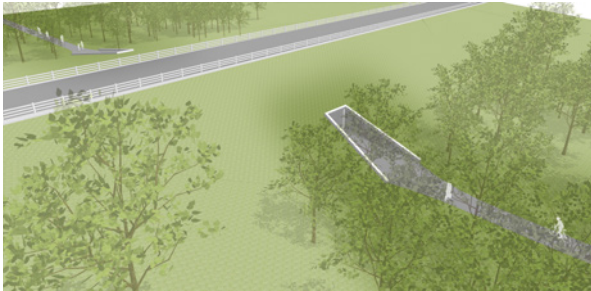


Benchmark: Leisure Trail Bevelhymmer Park

DESIGN STANDARDS - CROSSING TYPOLOGIES

Recommendations

BIKE PATH TUNNEL



Design Considerations

- Comfortable clearance height
- Open and well lighted
- ADA compliant

When to Use

- Use with high AADT roadways or highly-used leisure trail routes.
- May be used when intersection spacings are greater than 1,500'.



Benchmark: Leisure Trail under Morse Road

PROTECTED OVERPASS



Design Considerations

- Trail (or sidewalk) located above road deck with curb
- Protection fence, concrete wall, or bollards
- ADA compliant

When to Use

- This standard should be used when a leisure trail accompanies a road overpass/highway interchange.



Benchmark: SR-605 over SR-161

PROTECTED HIGHWAY UNDERPASS



Design Considerations

- Protective concrete hip-wall and rail
- Banked elevation of trail above grade
- Pedestrian activated signal at highway ramp crossings
- ADA compliant

When to Use

- This standard should be used when a leisure trail accompanies a road underpass/highway interchange.



Benchmark: Riverside Drive/US-33, Dublin

DESIGN STANDARDS - MITIGATION MEASURES

Recommendations

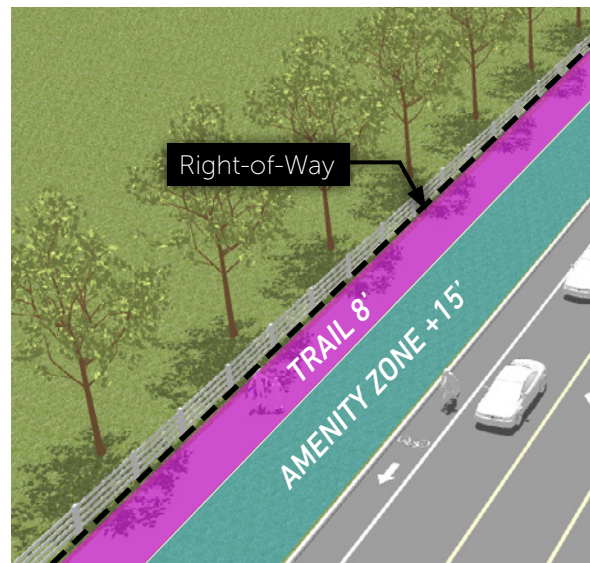
DESIGN MITIGATION FOR LIMITED RIGHT-OF-WAY

> Address limited right-of-way conditions to still provide leisure trail connectivity.

The guidelines specified in these Design Standards should be followed for leisure trail construction. However, there may be conditions in which, due to limited right-of-way, physical barriers, or other limiting factors, it is infeasible to construct a leisure trail to design standards. The diagram series to the right highlights mitigation measures to follow, in order of preference.

Figure 1 illustrates and details the preferred condition which allows leisure trail design standards to be met. Figures 2-5 illustrate scenarios where there is limited right-of-way or other limiting factors and then detail the mitigation process to reduce negative impacts on the quality and level of service of the leisure trail system.

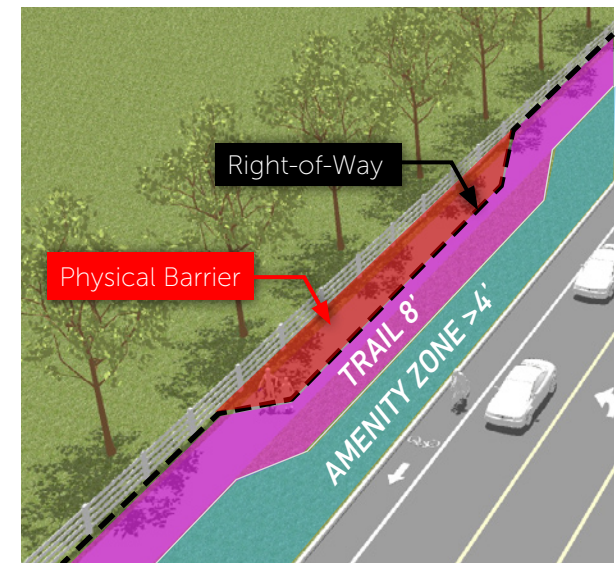
FIGURE 1: DESIGN STANDARD



Details

The diagram above illustrates an example of a preferred leisure trail section, following the leisure trail design standards in the typical condition where adequate right-of-way is available. In this case, the leisure trail width and amenity "buffer" zone meet the minimum standards.

FIGURE 2: REDUCE AMENITY ZONE WIDTH



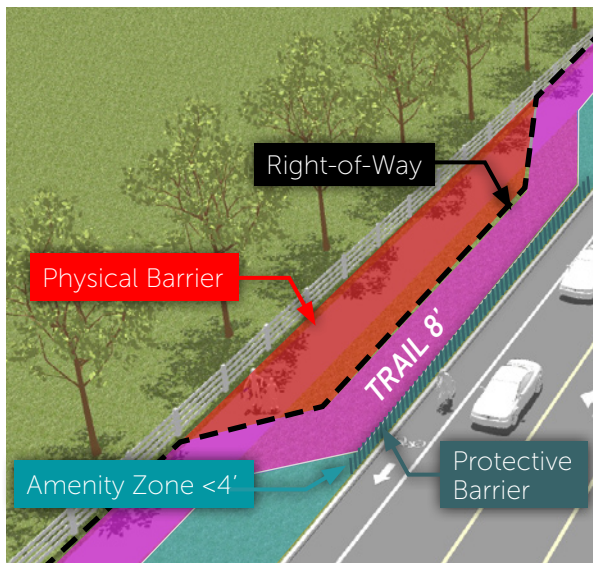
Mitigation Measure 1 Details

If there is limited right-of-way or a major physical barrier preventing the full implementation of the design standards, the first step is to reduce the amenity zone width in order to still accommodate the full-width leisure trail.

DESIGN STANDARDS - MITIGATION MEASURES

Recommendations

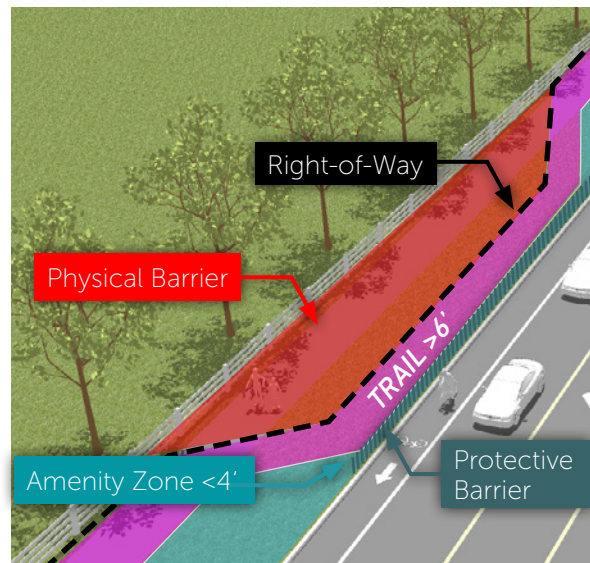
FIGURE 3: REDUCE AMENITY ZONE WIDTH TO <4'



Mitigation Measure 1B Details

If the amenity "buffer" zone must be reduced to less than four (4) feet (measured from the roadway edge of pavement to the leisure trail), a protective barrier must be installed between the travel lanes and leisure trail. This protective barrier should include a crash resistant fencing/railing with a 6" concrete curb. This protective barrier is similar to that shown in the Protected Overpass typology on page 51.

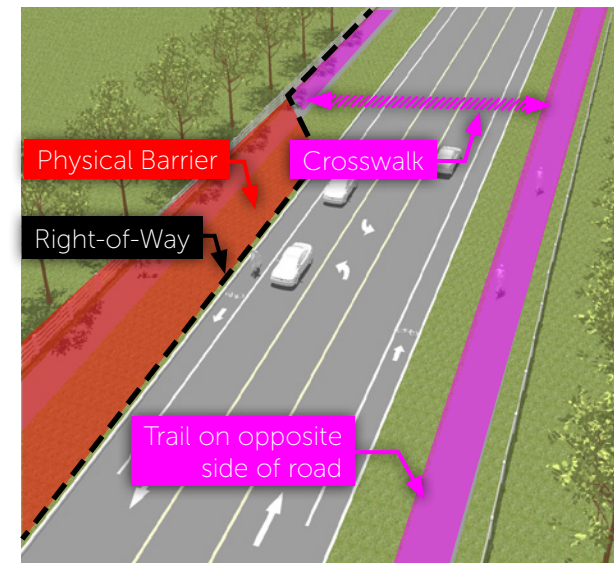
FIGURE 4: REDUCE LEISURE TRAIL WIDTH



Mitigation Measure 2 Details

If Mitigation Measure 1, reducing the amenity "buffer" zone width standards, still does not permit the space needed to accommodate a leisure trail with the minimum width to meet the design standard, the leisure trail width may be reduced. The minimum acceptable width is six (6) feet.

FIGURE 5: TRAIL CIRCUMNAVIGATION



Mitigation Measure 3 Details

If Mitigation Measures 1 and 2 still can not accommodate a trail, no leisure trail should be constructed on that side. To mitigate impacts on the quality and level of service of the leisure trail system, all steps should be taken to circumnavigate the segment of the trail which is infeasible. Steps include installing crossings to connect to the trail on the opposite side, constructing a trail on the opposite side of the road, and/or incorporating a leisure trail into and through the development.

SAFETY

Recommendations

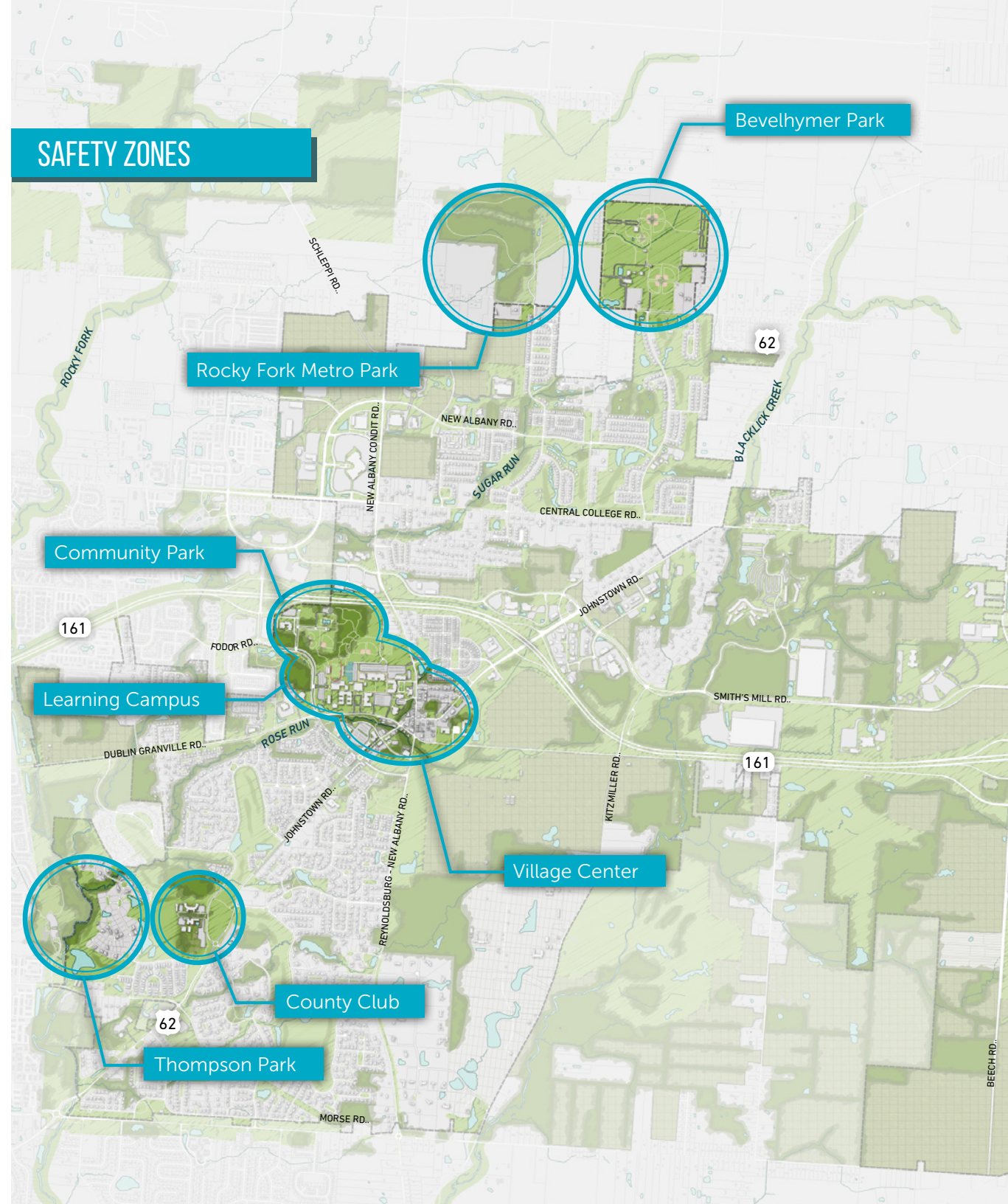
> **Establish Safety Zones around major community centers of activity where increased safety measures should be provided.**

Locations for Safety Zones should include:

- + Village Center
- + Learning Campus
- + Wexner Community Park, Thompson Park, Bevelhymer Park, and Rocky Fork Metro Park
- + The Country Club

Safety measures to consider within the Safety Zones may include:

- + Increased signage to alert drivers to leisure trail crossings.
- + Raised or differentiated materials at crossings.
- + Signage indicating rules of conduct for the system.
- + Pedestrian-triggered stop lights.
- + Street lighting when appropriate; for example at crossings.



SAFETY

Recommendations

> Establish a system and protocol for emergency management specifically for the leisure trail system.

Establish a system and protocol for emergency management specifically for the leisure trail system to make locating someone who is injured or ill much easier. This system could be similar to the color coded system in place for Bevelhymer Park.

> Periodically evaluate City ordinances on bicycle and pedestrian safety for effectiveness and amend as needed.

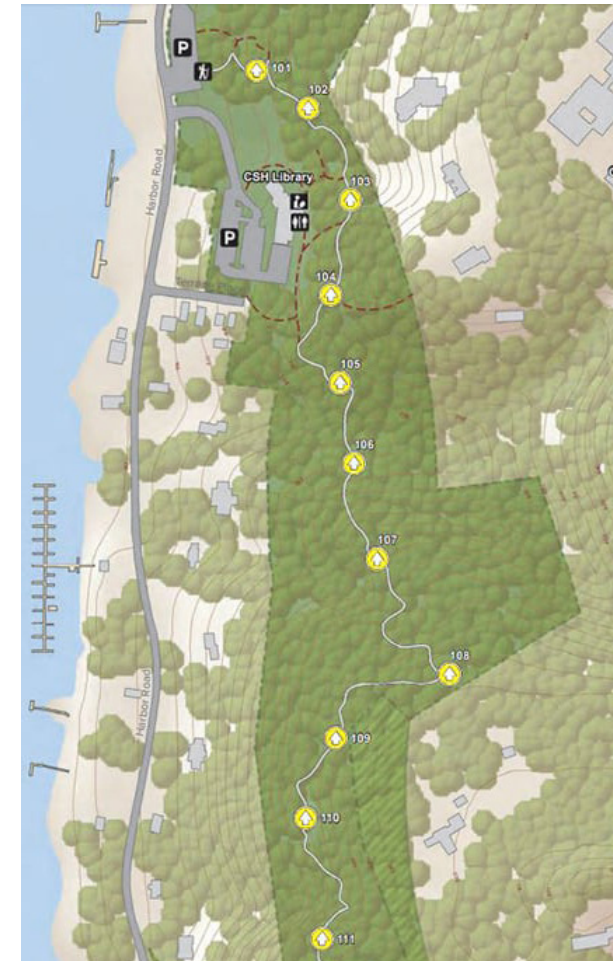
These ordinances may include:

- + Crosswalk/intersection alignment and geometry standards.
- + Pedestrian and bicycle signage standards for on-street and crossing facilities.
- + Requirements for distance between vehicles passing cyclists on streets.
- + Connectivity requirements
- + Maintenance requirements

BEVELHYMER PARK COLOR-CODED EMERGENCY ZONES PLAN



EXAMPLE OF EMT/SAFETY TRAIL MARKER MAPPING SYSTEM



MATERIALS

Recommendations

> Establish standard minimum widths for leisure trail construction.

Roadside leisure trails should be constructed with a minimum dimensional width of eight (8) feet. Dimensions in the Village Center may vary but should comply with ADA standards. Construction of greenway leisure trails should be a minimum twelve (12) feet dimensional width.

> Leisure trails should always be paved and asphalt should be the primary material.

There are five exceptions when a material other than asphalt may be used for the leisure trail system.

1. Bridge surfaces may be composed of material other than asphalt. These materials must be designed or adapted to be slip resistant (ex. on wood deck) and durable.
2. Areas where the trail is not well drained, pervious materials may be used.
3. In park spaces, when there is an existing paved asphalt leisure trail (constructed to at least minimum standard width), trails may be built from alternate aggregate material. The intent of this is to preserve

the natural experience of the park. Crushed stone (compacted stone base with compacted gravel screenings surface or crushed stone with stabilizer), crushed aggregate, and grass pavers may be used if the above conditions are met.

4. Pedestrian facilities in the Village Center are not leisure trails but they are critical pieces of the leisure trail system. As such, it is important to ensure the Village Center allows for easy movement of pedestrians and cyclists. To keep with the aesthetics of the Village Center, alternative materials to asphalt are recommended. These high quality materials may include:

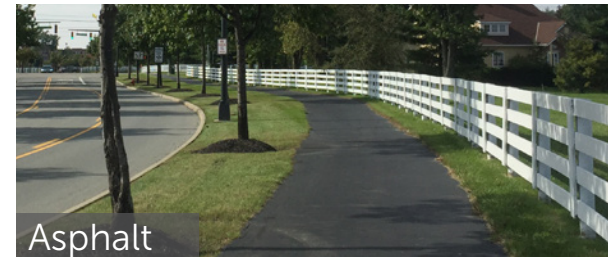
- + High-quality pavers
- + Pervious concrete

5. Similarly, neighborhood primary streets may have leisure trails constructed of concrete similar to sidewalks (see page 47).

> Consider sustainable materials.

Alternative, environmentally sustainable materials should be researched and reviewed to include as a potential trail material in the future. This should include recycled rubber.

MATERIALS



Note: All alternative materials must be approved by the City and should be evaluated against cost, maintenance, accessibility and feasibility considerations.

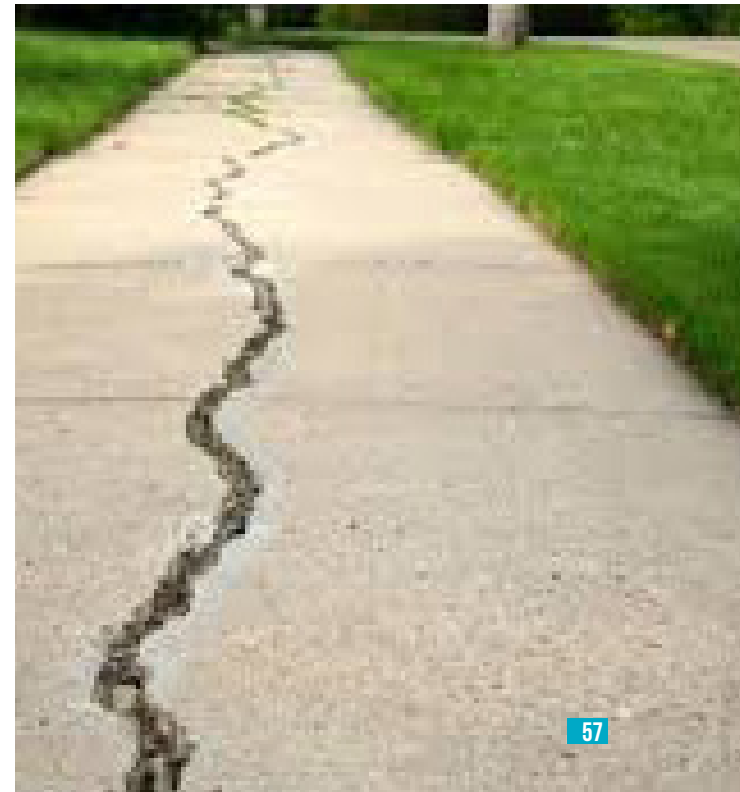
MAINTENANCE

Recommendations

> Consider crafting city guidelines for leisure trail maintenance.

The community uses the leisure trails all year, through the varying seasons and weather conditions. Residents expressed interest in ensuring the trails are regularly, consistently well-maintained. Maintenance guidelines should consider the following key components:

- + Establishing routine types of maintenance
- + Frequency of routine maintenance
- + Methods for funding route maintenance
- + Methods to prioritize maintenance needs
- + Procedures to address erosion and vegetative growth adjacent to trails
- + Procedures for addressing calls or e-mails for maintenance from community members
- + Seasonal issues, including flooding, snow removal, and leaf clearing policy (future snow removal policy should consider targeting areas around schools and the Village Center)
- + The use of technology to identify and assess maintenance needs
- + Preventative care strategies, including encouraging proper spacing of trees and tree species to ensure that tree roots do not damage the trail over time.



AMENITIES

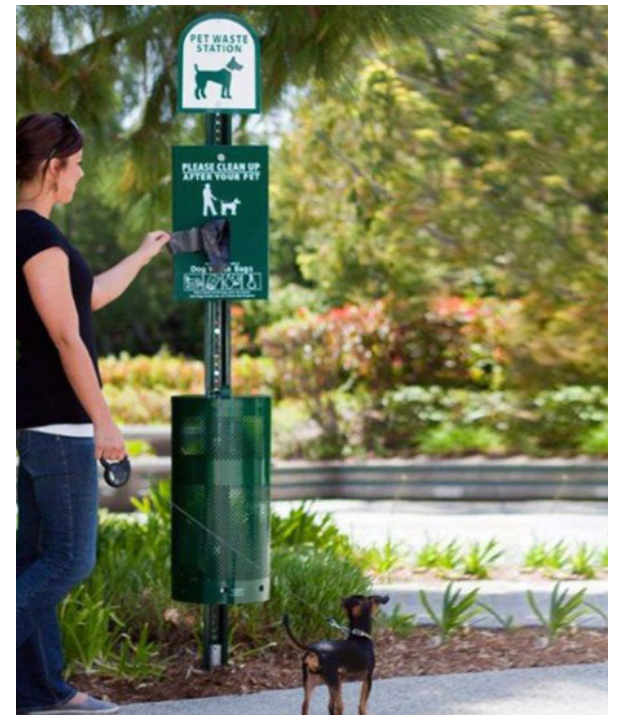
Recommendations

> Install amenities along leisure trails.

To support and encourage community use of existing and new leisure trails and to improve user experiences, amenities should be installed along the leisure trail system. Trail-side amenities to consider include:

- + Water fountains (@ trail heads)
- + Dog waste bins (every 1/2 mile along Loops)
- + Benches (@ trail heads and mile markers of regional systems)
- + Shelters (@ trail heads and mile markers of regional systems)
- + Tire pump station (@ trail heads)
- + Shade trees and seating (throughout system)
- + Bike lockers (@ Village Center)
- + Bike racks (@ trail heads)
- + Circuit training stations (@ trail heads)

EXAMPLES OF POTENTIAL AMENITIES TO BE INSTALLED ALONG TRAILS



AMENITIES

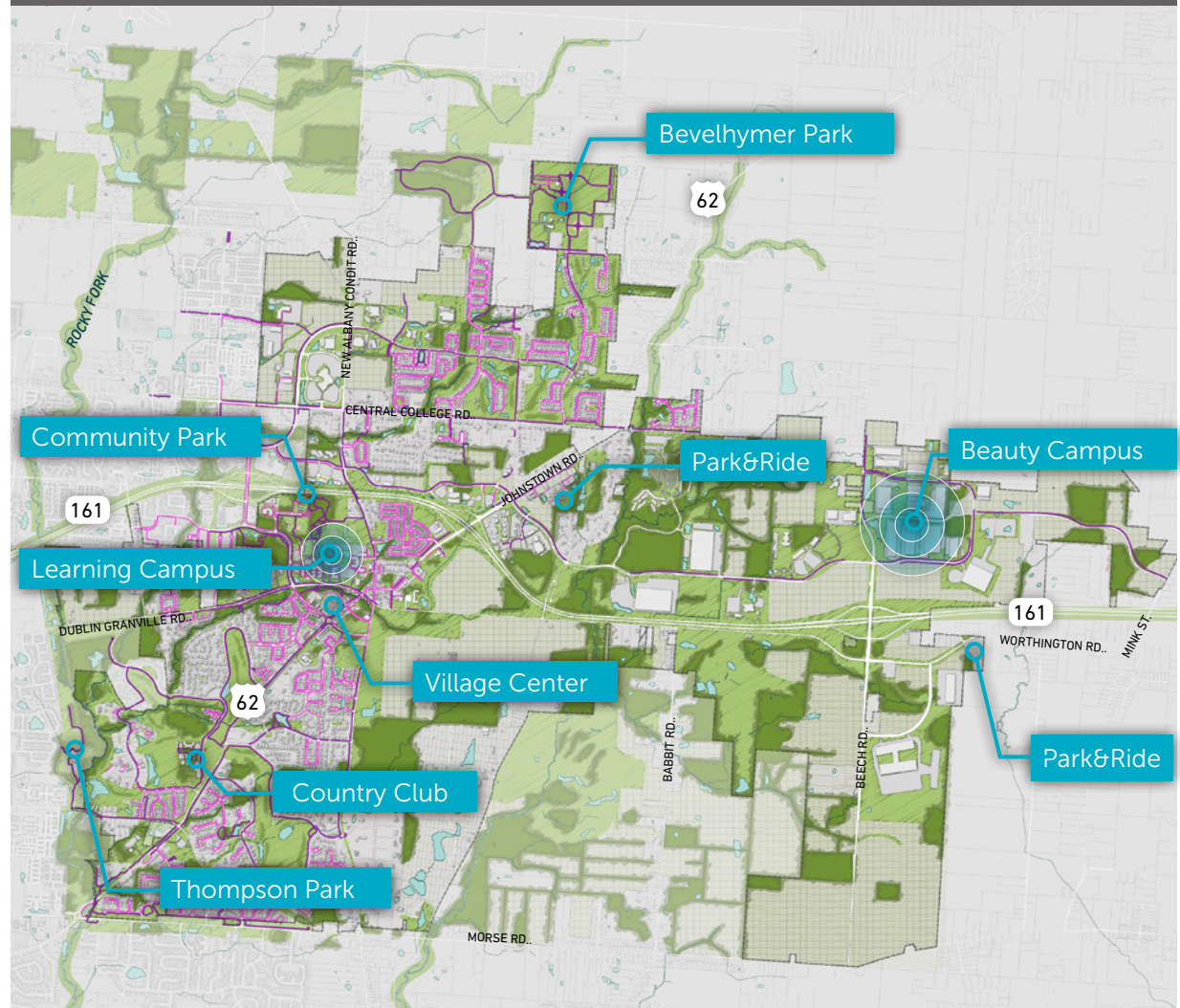
Recommendations

> Establish leisure trail trailheads at strategic locations.

The most capital intensive amenities should be targeted at trail heads. Trail heads should be located in destinations frequented by bike and pedestrian users and places where these amenities could encourage, promote, and make it easier for community members to bike and walk throughout the city. Locations to consider for trail heads include: the Village Center, the Learning Campus, Wexner Community Park, Thompson Park, Bevelhymer Park, the Personal Care and Beauty Innovation Campus, the New Albany County Club, and the Beech Road and Forest Drive Park-and-Rides. These potential trailhead locations are shown on the map at left.

In addition, the city should work with regional partners to provide similar amenities and trail connectivity at destinations outside the city limits. This is similar to what has been done at Rocky Fork Metro Park.

POTENTIAL TRAIL HEADS



WAYFINDING & SIGNAGE

Recommendations

> Craft a wayfinding and signage plan for the leisure trail network.

There is significant opportunity to brand and raise awareness of New Albany's leisure trail system. Appropriately done, it could encourage exploration of community destinations and amenities by the city's pedestrians and cyclists. It could brand the loops, provide travel distances and travel times, highlight points of interest and destinations, and locate trail amenities and service zones. Such a wayfinding plan should consider the following key components:

- + Create/adhere to signature "New Albany" character and brand, including simple high-quality signs that minimize visual clutter
- + Identify loops and routes
- + Provide directions to specific destinations
- + Provide directions to multiple destinations
- + Include distances and travel times
- + Place at intersections, decision points, and trail heads
- + Locate at destination to confirm arrival
- + Indicate to bicyclists that they are on designated bikeway
- + Indicate linkages to the regional system



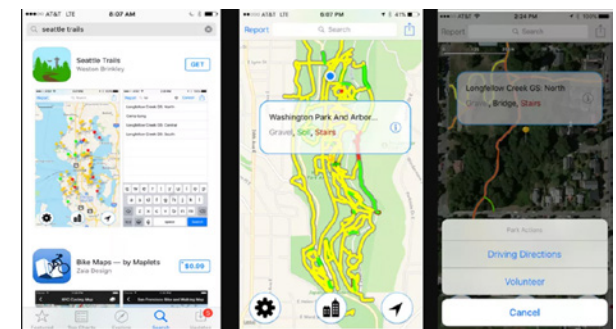
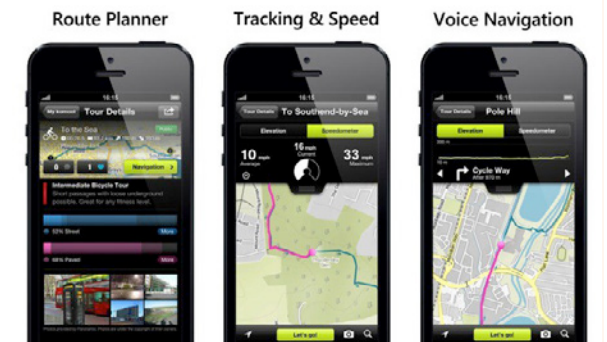
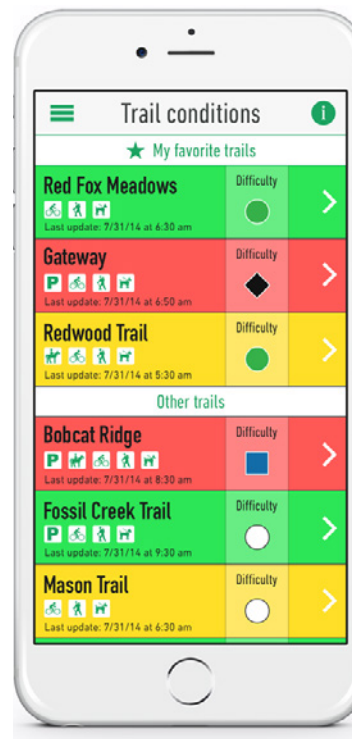
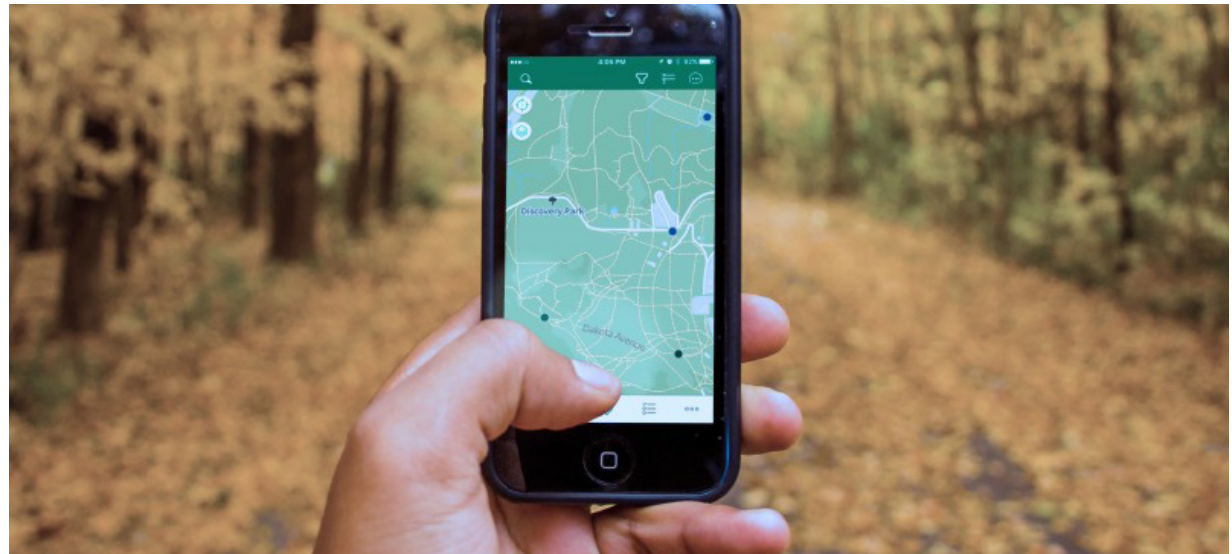
MOBILE APPLICATION

Recommendations

> Create a digital application for using and planning leisure trail trips and real-time location.

Creating a GPS mobile application customized to New Albany's leisure trails would make using trails more user friendly. Potential features of the application could include;

- + Categorize trails by difficulty level.
- + Locate nearby destinations.
- + Locate nearby trail amenities such as fix-it stations, water fountains, and restrooms.
- + Provide voice turn-by-turn directions.
- + Trail closings or detours.
- + If trail has been plowed during winter months.



EDUCATION & AWARENESS

Recommendations

> Create additional educational programs and activities geared toward promoting bike and pedestrian safety and awareness of leisure trails.

As part of a broader bicycle and pedestrian educational campaign, the city should collaborate with community and area organizations/agencies to create additional educational programs and activities geared toward promoting bike and pedestrian safety and raising awareness of leisure trails, their use and users, and the appropriate interaction between modes.

Key components of such an education program may include:

- + Cyclist education and policies for youth and adults
- + Motorist education and policies
- + Youth education and school campaigns
- + Continue New Albany Safety Town programs for children
- + Continue roundabout education
- + Create community/public walk and bike ride events
- + Work with local employers to expand Bike-to-Work Day



EDUCATION & AWARENESS

Recommendations

> Join the League of American Bicyclists.

Through information, advocacy, and promotion, the League of American Bicyclists works to create safer roads, stronger communities, and a bicycle-friendly America. This organization provides a wealth of technical support. Shown to the right is an example report card which provides insight into the performance of New Albany with regards to becoming a bicycle friendly community.



NEW ALBANY, OH

Fall 2018

TOTAL POPULATION

9,879

POPULATION DENSITY

844.36

TOTAL AREA (sq. miles)

11.7

OF LOCAL BICYCLE FRIENDLY BUSINESSES

0

OF LOCAL BICYCLE FRIENDLY UNIVERSITIES

0

10 BUILDING BLOCKS OF A BICYCLE FRIENDLY COMMUNITY

	Average Bronze	New Albany
High Speed Roads with Bike Facilities	20%	0%
Total Bicycle Network Mileage to Total Road Network Mileage	25%	20%
Bicycle Education in Schools	AVERAGE	NEEDS IMPROVEMENT
Share of Transportation Budget Spent on Bicycling	7%	UNKNOWN
Bike Month and Bike to Work Events	AVERAGE	NEEDS IMPROVEMENT
Active Bicycle Advocacy Group	YES	YES
Active Bicycle Advisory Committee	MEETS QUARTERLY	NONE
Bicycle-Friendly Laws & Ordinances	AVERAGE	NEEDS IMPROVEMENT
Bike Plan is Current and is Being Implemented	YES	YES
Bike Program Staff to Population	1 PER 154K	1 PER 99K

CATEGORY SCORES

ENGINEERING <i>Bicycle network and connectivity</i>	1.9 /10
EDUCATION <i>Motorist awareness and bicycling skills</i>	2.1 /10
ENCOURAGEMENT <i>Mainstreaming bicycling culture</i>	2.1 /10
ENFORCEMENT <i>Promoting safety and protecting bicyclists' rights</i>	3.1 /10
EVALUATION & PLANNING <i>Setting targets and having a plan</i>	1.9 /10

KEY OUTCOMES

	Average Bronze	New Albany
RIDERSHIP <i>Percentage of commuters who bike</i>	1.3%	0.00%
SAFETY MEASURES CRASHES <i>Crashes per 10k bicycle commuters</i>	1093	RIDERSHIP TOO LOW TO CALCULATE
SAFETY MEASURES FATALITIES <i>Fatalities per 10k bicycle commuters</i>	20	0



KEY STEPS TO **BRONZE**



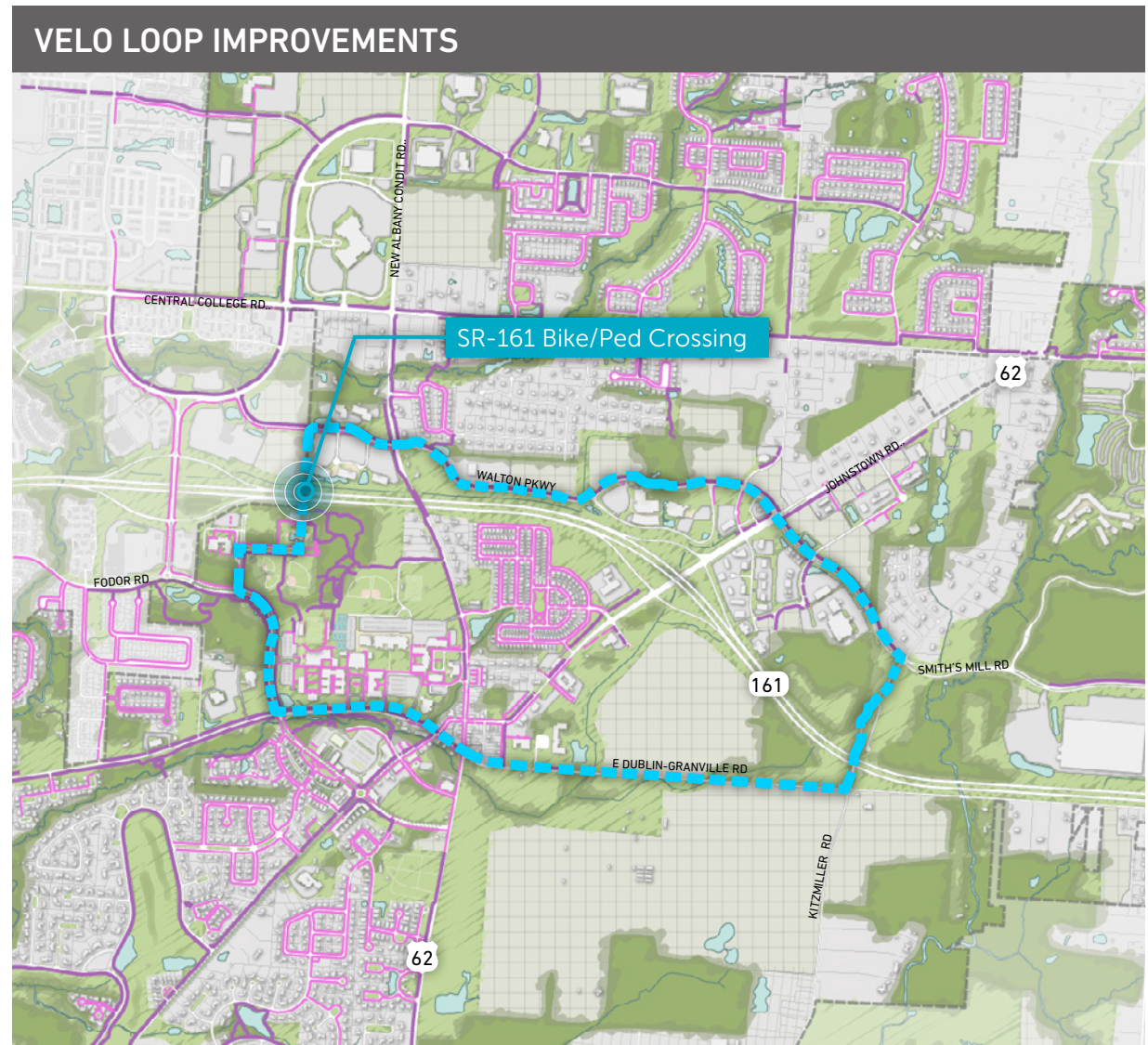
VELO LOOP IMPROVEMENTS

Recommendations

> Create the necessary improvements to complete the New Albany Velo Loop.

The Velo Loop creates a five-mile on-street bicycle facilities that are inviting to less experienced cyclists because of the physical separation from traffic that is a characteristic of cycle tracks. New improvements are needed to facilitate the proposed alignment of the Loop, presented in the Velo Loop Master Plan. A keystone segment includes, connecting the leisure trails to the proposed Velo Loop with a pedestrian bridge across State Route 161.

The proposed Loop alignment also creates a connection between the Village Center and the New Albany Business Park. The 2014 New Albany Strategic Plan emphasized the importance of creating stronger connections between these two community assets in order to help draw the approximately 12,000 employees from the Business Park to the businesses in the Village Center.



REGIONAL COORDINATION

Recommendations

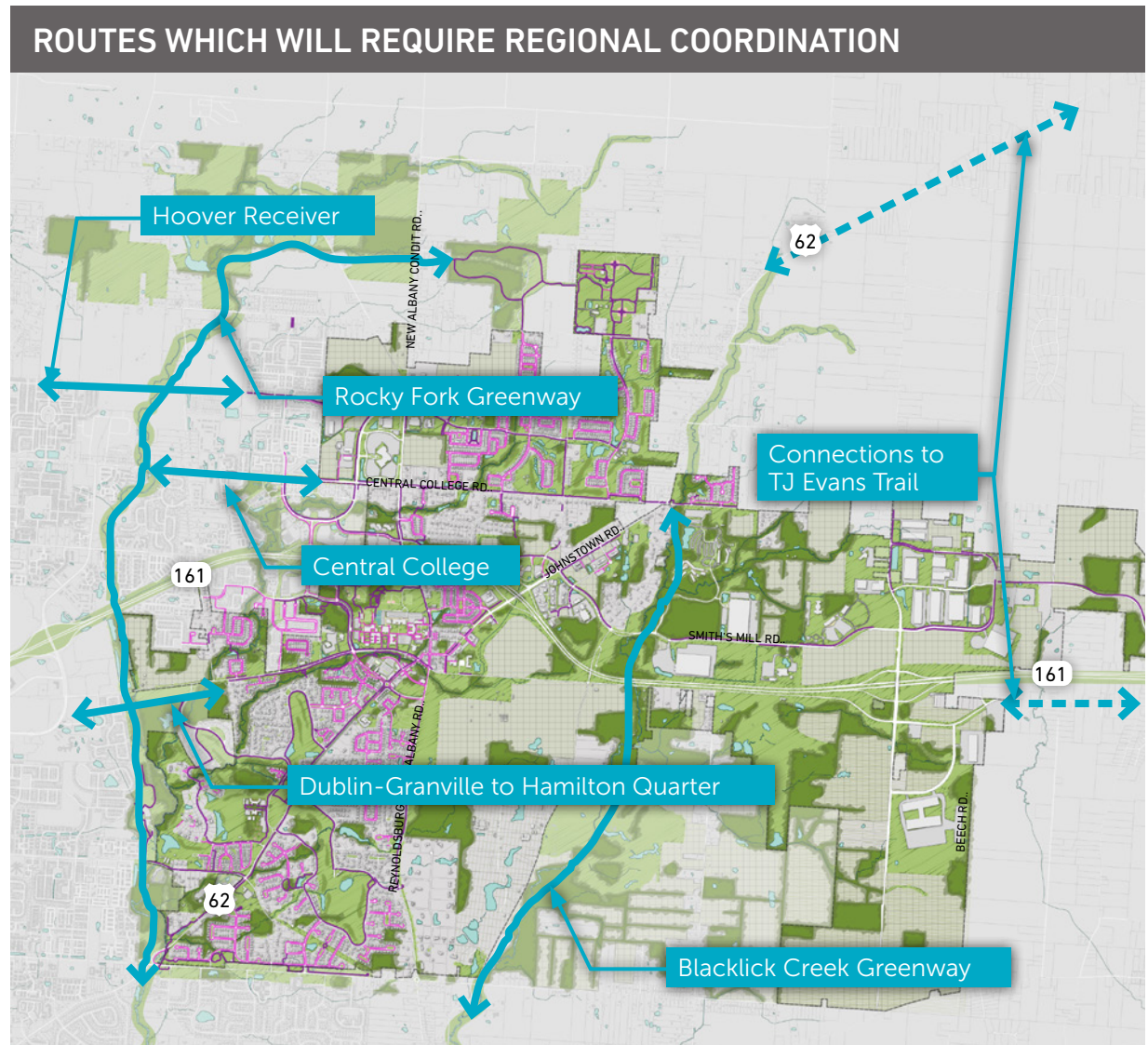
> **Coordinate regionally with neighboring communities, regional government entities, and other organizations to connect the leisure trail system to the Central Ohio and statewide trail systems.**

Potential regional partners include:

- + Adjacent municipalities
- + Metro Parks
- + Licking County Park District
- + TJ Evans Foundation
- + Preservation Parks of Delaware County
- + Mid-Ohio Regional Planning Commission (MORPC)
- + Ohio Department of Transportation (ODOT)
- + Multiple biking and walking advocacy organizations

The follow regional connections should be prioritized:

- + Rocky Fork Greenway Trail
- + Alum Creek Greenway Trail and the Hoover Reservoir
- + Blacklick Greenway Trail
- + Johnstown and TJ Evans & Panhandle Trails
- + US 50 Bike Route



SECTION 5: **IMPLEMENTATION**

NEXT STEPS

Implementation

This *Leisure Trail Master Plan* serves as a policy guide for City Council, City Boards and Commissions, City Staff, and the community. It should be a resource in evaluating regulations, policies, priorities, funding, and improvements related to the leisure trail system.

There is a wealth of reference information and guidance provided in the proceeding pages. It is expected that city staff will evaluate and advance the recommendations of this plan and develop initiatives for consideration by City Council and details for review by the Parks and Trails Advisory Board, Planning Commission, and other boards and commissions as necessary.

This plan should also serve as a guide and reference to be shared with: 1) developers to inform them of community expectations related to new development and leisure trails; 2) community members interested in the leisure trail system and desired future improvements; and 3) local, regional, and state funding partners to help achieve the vision.

It is expected that this Leisure Trail Master Plan will be updated in approximately ten years based upon changes in the intervening years and new conditions and expectations. Until that time, the City

PERFORMANCE METRICS

1. Increase in total system miles.
2. The completion status of each of the priority routes.
3. The completion status of loops and greenways.
4. Increase in the number of trail system users, by segment.
5. Increase in the number of residents, students, and workers directly connected to the leisure trail system.
6. Decrease in rate of bicycle and pedestrian related crashes.
7. Trail amenities installed.
8. Entry into the League of American Bicyclists.
9. Community Report Card by League of American Bicyclists.
10. Completion of wayfinding and signage master plan.

should regularly monitor and measure the performance and improvements to the leisure trail system, as well as the impacts of new improvements. Examples of performance metrics to track based upon the recommendations contained in the plan are included in the table above.

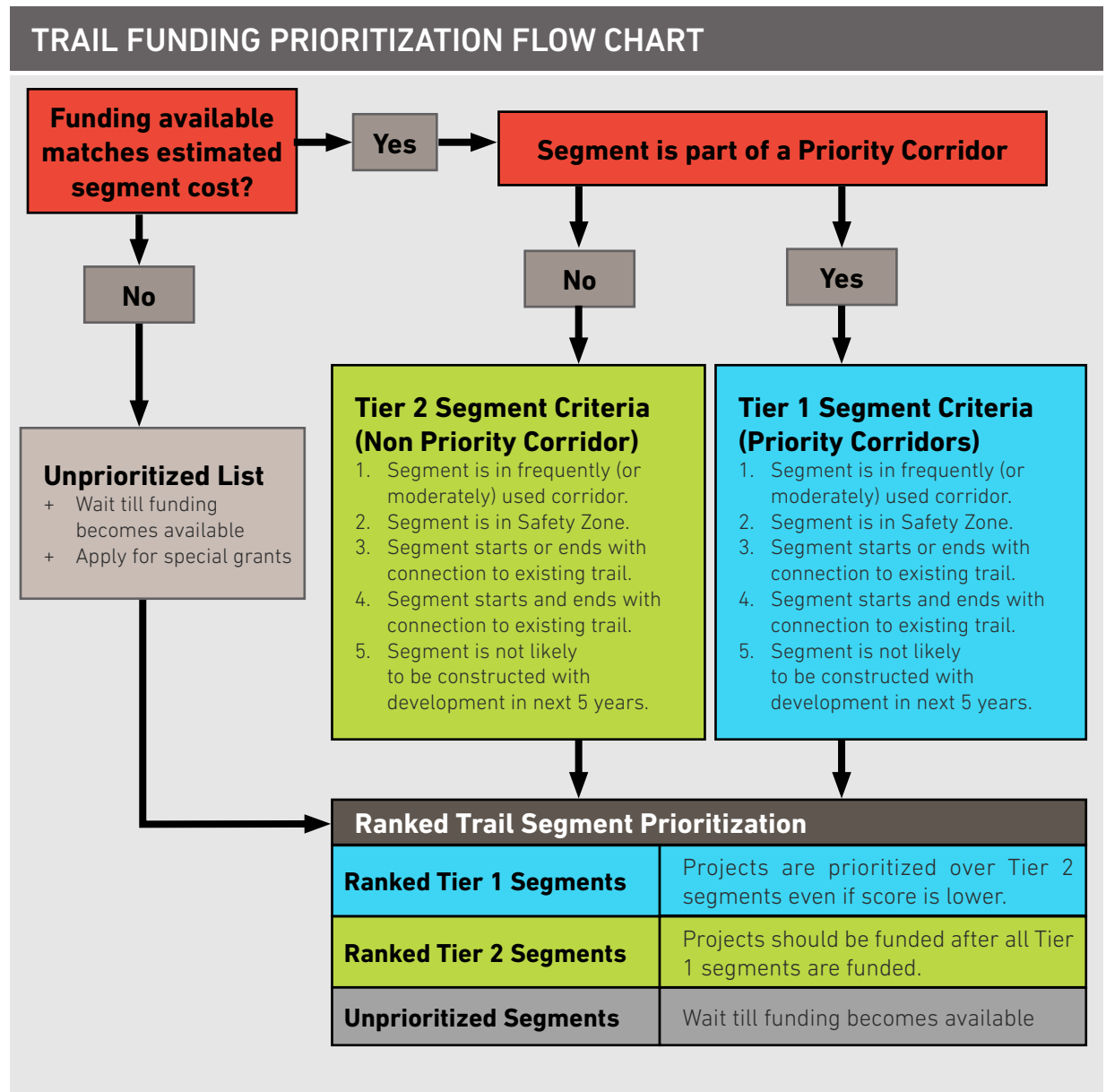
PRIORITIZATION

Implementation

A criteria and process to evaluate and score trail segments was developed to help prioritize and compare leisure trail projects which the city may fund or help to fund. The intent of this is not to be the rule but rather a guide to help aid decision makers. The figure to the right illustrates a suggested decision tree process. Additionally, the criteria that should be considered is listed below.

Trail prioritization criteria:

- + Trail is part of a frequently-used corridor (See map on page 29).
- + Trail is part of a priority corridor (See map on page 39).
- + Trail segment addresses a gap in the leisure trail system.
- + Trail is within a Safety Zone (See map on page 54).
- + Trail addresses an important safety issue.
- + Trail provides access to an underserved area or important community destination.
- + Segment starts or ends with connection to existing trail.
- + Trail construction is unlikely to occur via private development in the next 5 years.
- + There are interested funding partners.
- + Funding availability.



PRIORITY CORRIDOR IMPLEMENTATION

Implementation

PRIORITY CORRIDOR ASSESSMENTS

The potential build-out for each of the priority corridors was assessed to identify needed improvements. The leisure trail assessment identified existing and unbuilt corridor segments. For those unbuilt segments, the assessment also identified where existing development may limit trail construction as well as locations where segments may be constructed as part of new development. Below is a list of the segment types identified through the assessment.

- + *Existing Leisure Trail*: Segments of the corridors that have an existing leisure trail.
- + *Unbuilt with Challenged Development*: Areas of the corridors that are developed but have no leisure trail and where there is limited ROW to accommodate one.
- + *Unbuilt with Development*: Areas of the corridors that are developed but have no leisure trail but adequate right-of-way.
- + *Unbuilt with No Existing Development*: Areas of the corridors that are not developed and trails will likely be built with future development.
- + *Unbuilt Greenways*: Specific to greenway corridors, these unbuilt segments are part of large trail areas which will require further study to determine feasible alignments.

The remainder of the implementation section is dedicated to illustrating and detailing the potential improvements in the priority corridors per the above criteria.

WALTON/SMITH'S MILL

Recommendations

OVERVIEW

Walton Parkway and the Smith's Mill Road corridor were planned as the spine of the New Albany Business Park and has successfully been developed as that over the last 20 years. This corridor is the home of many successful businesses and thousands of employees. At 85%, this corridor is nearly complete. The segments that remain incomplete likely will be completed with future development except for one area located just east of Kitzmiller Road that was developed without trails.

WHY THIS CORRIDOR IS IMPORTANT

Walton/Smith's Mill is the spine of the New Albany International Business Park and one of the most frequently used corridors in New Albany for bike and pedestrian users. The corridor connects residential and employment opportunities as well as the Forest Drive P&R. Additionally, business park workers often use trails for exercise during lunch hours and because of gaps in the system are forced to walk on the grass or on the road, which is relatively busy.

POTENTIAL IMPROVEMENTS

Map ID	Improvement Description	Segment Length	Likely w/ Devel.	Side of Road
1	New leisure trail from Kitzmiller Rd.. to west entry drive of A&F Distribution South. Will require bridge over Blacklick Creek.	+/-2,000'	No	North
2	New trail construction from entry drive of A&F Distribution North to west of AEP offices.	+/-1,400'	Yes	North
3	New leisure trail from east of AEP offices to Beech Rd..	+/-820'	Yes	North
4	New leisure trail from Veepak Ohio entry drive to Innovation Campus Way.	+/-700'	Yes	North
5	New leisure trail from Kitzmiller Rd.. to west entry drive of A&F Distribution South. Will require bridge over Blacklick Creek.	+/-2,000'	Yes	South
6	New leisure trail between east and west entries to A&F Distribution South.	+/-1,400'	No	South
7	New leisure trail from east entry of A&F Distribution South to Bob Evans HQ property.	+/-1,400'	Yes	South
8	New leisure trail from east of Bob Evans HQ property to Beech Rd..	+/-3,400'	Yes	South
9	New leisure trail from Beech Rd.. east to existing trail.	+/-750'	Yes	South
10	Improve crossing at Kitzmiller Rd.. (see Design Standards for Arterial Crossing, page 48).	n/a	No	Crossing
11	Improve crossing at Beech Rd.. (see Design Standards for Arterial Crossing, page 48).	n/a	No	Crossing
12	Improve crossing at Innovation Campus Way (see Design Standards for Arterial Crossing, page 48).	n/a	No	Crossing

2.6 MILES
OF NEW
LEISURE TRAIL

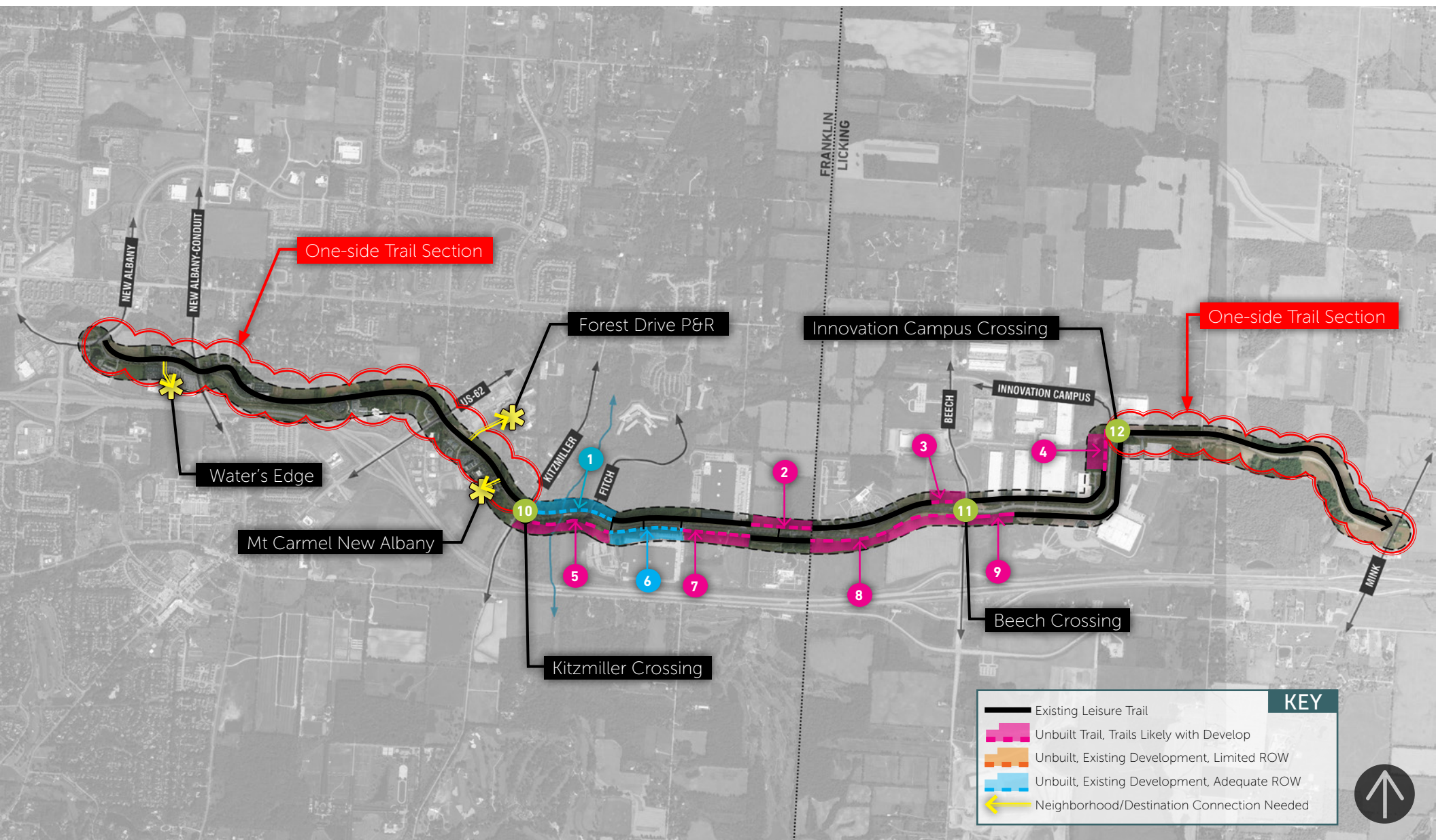
3

INTERSECTION
IMPROVEMENTS

3

CONNECTORS
TO DESTINATIONS

WALTON PARKWAY/SMITH'S MILL CORRIDOR IMPROVEMENTS



MORSE ROAD

Recommendations

OVERVIEW

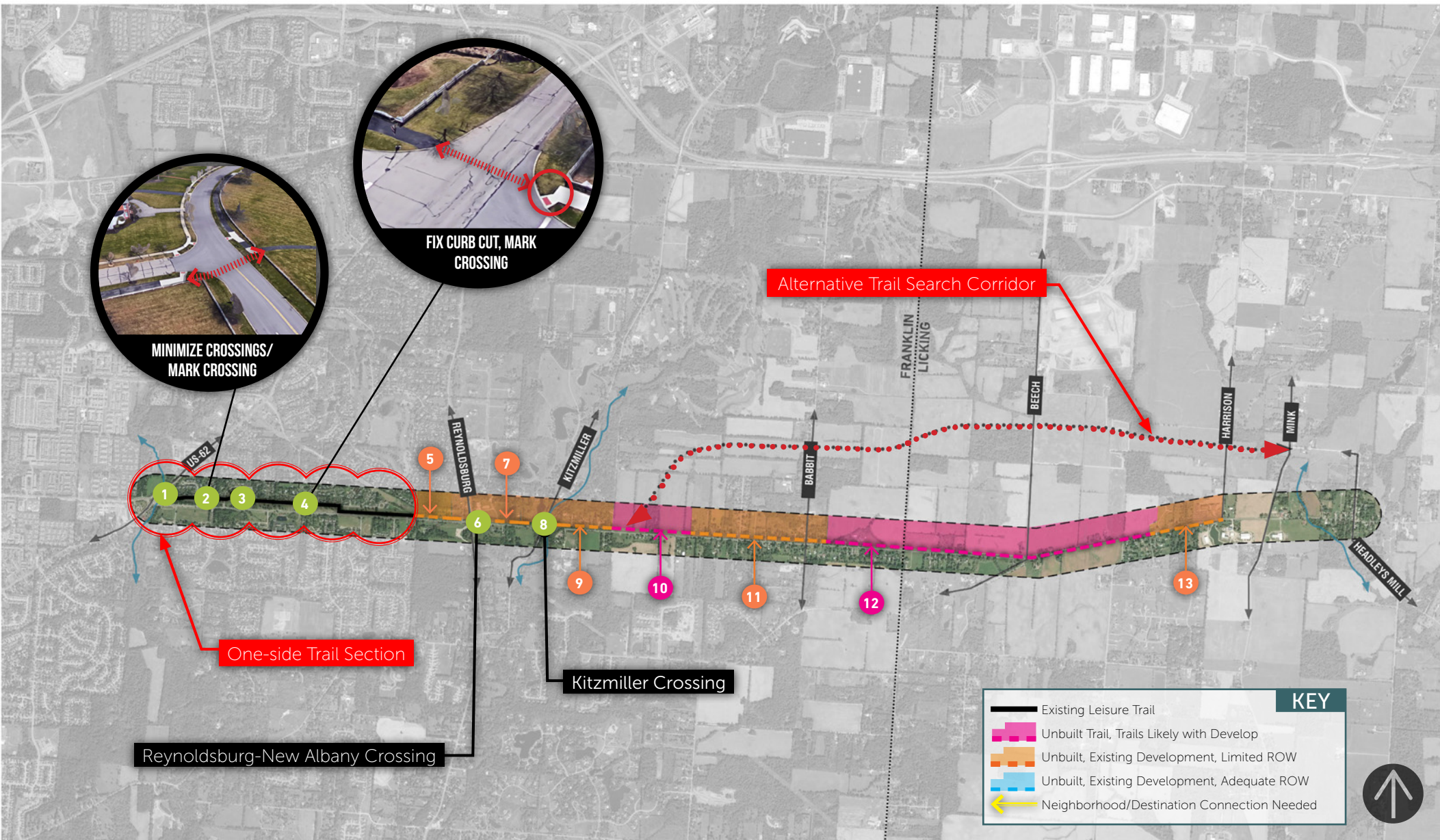
A significant rural corridor, Morse Road is only 23% complete. Only the north side of Morse Road is within the city, so all of the leisure trails have been constructed on that side. Between Johnstown Road and Reynoldsburg-New Albany Road, almost all of the trail is complete except a small section that will likely be built as part of a future private development and a small section that has already been developed without leisure trail. Between Reynoldsburg-New Albany Road and Beech Road, no trail has been built. Of this area, half of it is already developed by rural lots with limited right-of-way. The other half is expected to be built as part of private development.

WHY THIS CORRIDOR IS IMPORTANT

Morse Road is a major east-west connector for the city. Roadside leisure trails will greatly increase mobility and connectivity.

<div> <div>4.2 MILES OF NEW LEISURE TRAIL</div> <div>3 INTERSECTION IMPROVEMENTS</div> <div>2 WAYFINDING IMPROVEMENTS</div> </div>				
POTENTIAL IMPROVEMENTS				
Map ID	Improvement Description	Segment Length	Likely w/ Devel.	Side of Road
1	Improve wayfinding from Route 62 /Rocky Fork Greenway.	n/a	No	North
2	Improve Evelyton Rd.. crossing to minimize number of crossings for leisure trail.	n/a	No	North
3	Install wayfinding through park to clearly identify Morse Rd.. route through park.	n/a	No	North
4	Align curb cut at crosswalk of leisure trail crossing Harlem Rd. to provide a continues movement of leisure trail at intersection.	n/a	No	North
5	New trail leisure from end of Morse Rd.. trail to Reynoldsburg/ New Albany Rd..	+/-1,400'	No	North
6	Improve crossing at Reynoldsburg-New Albany roundabout (see Design Standards for Roundabout Crossing, page 48).	n/a	No	North
7	New leisure trail from Reynoldsburg-New Albany to Kitzmiller.	+/-1,900'	No	North
8	Improve crossing at Kitzmiller Rd.. (see Design Standards for Arterial Crossing, page 48).	n/a	Yes	North
9	New leisure trail from Kitzmiller Rd.. to Avis. Blacklick Creek crossing, existing road bridge may have capacity to incorporate trail.	+/-1,900'	Yes	North
10	New leisure trail from Avis Rd.. to west of Pine Meadows Rd..	+/-2,100'	Yes	North
11	New leisure trail from west of Pine Meadows Rd.. to east of Babbit Rd..	+/-3,800'	No	North
12	New leisure trail from east of Babbit Rd.. to west of Jersey Baptist Church.	+/-9,000'	Yes	North
13	New leisure trail from west of Jersey Baptist Church to Harrison Rd..	+/-2,100'	No	North

MORSE ROAD CORRIDOR IMPROVEMENTS



JOHNSTOWN/ROUTE 62

Recommendations

OVERVIEW

The Johnstown Road/Main Street corridor is one of the defining rural corridors that define New Albany. At 77% complete, this corridor is well developed and connects many of the major neighborhoods, businesses, and the Village Center. Underpasses have been added at Morse Road and Fenway Road to ease movement and improve safety. Currently, there is no access under SR-161. On the north end of the corridor between Central College Avenue and Walnut Street, the many existing developed residential rural lots with limited right-of-way pose a challenge for the addition of leisure trails.

WHY THIS CORRIDOR IS IMPORTANT

The Johnstown/Route 62 is a frequently used route that connects many of the County Club neighborhoods with the Village Center and is a significant North-South corridor across SR-161.

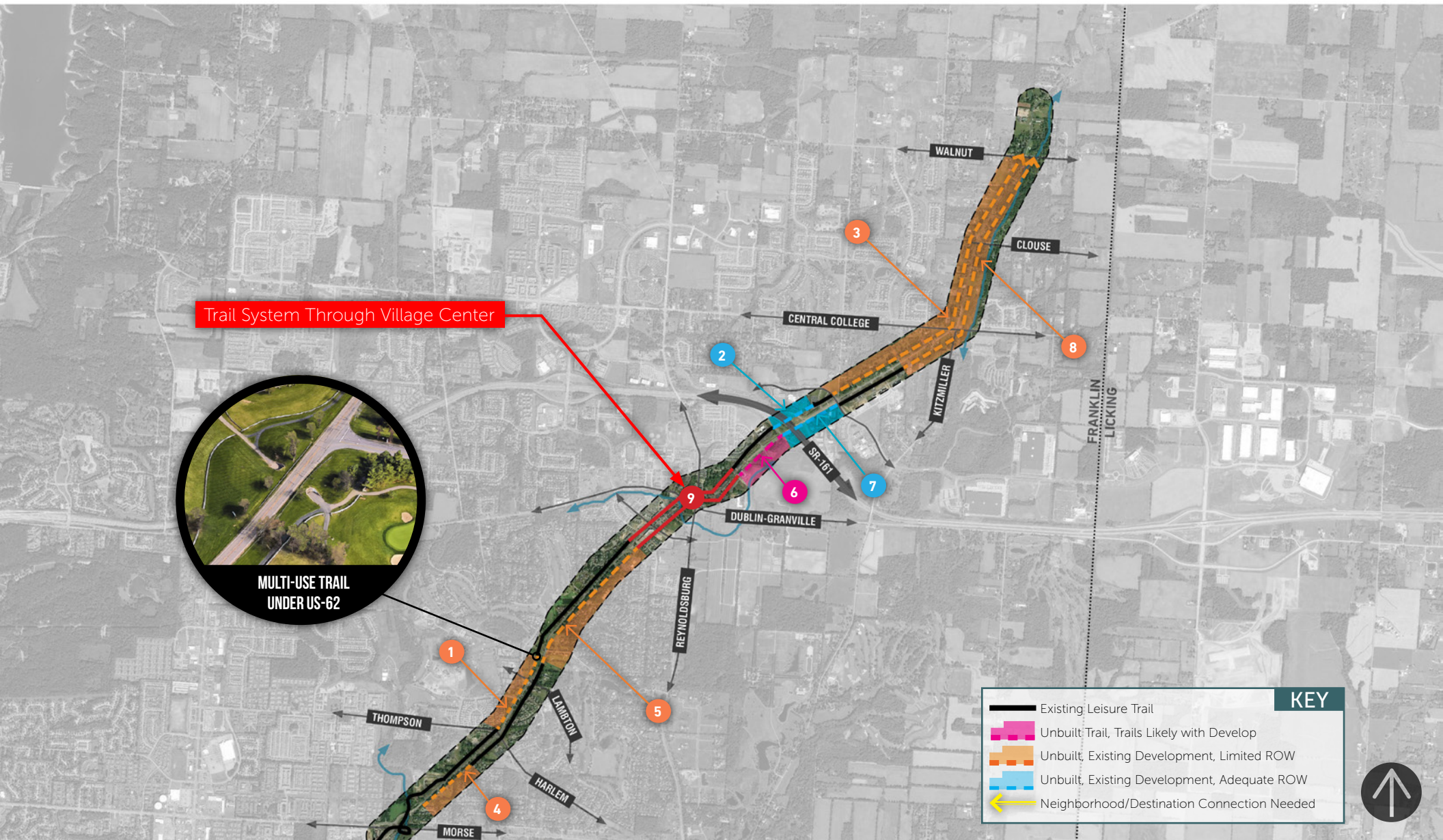
5.9 MILES
OF NEW
LEISURE TRAIL

NEW
CROSSING
UNDER SR-161

POTENTIAL IMPROVEMENTS

Map ID	Improvement Description	Segment Length	Likely w/ Devel.	Side of Road
1	Improve wayfinding from Route 62 /Rocky Fork Greenway.	+/-2,800'	No	North
2	New leisure trail from east of Marburn Academy, under SR-161 to Blue Agave/Tim Horton's. (see Design Standards for Protected Highway Underpass Crossing, page 49).	+/-1,500'	No	North
3	New leisure trail from Walton Pkwy. to Walnut St.	+/-9,800'	No	North
4	New leisure trail from east of Rocky Fork Creek Bridge to Whyte House Ln.	+/-2,000'	No	South
5	New leisure trail from Fenway Rd.. to Village Center.	+/-4,000'	No	South
6	New leisure trail from Fire Station to west of SR-161	+/-600'	Yes	South
7	New trail leisure from west of SR-161 to Walton Pkwy. (see Design Standards for Protected Highway Underpass Crossing, page 49).	+/-2,000'	No	South
8	New leisure trail from school bus garage to Walnut St.	+/-7,600'	No	South
9	Various segments and minor Improvements to sidewalks in the Village Center along corridor.	+/-1,000'	No	North & South

JOHNSTOWN ROAD/ROUTE 62 CORRIDOR IMPROVEMENTS



BEECH ROAD

Recommendations

OVERVIEW

With the addition of a new interchange at SR-161, the Beech Road corridor has developed into a successful extension of the New Albany Business Park. With the improvements on Beech Road south of SR-161, including leisure trails on both sides of the road extending to Morse Road, this corridor is now 84% complete. In addition to these improvements, a trail was completed under SR-161. North of SR-161, there is one segment that is likely to be completed with future development.

WHY THIS CORRIDOR IS IMPORTANT

Recent and ongoing employment centers along Beech Road have made the corridor an important economic development tool for the city. Transportation alternatives and work place amenities such as trails for exercise promote health and wellness and make these employment sites more attractive for employers and employees.

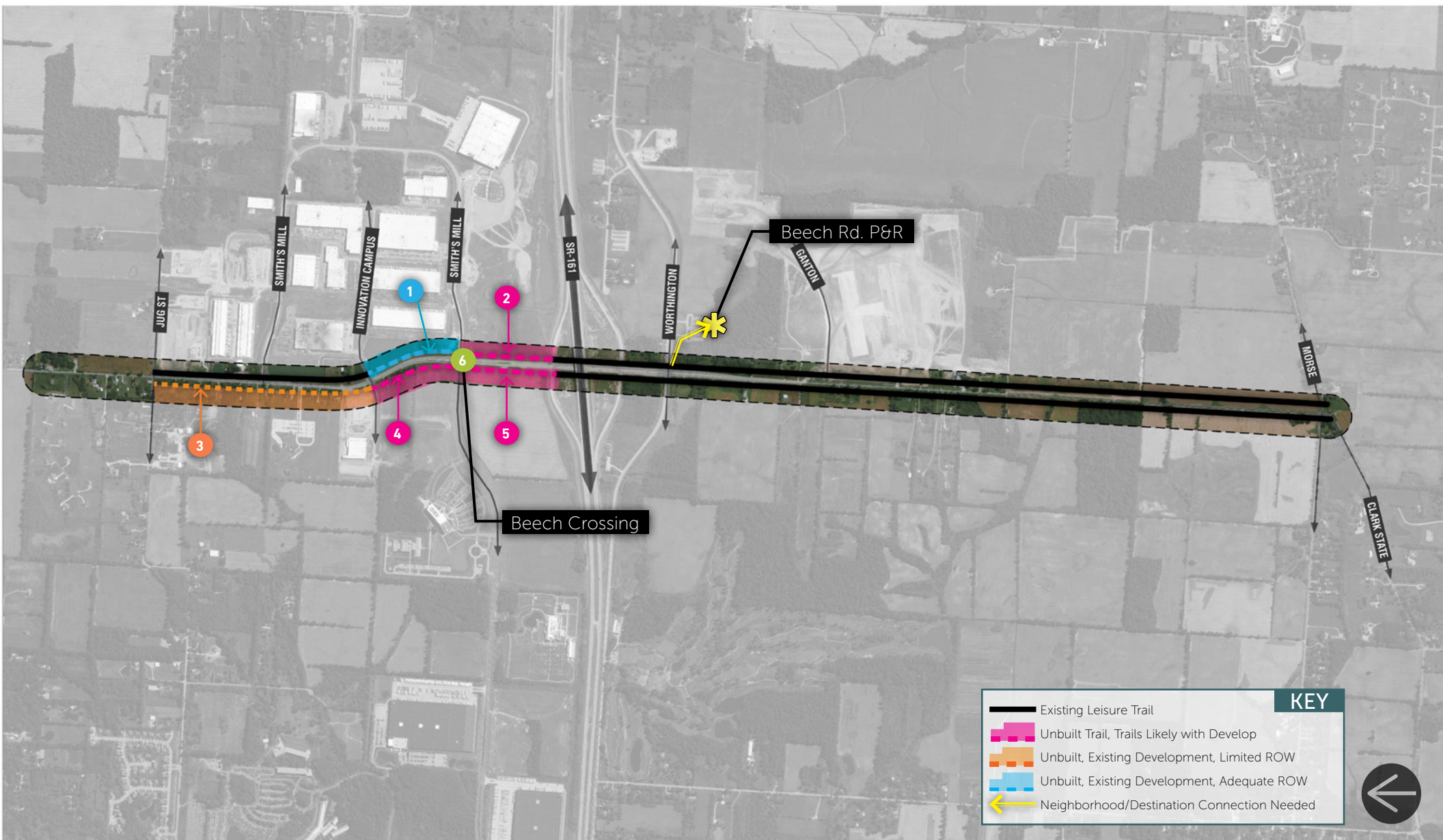
POTENTIAL IMPROVEMENTS				
Map ID	Improvement Description	Segment Length	Likely w/ Devel.	Side of Road
1	New leisure trail from Innovation Campus Way to Smith's Mill Rd..	+/-1,400'	No	North
2	New leisure trail from Smith's Mill Rd.. to SR-161.	+/-1,300'	Yes	North
3	New leisure trail from Jug St. to Innovation Campus Way.	+/-3,100'	No	South
4	New leisure trail from Innovation Campus Way to Smith's Mill Rd..	+/-1,300'	Yes	South
5	New leisure trail from Smith's Mill Rd.. to SR-161.	+/-1,300'	Yes	South
6	Improve crossing at Beech Rd. (see Design Standards for Arterial Crossing, page 48).	n/a	No	Inters.

1.6 MILES
OF NEW
LEISURE TRAIL

1
INTERSECTION
IMPROVEMENTS

1
CONNECTOR
TO DESTINATIONS

BEECH ROAD CORRIDOR IMPROVEMENTS



ROCKY FORK GREENWAY

Recommendations

OVERVIEW

The Rocky Fork Greenway corridor is one of the major greenway corridors in the Central Ohio area. This plan envisions a leisure trail to run from the Rocky Fork Metro Park—located just north of the city—to Three Creeks Metro Park in Obetz on the south side of Columbus. The New Albany reach of the greenway runs from Rocky Fork Metro Park to Morse Road. Currently, around 18% of this has been developed with trails in Oxford Green, Thompson Park, and in the Metro Park. Current efforts are looking at additional segments to complete the New Albany segment.

WHY THIS CORRIDOR IS IMPORTANT

The Rocky Fork Greenway is a priority corridor of the Central Ohio Greenways system and will improve regional connectivity. The greenway is also an important corridor which can be used to connect many areas of the city with Rocky Fork Metro Park.

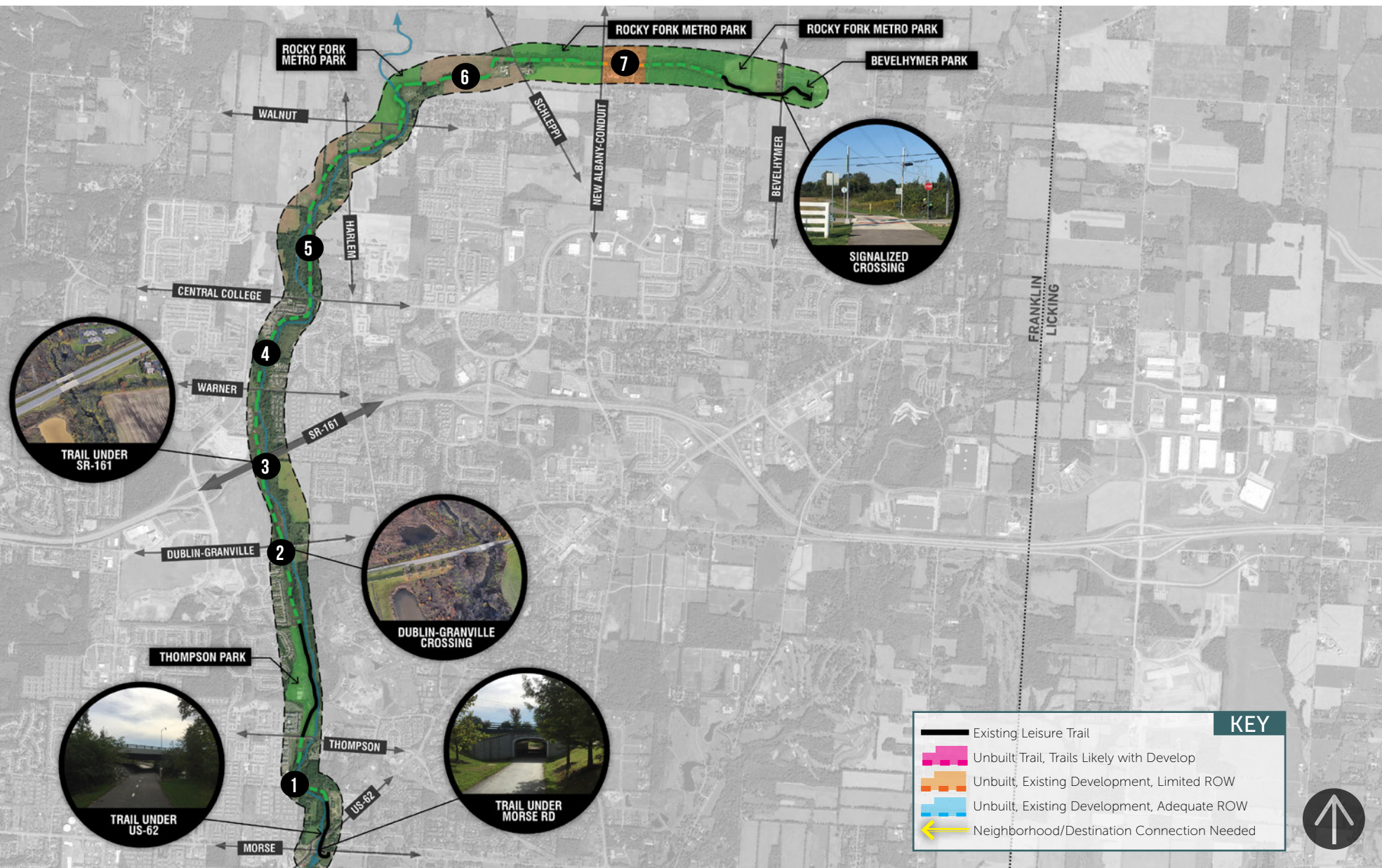
5.6 MILES
OF NEW
LEISURE TRAIL

6
NEW BRIDGES

POTENTIAL IMPROVEMENTS

Map ID	Improvement Description	Segment Length	Likely w/ Devel.	Side of Road
1	New leisure trail. Will potentially need bridge crossing at Rocky Fork.	+/-2,700'	No	n/a
2	New leisure trail. Will potentially need bridge crossing at Rocky Fork.	+/-2,500'	No	n/a
3	New leisure trail. Crossing under SR-161 required. Will potentially need bridge crossing at Rocky Fork.	+/-4,700'	No	n/a
4	New leisure trail. Will potentially need bridge crossing at Rocky Fork.	+/-3,300'	No	n/a
5	New leisure trail. Will potentially need bridge crossing at Rocky Fork.	+/-6,700'	No	n/a
6	New leisure trail. Will potentially need bridge crossing at Rocky Fork.	+/-8,200'	No	n/a
7	New leisure trail. Trail thru subdivision to connect with Rocky Fork Metro Park.	+/-1,200'	No	n/a

ROCKY FORK GREENWAY CORRIDOR IMPROVEMENTS



ROSE RUN GREENWAY

Recommendations

OVERVIEW

Since the City's original strategic plan, the Rose Run Corridor has been identified as a critical natural feature and east-west spine for the city and the Village Center. The goal is for it to serve as an environmental laboratory, outdoor living room, and community connection rather than a divide. Over the years, steady progress has been made on that vision. In the early 2000's, the original trail system and bridges were constructed and served the community well for many years. The central area between the Learning Campus and Market Square is now entering a new phase with upgrades to the trails, a new signature bridge and gathering areas between the campus and library, and an emphasis on outdoor spaces.

Dubbed "Rose Run I," this phase is now under construction and will be transformative for the Village Center. Just to the east, Bill Resch Park has been developed into an outdoor classroom and is used by the New Albany School District for environmental classes. North of Dublin-Granville Road, the headwaters of Rose Run have been restored and a pedestrian easement has been established that allows for future trail extension through this area. A master plan for the Village Hall Block was completed in 2018 and is identified as the next focus of the City to continue the development of the Rose Run corridor.

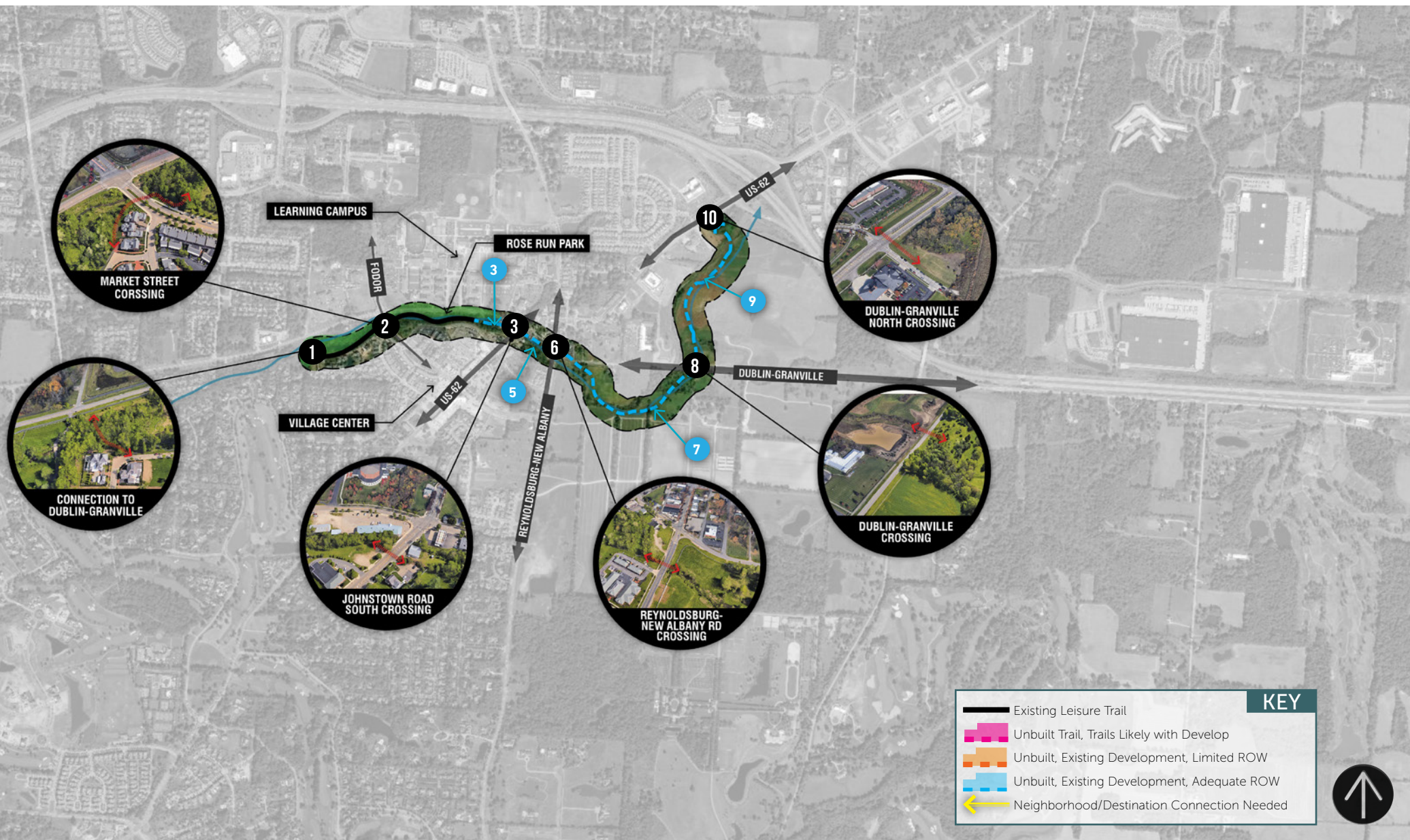
1 MILE
OF NEW
LEISURE TRAIL

5
INTERSECTION
IMPROVEMENTS

POTENTIAL IMPROVEMENTS

Map ID	Improvement Description	Segment Length	Likely w/ Devel.	Side of Road
1	Connection across Rose Run to Dublin-Granville Rd..	n/a	No	TBD
2	Improve crossing at Market St.	n/a	No	TBD
3	New leisure trail from parking lot to Johnstown Rd..	+/-500'	No	TBD
4	Johnstown Rd.. crossing south of Rose Run.	n/a	No	TBD
5	New leisure trail from Johnstown Rd.. to High St.	+/-6,00'	No	TBD
6	Crossing at High St.	n/a	No	TBD
7	New leisure trail from High St. to Dublin-Granville Rd..	+/-2,000'	Yes	TBD
8	Crossing at Dublin-Granville Rd..	n/a	No	TBD
9	New leisure trail from Dublin-Granville Rd.. to Johnstown Rd..	+/-2,400'	Yes	TBD
10	Improve crossing at Johnstown Rd.. and Theisen Rd..	n/a	No	TBD

ROSE RUN GREENWAY CORRIDOR IMPROVEMENTS



DUBLIN-GRANVILLE ROAD

Recommendations

OVERVIEW

Formerly known as SR-161, Dublin-Granville Road is a significant local and regional east-west connector and is heavily used as a cycling corridor. Being an old state route, Dublin-Granville Road is ideal for cycling because it now has fairly low vehicular traffic, but was built to state route standards with larger road berms. This creates a safe cycling environment from the Village Center to the surrounding rural areas. About 23% of this corridor is complete. The corridor west of the Village Center has been fairly developed, but a couple of key gaps in the trail exist. East of the Village Center, there has been little leisure trail development, but it is anticipated that much of this area will be privately developed. There is one area just west of Babbitt Road that has been developed without a leisure trail.

WHY THIS CORRIDOR IS IMPORTANT

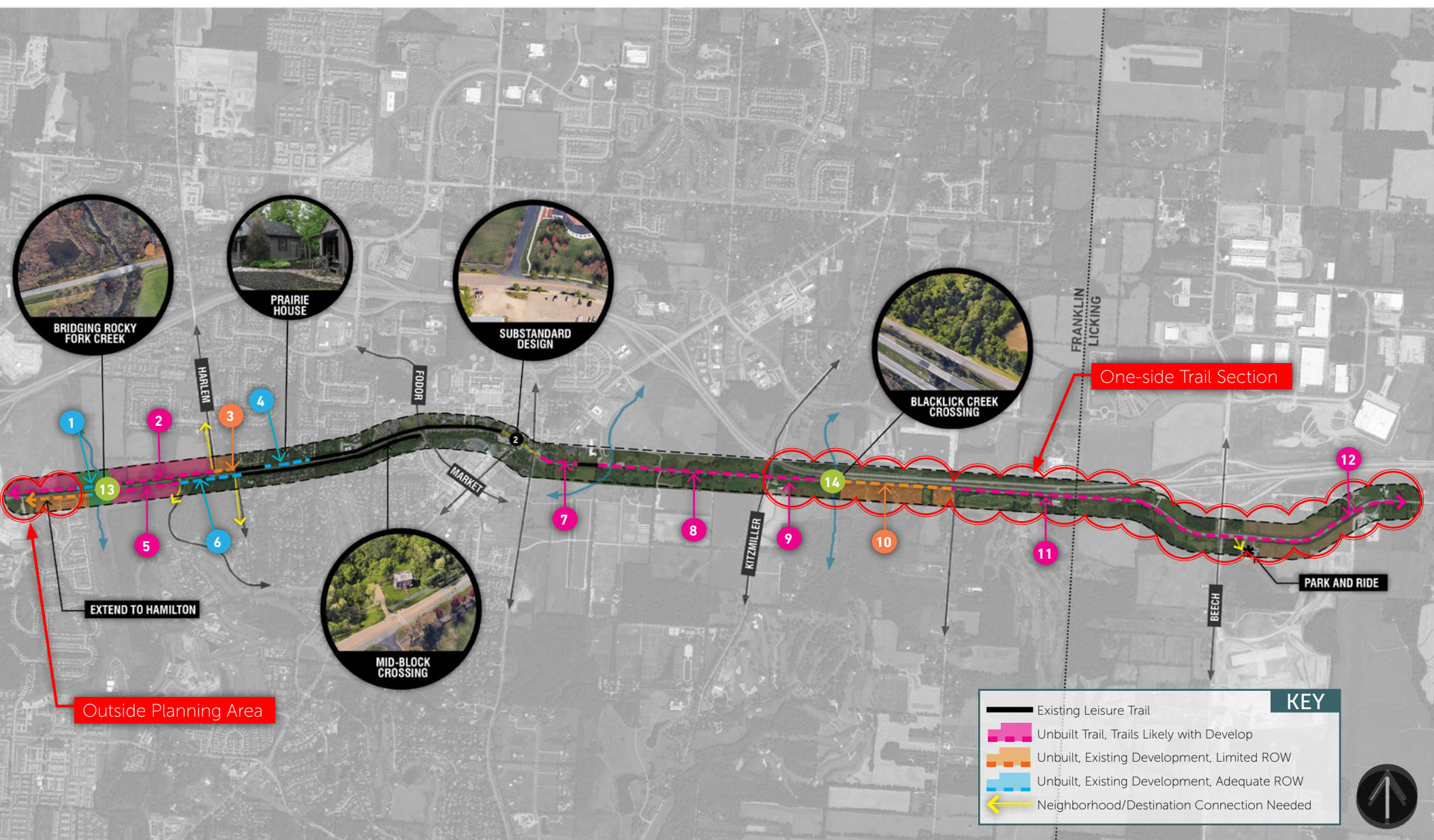
Dublin-Granville Road is a major east-west connector for the city. Roadside leisure trails will greatly increase mobility and connectivity.

4.6 MILES
OF NEW
LEISURE TRAIL

POTENTIAL IMPROVEMENTS

Map ID	Improvement Description	Segment Length	Likely w/ Devel.	Side of Road
1	New leisure trail through greenway area.	+/-700'	No	North
2	New leisure trail from Rocky Fork Creek to Harlem Rd..	+/-2,500'	Yes	North
3	New leisure trail from Harlem Rd.. to west of Brookview Rd..	+/-380'	No	North
4	New leisure trail from east of Brookview Rd.. to Mead Way.	+/-1,000'	No	North
5	New leisure trail from Rocky Fork Creek to Greensward Rd.	+/-1,700'	Yes	South
6	New leisure trail from Greensward Rd. to Harlem Rd..	+/-1,200'	No	South
7	New leisure trail from High St. to JCC.	+/-500'	Yes	South
8	New leisure trail from JCC to Kitzmiller Rd..	+/-3,900'	Yes	South
9	New leisure trail from Kitzmiller Rd.. to Blacklick Creek.	+/-1,700'	Yes	South
10	New leisure trail from Blacklick Creek to Babbit Rd..	+/-2,600'	No	South
11	New leisure trail from Babbit Rd.. to Beech Rd.	+/-6,000'	Yes	South
12	New leisure trail from Beech Rd.. to Mink St.	+/-8,500'	Yes	South
13	Bridge crossing on north and or south side of Rocky Fork Creek.	n/a	No	North & South
14	New leisure trail bridge across Blacklick Creek.	n/a	No	South

DUBLIN-GRANVILLE ROAD CORRIDOR IMPROVEMENTS



REYNOLDSBURG/NEW ALBANY ROAD

Recommendations

3 MILES
OF NEW
LEISURE TRAIL

OVERVIEW

The Reynoldsburg/New Albany Road corridor is another one of the defining rural corridors of the city. Around 65% of this corridor is developed with leisure trails. South of the Village Center, the corridor is well developed except between Brandon Road and Morse Road where development has occurred without leisure trails. North of the Village Center, New Albany-Condit Road has one of the best protected overpasses in the city, which goes over SR-161. North of Walton Parkway, there are a number of existing developments without leisure trails.

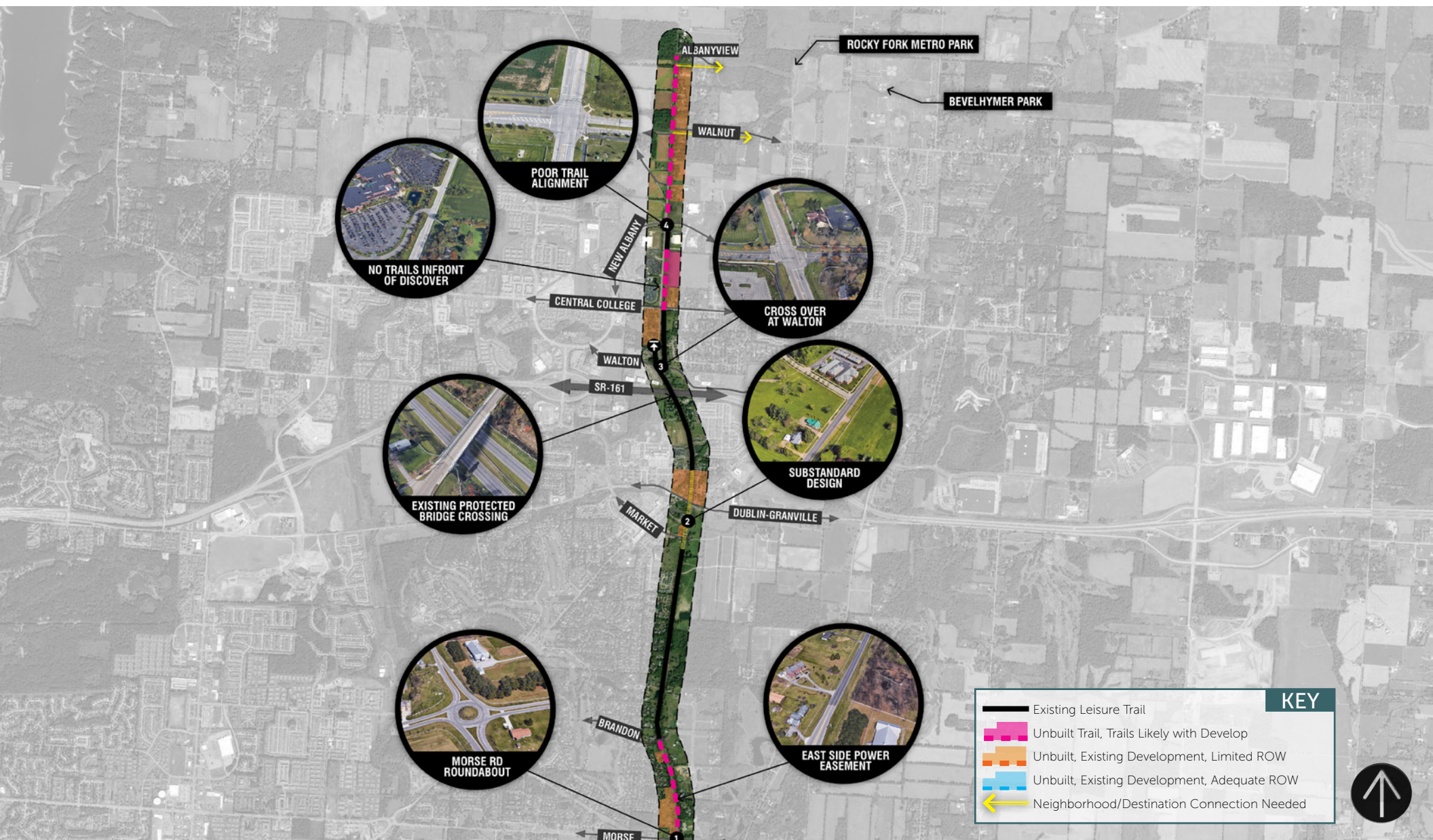
WHY THIS CORRIDOR IS IMPORTANT

The Reynoldsburg/New Albany Road is a major north-south connection for city residents. It connects neighborhoods with the Village Center and is currently only 1 of 2 designated bike/ped crossings of SR-161 in the city.

POTENTIAL IMPROVEMENTS

Map ID	Improvement Description	Segment Length	Likely w/ Devel.	Side of Road
1	Construct spur to Rocky Fork Metro Park	+/-1,100'	No	North
2	Construct spur to Rocky Fork Metro Park	+/-3,700'	No	North
3	New trail construction	+/-2,400'	No	North
4	New trail construction	+/-2,900'	No	North
5	New trail construction	+/-1,700'	No	South
6	Sidewalk should be updated to standard trail design	+/-1,000'	No	South
7	New trail construction	+/-3,000'	No	South

REYNOLDSBURG/NEW ALBANY ROAD CORRIDOR IMPROVEMENTS



BLACKLICK CREEK GREENWAY - HYBRID

Recommendations

3.4 MILES
OF NEW
LEISURE TRAIL

OVERVIEW

Blacklick Creek Greenway generally runs north to south paralleling Kitzmiller Road. With it's close proximity to Reynoldsburg/ New Albany Road, a future could be envisioned that if segments of the corridor could not be developed in the near future those segments could jump out to the Reynoldsburg-New Albany Road Trail to have a complete route. A portion of the corridor goes through the business campus and efforts should be made to gain pedestrian easements along the creek such as the Abercrombie and Fitch Corporate Campus. On the south end of the corridor is a private golf course; if this area ever would redevelop, the trail should be extended through this area.

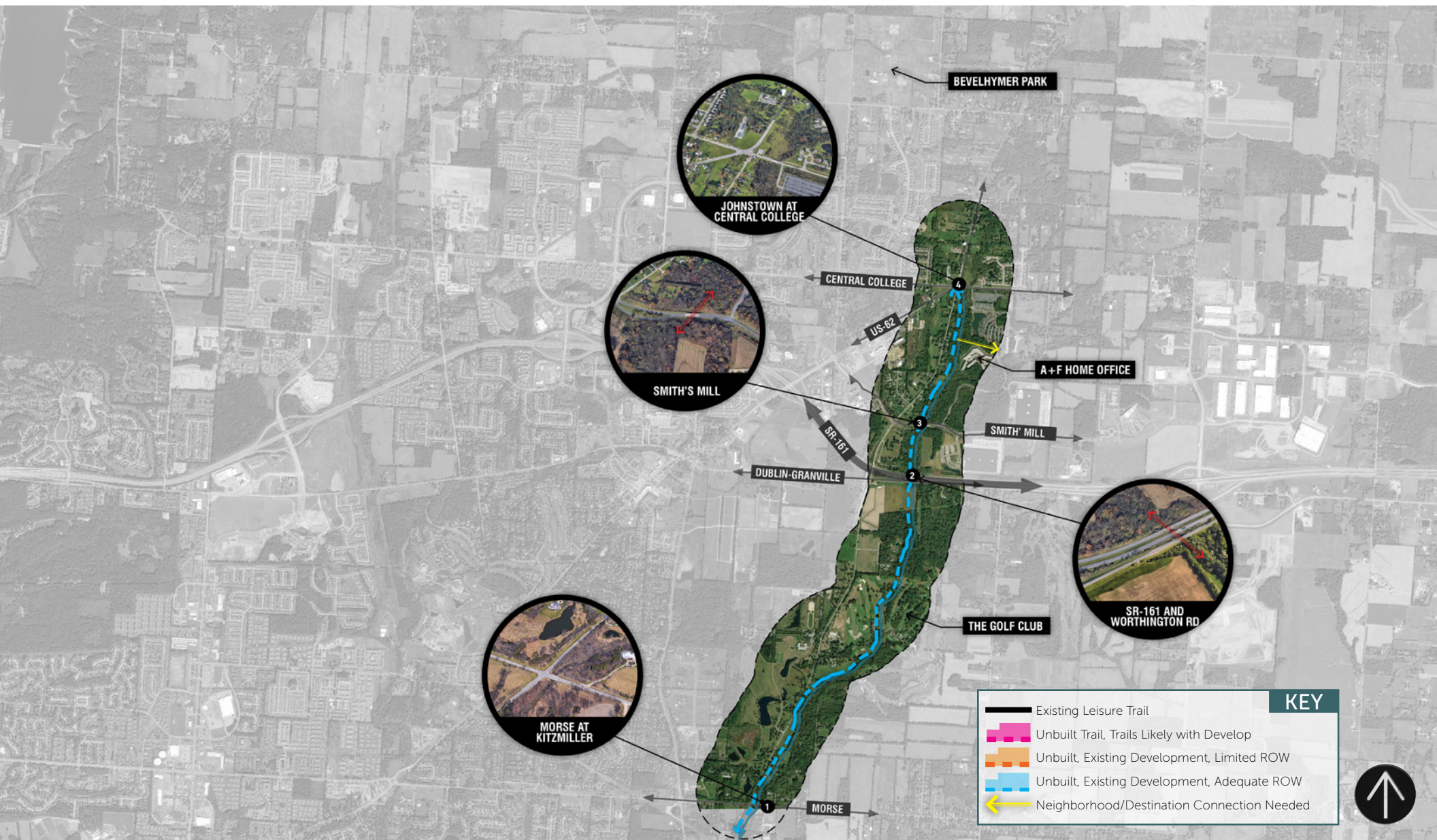
WHY THIS CORRIDOR IS IMPORTANT

The Blacklick Greenway is a priority of the Central Ohio Greenways system and will improve regional connectivity. Additionally, the corridor will provide leisure trail access to residents in rural sections of the planning area.

POTENTIAL IMPROVEMENTS

Map ID	Improvement Description	Segment Length	Likely w/ Devel.	Side of Road
1	Connection to future Morse Rd.. roadside trail.	n/a	No	n/a
2	Trail search corridor, potentially paralleling Blacklick Creek from Morse Rd. to Dublin-Granville Rd.. May encounter challenges negotiating easements with property owners. This segment also crosses the Golf Club golf course.	+/-11,700'	No	n/a
3	Potential Creekside underpass of SR-161.	n/a	No	n/a
4	10' trail segment potentially paralleling Blacklick Creek.	+/-1,600'	No	n/a
5	Potential Creekside underpass of Smith's Mill Rd..	n/a	No	n/a
6	10' trail segment potentially paralleling Blacklick Creek.	+/-4,500'	No	n/a
7	Connection of main trail route to A&F Home Office.	n/a	No	n/a
8	Trail crossing to Central College Rd.. trail on north side.	n/a	No	n/a

BLACKLICK CREEK GREENWAY CORRIDOR IMPROVEMENTS



CENTRAL COLLEGE ROAD

Recommendations

OVERVIEW

Central College provides significant east-west connectivity for neighborhoods north of SR-161 and connects western parts of the Business Park with residential neighborhoods. The leisure trail on the north side of the road is nearly completed, however, there is limited existing leisure trail on the south side.

WHY THIS CORRIDOR IS IMPORTANT

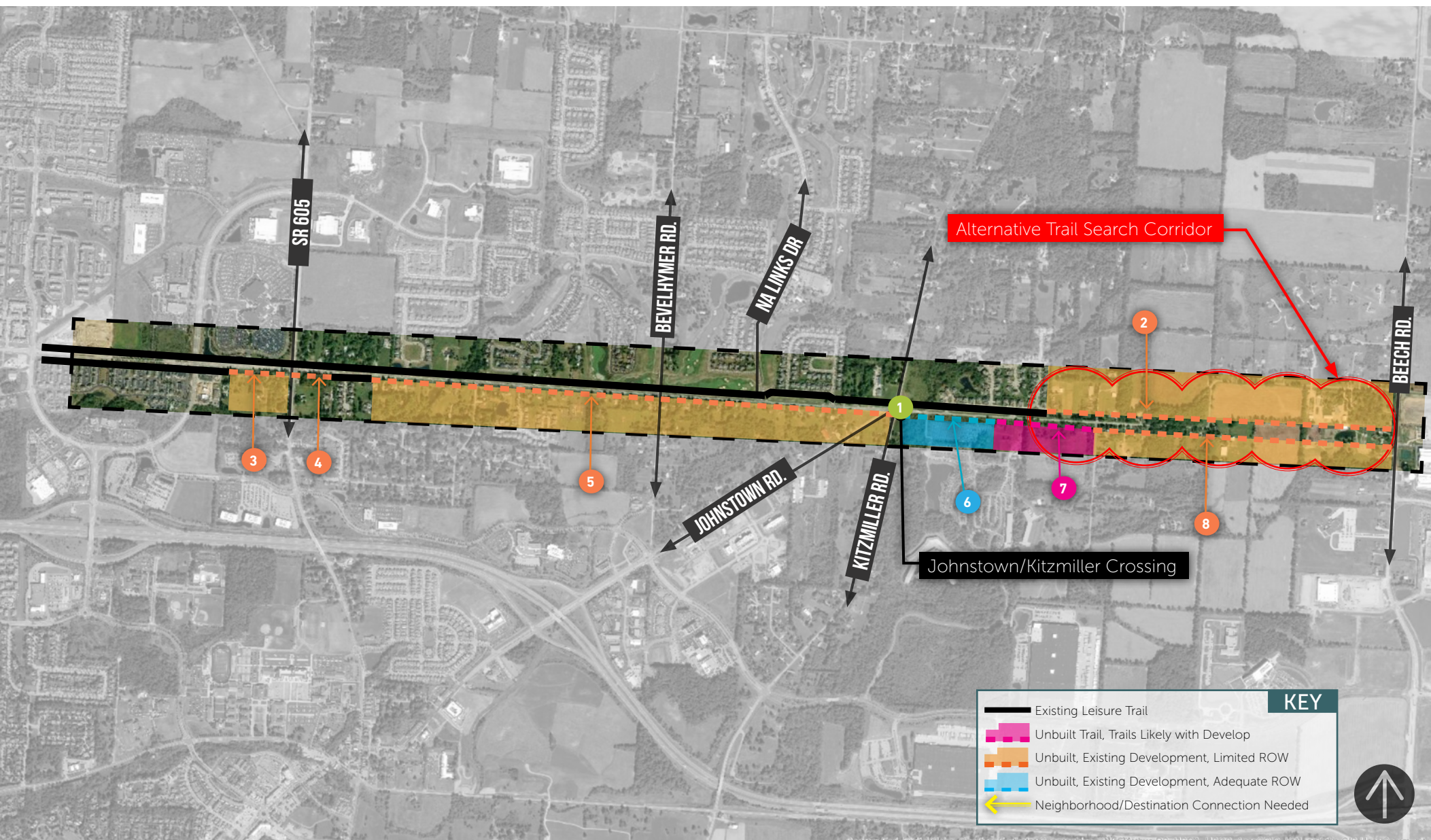
Central College provides significant east-west connectivity for neighborhoods north of SR-161.

4.1 MILES
OF NEW
LEISURE TRAIL

POTENTIAL IMPROVEMENTS

Map ID	Improvement Description	Segment Length	Likely w/ Devel.	Side of Road
1	Improve crossing at Johnstown/Kitzmill Rd.. (see Design Standards for Arterial Crossing, page 48).	n/a	No	Inters.
2	New leisure trail from Tidewater neighborhood to Beech Rd.. There may be future roadway alignments if this are redevelops.	+/-5,100'	No	n/a
3	New leisure trail from west of New Albany Rd.. to SR-605.	+/-1,000'	No	n/a
4	New leisure trail from SR-605 to west of Steele Dr.	+/-500'	No	n/a
5	New leisure trail from east of Steele Dr. to Johnstown/Kitzmill Rd..	+/-7,800'	No	n/a
6	New leisure trail from Johnstown/Kitzmill Rd.. to McClellan Dr.	+/-1,700'	No	n/a
7	New leisure trail from McClellan Dr. to Evans Rd..	+/-1,200'	Yes	n/a
8	New leisure trail from Evans Rd. to Beech Rd.. There may be future roadway alignments if this are redevelops.	+/-4,400'	No	n/a

CENTRAL COLLEGE RD. CORRIDOR IMPROVEMENTS



SECTION 5: **IMPLEMENTATION**

IMPLEMENTATION

Funding & Performances Metric

This section to elaborate on funding strategies which may be used to aid in implementing the plan's recommendations.

This section to describe performance metric to tract the implementation of the plan's recommendations.

FUNDING STRATEGIES

1. Continue to pursue grant opportunities for new trail construction.
2. Explore the feasibility of selling naming rights to the system components to fund future trail expansion and/or amenities.
3. New development must construct leisure trails.
4. New development must provide trail or sidewalk connections to the leisure trail from their site when adjacent or across the street from an existing or planned leisure trail.
5. Cost of construction of leisure trails should be updated annually for the City to help guide decision making for future trail construction and any fee-in-lieu of situations.

PERFORMANCE METRIC

1. Total system miles.
2. The completion status of each of the priority routes.
3. Number of trail system users.
4. Change in rate of bicycle and pedestrian related crashes.
5. How often community members use system.
6. Trail amenities installed.
7. Entry into the League of American Bicyclists
8. Community Report Card by League of American Bicyclists
9. Completion of wayfinding and signage master plan.