



New Albany Planning Commission Meeting Agenda
Monday, May 5, 2025 at 7:00 p.m.

Members of the public must attend the meeting in-person to participate and provide comments at New Albany Village Hall at 99 West Main Street. The meeting will be streamed for viewing purposes only via the city website at <https://newalbanyohio.org/answers/streaming-meetings/>

I. Call to order

II. Roll call

III. Action on minutes: April 21, 2025

IV. Additions or corrections to the agenda

Administration of oath to all witnesses/applicants/staff who plan to speak regarding an application on tonight's agenda. "Do you swear to tell the truth and nothing but the truth."

V. Hearing of visitors for items not on tonight's agenda

VI. Cases:

FDP-05-2025 AEP Substation

Final development plan to allow for construction of an AEP electrical substation on 9.50 acres located at 7375 Souder Road (PID: 222-002282).

Applicant: AEP Ohio Transmission Company Inc.

Motion of acceptance of staff reports and related documents into the record for FDP-05-2025.

Motion of approval for application FDP-05-2025 based on the findings in the staff report with the conditions listed in the staff report, subject to staff approval.

VII. Other business

1. Engage New Albany Strategic Plan Update: US-62 Interchange Focus Area

VIII. Poll members for comment

IX. Adjournment



New Albany Planning Commission
Monday, April 21, 2025 Meeting Minutes - DRAFT

I. Call to order

The New Albany Planning Commission held a regular meeting on Monday, April 21, 2025 in the New Albany Village Hall. Chair Kirby called the meeting to order at 7:01 p.m. and asked to hear the roll.

II. Roll call

Mr. Kirby	present
Mr. Wallace	absent
Mr. Schell	present
Ms. Briggs	present
Mr. Larsen	present
Council Member Wiltrout	absent; arrived at 7:15 p.m.

Having four voting members present, the commission had a quorum to transact business.

Staff members present: Law Director Albrecht, Community Development Engineer Albright, Planner Bennett, Planner Blackburn, Planner Henderson, Planning Manager Christian, Planner II Saumenig, Deputy Clerk Madriguera.

III. Action on minutes: April 7, 2025

Chair Kirby stated that in the fourth paragraph on page three, in a comment attributed to him, the word “bedroom,” should be “room.”

Deputy Clerk Madriguera noted the change.

Chair Kirby asked whether there were any other corrections.

Hearing none, Commissioner Schell moved for approval of the April 7, 2025 meeting minutes as corrected. Commissioner Briggs seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Mr. Schell yes, Ms. Briggs yes, Mr. Kirby yes, Mr. Larsen abstained from the vote because he was not present. Having three yes votes, the motion passed and the April 7, 2025 meeting minutes were approved as corrected.

IV. Additions or corrections to the agenda

Chair Kirby asked whether there were any additions or corrections to the agenda.

Planner II Saumenig answered that the applicants in the Graeter’s application would like to add an informal discussion of the drive through under other business.

Chair Kirby administered the oath to all present who would be addressing the commission and reminded everyone that now would be a good time to silence all cell phones.

V. Hearing of visitors for items not on tonight's agenda

Chair Kirby asked if there were any visitors present who wished to address the commission on an item not on the agenda. Hearing none, he introduced the first case and asked to hear the staff report.

VI. Cases:

FDP-05-2025 AEP Substation

Final development plan to allow for construction of an AEP electrical substation on 9.50 acres located at 7375 Souder Road (PID: 222-002282).

Applicant: AEP Ohio Transmission Company Inc.

Planner II Saumenig stated that after review, the city was recommending that this item should be tabled again.

Law Director Albrecht further explained that the city has requested additional information from the applicants, and that the application should be ready for consideration on May 5th.

Chair Kirby asked if there was anyone present from the public who wished to comment on the application. Hearing none he moved to table FDP-05-2025 until the May 5, 2025 meeting. Commissioner Briggs seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Mr. Kirby yes, Ms. Briggs yes, Mr. Larsen yes, Mr. Schell yes. Having four yes votes, the motion passed and FDP-05-2025 was laid upon the table until the May 5, 2025 meeting.

FDM-19-2025 Speedway FDP Amendment

Modification to the approved final development plan to allow 5 additional parking spaces for an approved Speedway gas station located at the northwest corner of Mink Street and Innovation Campus Way (PID: 093-107478-00.002).

Applicant: J. Carter Bean Architect LLC

Planner II Saumenig delivered the staff report.

Chair Kirby asked whether there were comments from engineering.

Development Engineer Albright stated there were no comments from engineering.

Chair Kirby asked to hear from the applicant.

Applicant J. Carter Bean, Architect for the project apologized for the amendment. He explained that a convenience store of this size required additional parking, and as a result, Speedway was requesting five additional parking spaces.

Commissioner Schell asked whether this was typical for a convenience store of this size.

Mr. Bean responded that it is typical for a convenience store of this size.

Chair Kirby asked whether there was anyone present from the public who wished to comment on the application.

Hearing none, Chair Kirby moved to admit the staff reports and related documents into the record for FDM-19-2025. Commissioner Schell seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear to hear the roll.

Upon roll call: Mr. Kirby yes, Mr. Schell yes, Ms. Briggs yes, Mr. Larsen yes. Having four yes votes, the motion passed and the staff reports and related documents were admitted to the record for FDM-19-2025.

Commissioner Larsen moved for approval of FDM-19-2025 based on the findings in the staff report with the conditions listed in the staff report, subject to staff approval. Commissioner Briggs seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Mr. Larsen yes, Ms. Briggs yes, Mr. Kirby yes, Mr. Schell yes. Having four yes votes, the motion passed and the modification was approved.

The commission wished the applicant good luck.

Chair Kirby introduced the next case and asked to hear the staff report.

FDM-21-2025 Courtyards at Haines Creek

Modification to the approved final development plan for review and approval of a private clubhouse on 1.5 acres located at the northwest corner of Central College Road and Jug Street Rd NW (PID: 222-005497).

Applicant: EC New Vision Ohio LLC

Planner Henderson delivered the staff report.

Chair Kirby asked whether there were comments from engineering.

Community Development Engineer Albright said there were no engineering comments.

Chair Kirby asked to hear from the applicant.

Applicant Lisa Minklei of Epcon Communities, spoke in support of the application. She explained that since the commission's approval of the final development plan, the architecture of the clubhouse has been completed.

Chair Kirby asked whether she had any conflict with the conditions.

Ms. Minklei responded that there was no conflict.

Chair Kirby asked whether anyone from the public wished to comment on the application.

Hearing none, Chair Kirby moved to admit the staff reports and related documents into the record for FDM-21-2025. Commissioner Schell seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none he asked to hear the roll.

Upon roll call: Mr. Kirby yes, Mr. Schell yes, Ms. Briggs yes, Mr. Larsen yes. Having four yes votes, the motion passed and the staff reports and related documents were admitted to the record for FDM-21-2025.

Commissioner Larsen moved to approve FDM-19-2025 based on the findings in the staff report with the conditions in the staff report, subject to staff approval. Commissioner Briggs seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Mr. Larsen yes, Ms. Briggs yes, Mr. Kirby yes, Mr. Schell yes. Having four yes votes, the motion passed and the modification to the clubhouse was approved.

The commission wished Ms. Minklei good luck.

Chair Kirby introduced the next case and asked to hear from staff.

FDP-20-2025 Canini Medical Office Building

Final development plan to allow for the construction of two medical office buildings on 2.06 acres located on Forest Drive (PID: 222-004888).

Applicant: Canini & Associates LLC

Planner Blackburn delivered the staff reports for FDP-20-2025 and VAR-24-2025 in a single presentation. She explained that staff was recommending the following modification to condition 3 of the final development plan: A cross-access easement must be established along the entire front drive aisle to allow future connection onto parcel 222-004353 in order to meet the requirement from zoning text section 8b.02(5).

Chair Kirby asked whether there were any comments from engineering.

Community Development Engineer Albright delivered the engineering report.

Chair Kirby asked whether the commission had questions for staff. Hearing none, Chair Kirby asked to hear from the applicant.

Applicant Larry Canini spoke in support of the application. He explained that the existing cross-access easement must be adhered to, but in light of that the building must be flipped. Mr. Canini requested that the commission vote on the application as presented. Following that, his team will flip the building subject to staff approval. After flipping the building there will be shared parking with the building to the south. He apologized for the mix-up and stated that the existing language was missed in the application process.

Chair Kirby asked whether that was amenable to staff.

Planning Manager Christian said yes, staff is comfortable working with the applicant to flip the building to accommodate the existing cross-access easement between Smith's Mill Ventures and the Estate.

Chair Kirby confirmed that the north south street is a private drive.

Commissioner Larsen remarked that it appeared that if the two buildings are flipped, a public road might be more feasible

Mr. Canini replied that he did not think so.

Chair Kirby asked whether the curb cuts would change. He further noted that there was a typographical error in the engineering report, “site” lines should read “sight” lines.

Mr. Canini replied that the curb cuts will not change.

Commissioner Larsen asked what, if any, impact there will be to traffic.

Community Development Engineer Albright answered that there will not be impact to traffic.

Chair Kirby reviewed the following additional conditions:

9. Flip the buildings to get the parking correct with the neighbor to the south, subject to staff approval.

Accept the modification to the third condition.

Fix the typo in the Engineering Memo (site should be sight).

Chair Kirby moved to accept the staff reports and related documents into the record for FDP-20-2025. Commissioner Briggs seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Mr. Kirby yes, Ms. Briggs yes, Mr. Larsen yes, Mr. Schell yes. Having four yes votes, the motion passed and the staff reports and related documents were accepted into the record for FDP-20-2025.

Chair Kirby moved to approve FDP-20-2025 based on the findings in the staff report with the conditions listed in the staff report and the following modifications and additional condition:

9. Flip the buildings to get the parking correct with the neighbor to the south, subject to staff approval.

Accept the following modification to the third condition: A cross-access easement must be established along the entire front drive aisle to allow future connection onto parcel 222-004353 in order to meet the requirement from zoning text section 8b.02(5).

Fix the typo in the Engineering Memo (site should be sight).

Commissioner Schell seconded the motion. Chair Kirby asked whether there was any discussion on the motion. Hearing none he asked to hear the roll.

Upon roll call: Mr. Kirby yes, Mr. Schell yes, Ms. Briggs yes, Mr. Larsen yes. Having four yes votes, the motion passed and FDP-20-2025 was approved subject to the conditions in the staff report and as stated above.

VAR-24-2025 Canini Lot Split

Variance to codified ordinance 1153.04(b) to allow the creation of a lot that does not front on a public or private street located on Forest Drive (PID: 222-004888).

Applicant: Canini & Associates LLC

Chair Kirby asked whether there were any additional questions about VAR-24-2025 or whether there was anyone from the public present who wished to comment on the application.

Hearing none he moved to accept the staff reports and related documents into the record for VAR-24-2025. Commissioner Briggs seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none he asked to hear the roll.

Upon roll call: Mr. Kirby yes, Ms. Briggs yes, Mr. Schell yes, Mr. Larsen yes. Having four yes votes, the motion passed and the staff reports and related documents were accepted in the record for VAR-24-2025.

Commissioner Briggs moved for approval of VAR-24-2025 based on the findings in the staff report with the conditions listed in the staff report, subject to staff approval. Commissioner Schell seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Ms. Briggs yes, Mr. Schell yes, Mr. Kirby yes, Mr. Larsen yes. Having four yes votes, the motion passed and VAR-24-2025 was approved subject to the conditions in the staff report.

Chair Kirby and the commission wished the applicants good luck. Thereafter, Chair Kirby introduced the final case and asked to hear the staff report.

FPL-23-2025 Final Plat

Final Plat for the dedication of public right-of-way for Ganton Parkway West Phase 3B located west of Beech Road, south of Worthington Road, and east of Babbitt Road.

Applicant: City of New Albany

Planner Bennett delivered the staff report.

Chair Kirby asked whether there were any comments from engineering.

Development Engineer Albright responded that there were no additional comments from engineering.

Chair Kirby asked whether the large squared off area was for a roundabout. He further asked whether more than one roundabout was planned.

Planner Bennett answered yes, the squared off area was for a roundabout and that a single roundabout was planned.

Chair Kirby asked for additional questions from the commission. Hearing none, he opened the public hearing.

Ronald Farber, 5596 Babbitt Road, asked whether there was a traffic study. He was also curious about the setbacks. He stated that his main concerns are the car counts, the traffic study, future development plans, the timing of the survey, and the timing of future development of Babbitt Road.

Planning Manager Christian responded that the city has a road maintenance agreement for Babbitt Road, and pursuant to that agreement, the city will maintain the road. Regarding the traffic counts, Engineer Albright stated that he would check and get back to Mr. Farber. The Public

Service Department will be managing the development of Babbitt Road. Deputy Director Steve Mayer will have more information about the timing and the development plans. The planning meeting will be held in council chambers. Regarding the setbacks, the pavement setback is 90 feet, the building setback is 100 feet. Both are measured from the center line.

Planning Manager Christian said the strategic plan called for Babbitt Road to be made into a three-lane section, as indicated by the teal on the site plan.

Cynthia Coleman, 6145 Babbitt Road, asked whether pictures are available. She also asked whether there are plans for widening the road, and whether a roundabout (or more than one) will be installed, and whether the trees that are not currently marked for removal will be removed.

Planning Manager Christian answered that he will send pictures, and that Deputy Director of Public Service Mayer will be the best resource to answer questions about the changes to the road. He clarified that this application only involved the small section of the road (indicated in teal) on the site plan.

Development Engineer Albright added that if a tree is not currently marked, it is not likely that it will be removed. Work hours would most likely be from 7:30 – 3:30.

David McLain, 5496 Babbitt Road, said that there is not enough right-of-way, particularly to the south. He stated that he is most interested in knowing the timelines, and that most of his questions had been answered. He clarified that there would only be one roundabout, and asked whether the utilities would need to be relocated.

Chair Kirby pointed out that Bevelhymer is quite narrow and needs to be widened.

Development Engineer Albright stated he was not sure at this point. Those discussions and surveys (regarding moving the utilities) are still underway.

Planning Manager Christian said that staff will share the information as soon as they have it.

Tamara Gannon, attending on behalf of her parents who live on Babbitt Road. She clarified the extent of the three lanes. She said that she had heard rumors about the traffic and the number of roundabouts.

Justin Parker, 5085 Babbitt Road. Mr. Parker remarked that it seemed clear that neighbors are starting to get nervous. They are worried about property values going down. He asked for the timelines and asked whether the Babbitt Road residents are going to be required to connect to sewer and water.

Chair Kirby responded that residents of Plain Township will not be required to connect to water and sewer. Plain Township residents are not bound by the City of New Albany's agreement with the City of Columbus.

Planning Manager Christian added that it is up to individual residents whether they want to sell and connect. He also remarked that the strategic plan shows commercial development on that side of the road.

Mr. Parker asked whether there is a point person they should meet with.

Planning Manager Christian responded that staff would collect contact information and connect the residents with Deputy Director of Public Service Mayer.

Kim Doran, 5481 Babbitt Road. Ms. Doran explained that she owns a wedding venue on Babbitt Road. She recalled that during prior construction Babbitt Road was shut down. She asked whether the residents of Babbitt Road will get notice of a shut down this time.

Planning Manager Christian answered yes, everyone will get notice.

Ms. Doran asked whether Ganton Parkway will bring in massive traffic and whether it will be truck traffic, or will the traffic will be pushed on to old 161? She also referenced QTS and the new data centers

Planning Manager Christian explained that the heavy traffic will not be on Babbit Road. Ganton Parkway will provide alternative access.

Michelle Doran, Babbitt Road. Ms. Doran asked whether a leisure path will be installed and whether it would stop at her property.

Planning Manager Christian answered yes to both questions. A leisure path will be installed and yes, it will stop at her property because the boundary of her property is the centerline of the right of way.

Chair Kirby added that the path will end at the city boundary.

Planning Manager Christian agreed and further explained that Google was required to give the city the right of way, but private owners are not required to give their property if it extends into the right of way.

Mr. Parker asked whether a traffic light (or something other than a stop sign) will be installed.

Development Engineer Albright responded that it is not clear now, but it seemed like it was only a stop condition now.

Ms. Coleman approached the lectern again and asked for the current zoning classification for 6145 Babbitt Road.

Planning Manager Christian answered that the current zoning is residential and he clarified that the property owner requests rezoning. He noted that pursuant to the Strategic Plan, the future use of the property is residential.

Ms. Coleman asked how wide the road will be from curb to curb.

Development Engineer Albright answered that he would confirm but it was typically about 50 feet. It is 100 feet for Ganton and 80 feet for Babbitt.

Emily Doran, 5771 Babbitt Road. She asked where the first creek is. She further remarked that the residents were initially told that Babbitt Road was not going to change and now the residents are being told it will be expanded to three lanes. She requested that the residents be kept up to date.

Planning Manager Christian verified the location of the creek.

Ms. Doran (Kim) approached the lectern and asked whether old 161 is going away?

Planning Manager Christian responded that old 161 will still exist. He further explained that the creek being referenced is just south of her property. It is right in front of QTS. He noted that Planner Henderson will collect all the information and make sure it is given to the Deputy

Director of Public Service Mayer. He further explained that he understood this was frustrating but Deputy Director Mayer will provide the timelines and that there will be a separate meeting to discuss this development.

Una Cho Hunter, 5556 and 5251 Babbitt Road, they are her rental properties. She asked what will be happening in the future.

Planning Manager Christian explained that Ms. Hunter's properties were significantly further south and would not be affected by this application. He further clarified that it was just the teal section of road on the diagram. The reason everyone was notified was because during the informal presentation the Community Development Department agreed to notify everyone on Babbitt Road.

Ms. Doran (Michelle) confirmed with Planning Manager Christian that the east side of Babbitt Road was planned as for commercial for future development.

Hearing no further questions from the public or from the commission, Chair Kirby moved to admit the staff reports and related documents into the record for FPL-23-2025. Commissioner Larsen seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Mr. Kirby yes, Mr. Larsen yes, Ms. Briggs yes, Mr. Schell yes. Having four yes votes, the motion passed and the staff reports and related documents were admitted to the record for FPL-23-2025.

Commissioner Schell moved for approval of FPL-23-2025 with the findings in the staff report with the conditions in the staff report, subject to staff approval. Chair Kirby seconded the motion.

Chair Kirby asked if there was any discussion on the motion. Hearing none he asked to hear the roll.

Upon roll call: Mr. Schell yes, Ms. Briggs, Mr. Kirby yes, Mr. Larsen yes. Having four yes votes, the motion passed and FPL-23-2025 was favorably recommended to the city council.

At 8:23 Chair Kirby called a 10-minute recess.

At 8:32 Chair Kirby called the meeting to order.

Immediately following the recess Development Engineer Albright confirmed that it was 34-feet for Babbitt Road.

Thereafter, Chair Kirby introduced the first item of Other business, the Graeter's Drive Through informal presentation.

VII. Other business

1. Graeter's Drive through – informal presentation.

Applicants Larry Canini and J. Carter Bean spoke with commission about issues surrounding the creation of the drive through. They explained that the drive through is split in two pieces. There was discussion about access, car stacking, and the mix of tenants in that area. Mr. Canini

explained that the ice cream store would work well with existing medical office tenants because the ice cream store will primarily be busy between the hours of 3:00 p.m. and 10:00 p.m. He further explained that it was originally conceived as pick-up only location, but then it was recognized that ice cream is different than other foods and it needs to be seen. The commission discussed access from Woodcrest, and noted that striping would likely help. Mr. Canini and Mr. Bean thanked the commission and said they looked forward to presenting another concept at an upcoming meeting.

2. FDP Major/Minor Modifications Workshop

Planner Saumenig delivered the staff report. She explained that staff was beginning the process and were seeking feedback from the commission. Under the current code, any change to a final development plan must go back before the commission. In an effort to minimize modification applications, staff is researching other communities to see if there are examples of minor modifications that are subject to administrative approval. In Dublin a memo is sent to their planning commission for modifications that have been approved by staff.

Chair Kirby remarked that the numbers would dictate how to proceed. He asked whether staff had counts. If the counts are small, then there is no need to bother.

Commissioner Briggs asked whether staff would email the data to the commission.

Planner Saumenig said, yes of course. She continued that, in her opinion, the Speedway Modification application on tonight's agenda (asking for additional 5 parking spaces) would be minor.

Chair Kirby said that running the numbers would be helpful. He asked what the typical turnaround time is.

Planning Manager Christian responded that the typical turnaround time was days and sometimes hours.

Council Member Wiltout remarked that the devil is in the details of these modifications. Swimming pools and fences should still come through pc, as should any variances.

Everyone agreed.

Planner Saumenig displayed an ARB example of major v. minor modifications. She thanked the commission and said that staff would return with research and recommendations.

The commission thanked staff.

Chair Kirby then called the annual organizational meeting to order.

3. Annual Organizational Meeting

- Elect Chairperson
 - Commissioner Schell nominated Neil Kirby to be the chairperson of the New Albany Planning Commission. Commissioner Briggs seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Mr. Schell yes, Ms. Briggs yes, Mr. Kirby yes, Mr. Larsen yes. Having four yes votes, Neil Kirby was re-elected chairperson of the New Albany Planning Commission for 2025.

- Elect Vice-Chairperson
 - Chair Kirby nominated David Wallace to be vice-chairperson of the New Albany Planning Commission. Commissioner Schell seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Mr. Schell yes, Ms. Briggs yes, Mr. Kirby yes, Mr. Larsen yes. Having four yes votes, David Wallace was re-elected vice-chairperson of the New Albany Planning Commission for 2025.

- Elect Secretary
 - Commissioner Briggs nominated Hans Schell to be secretary of the New Albany Planning Commission. Commissioner Larsen seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Mr. Schell yes, Ms. Briggs yes, Mr. Kirby yes, Mr. Larsen yes. Having four yes votes, Hans Schell was re-elected secretary of the New Albany Planning Commission for 2025.

- Elect Planning Commission liaison to the New Albany Board of Zoning Appeals.
 - Chair Kirby nominated Hans Schell to be the commission liaison to the New Albany Board of Zoning Appeals. Commissioner Briggs seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Mr. Schell yes, Ms. Briggs yes, Mr. Kirby yes, Mr. Larsen yes. Having four yes votes, Hans Schell was re-elected to be the commission's liaison to the New Albany Planning Board of Zoning Appeals for 2025.

- Establish date, time, and location for 2025 informal and regular meetings
 - Chair Kirby moved that the standing meeting times/days/and locations continue for 2025. That the commission would meet informally on the first Monday of the month at 7:00 p.m., if needed; that the commission would meet regularly on the third Monday of the month at 7:00 p.m., if needed; and that all meetings would take place in the New Albany Village Hall. Commissioner Larsen seconded the motion.

Chair Kirby asked whether there was any discussion on the motion. Hearing none, he asked to hear the roll.

Upon roll call: Mr. Schell yes, Ms. Briggs yes, Mr. Kirby yes, Mr. Larsen yes. Having four yes votes, the time, date, and location of the New Albany Planning Commission meetings for 2025 was established.

*Attendance is defined as in-person presence during the hearing and consideration of applications without a conflict of interest before that commission/board at that meeting. Attendance of all currently serving members of the commission/board is encouraged, and three (3) consecutive absences by any member or four (4) absences in any 12-month period shall be considered grounds for the forfeiture of the membership to the commission/board. The forfeiture would occur unless the commission/board member's absence is "excused," as determined in the sole discretion of City Council, due to an illness, injury, or other emergency circumstance of the member, or an immediate family member. Upon the occurrence of the absence that creates the grounds for forfeiture, the applicable department designee and/or a council member will then notify the clerk of council who will inform the full council that action concerning the appointment is required. C.O. 159.02(d).

VIII. Poll members for comment

Chair Kirby polled the members for comment. All of the commissioners wished each other a pleasant evening.

IX. Adjournment

Having completed the April 21, 2025 meeting agenda and having no further business, Chair Kirby adjourned the meeting without objection at 9:15 p.m.

Submitted by Deputy Clerk Madriguera, Esq.

Appendix

FDP-05-2025

Staff Report

Record of Action – to be added

FDM-19-2025

Staff Report

Record of Action

FDM-21-2025

Staff Report

Record of Action

FDP-20-2025

Staff Report

Record of Action

VAR-24-2025

Staff Report

Record of Action

FPL-23-2025

Staff Report

Record of Action

**Planning Commission Staff Report
April 21, 2025 Meeting**

**AEP SUBSTATION
FINAL DEVELOPMENT PLAN**

LOCATION: 7375 Souder Road (PID: 222-004891)
APPLICANT: AEP, Ohio Transmission Company, Inc. c/o Amy Toohey
REQUEST: Final Development Plan
ZONING: Souder East Office, Research, & Information District I-PUD
STRATEGIC PLAN: Employment Center
APPLICATION: FDP-05-2025

Review based on: Application materials received January 23, 2025

Staff report prepared by Sierra Saumenig, Planner

I. REQUEST

City staff requests that this application be tabled to the May 5, 2025 or May 19, 2025, Planning Commission meeting.

II. ACTION

Move to table final development plan application FDP-05-2025 to the May 5, 2025 or May 19, 2025, Planning Commission meeting.

Approximate Site Location:



Source: NearMap



**Planning Commission Staff Report
April 21, 2025 Meeting**

**SPEEDWAY
FINAL DEVELOPMENT PLAN MODIFICATION**

LOCATION: Generally located at the northwest intersection of Mink Street and Innovation Campus Way (PID: 093-107478-00.002)
APPLICANT: J. Carter Bean Architect LLC c/o Carter Bean
REQUEST: Final Development Plan
ZONING: Mink Interchange I-PUD
STRATEGIC PLAN: Retail/Employment Center
APPLICATION: FDM-19-2025

Review based on: Application materials received March 13, 2025

Staff report prepared by Sierra Saumenig, Planner II

I. REQUEST AND BACKGROUND

The final development plan application was approved at the March 3, 2025 Planning Commission meeting. The applicant has submitted a final development plan modification application to be heard by the Planning Commission.

The proposed changes include:

1. ☐ The addition of five parking spaces increasing from 36 spaces to 41 spaces.
2. ☐ A minimal increase of lot coverage from 42.7% to 43.6%

II. SITE DESCRIPTION & USE

The site is generally located north of Innovation Campus Way and west of Mink Street. It is bordered by commercial properties to the north (New Albany 525), west (AmplifyBio), and south (Axium Building 6), with residential areas in Jersey Township to the east.

III. EVALUATION

Staff's review is based on New Albany plans and studies, zoning text, and zoning regulations. Primary concerns and issues have been indicated below, with needed action or recommended action in underlined text. Planning Commission's review authority is found under Chapter 1159.

The Commission should consider, at a minimum, the following (per Section 1159.08):

- a. *That the proposed development is consistent in all respects with the purpose, intent and applicable standards of the Zoning Code;*
- b. *That the proposed development is in general conformity with the Strategic Plan/Rocky Fork-Blacklick Accord or portion thereof as it may apply;*
- c. *That the proposed development advances the general welfare of the Municipality;*
- d. *That the benefits, improved arrangement and design of the proposed development justify the deviation from standard development requirements included in the Zoning Ordinance;*
- e. *Various types of land or building proposed in the project;*

- f. *Where applicable, the relationship of buildings and structures to each other and to such other facilities as are appropriate with regard to land area; proposed density may not violate any contractual agreement contained in any utility contract then in effect;*
- g. *Traffic and circulation systems within the proposed project as well as its appropriateness to existing facilities in the surrounding area;*
- h. *Building heights of all structures with regard to their visual impact on adjacent facilities;*
- i. *Front, side and rear yard definitions and uses where they occur at the development periphery;*
- j. *Gross commercial building area;*
- k. *Area ratios and designation of the land surfaces to which they apply;*
- l. *Spaces between buildings and open areas;*
- m. *Width of streets in the project;*
- n. *Setbacks from streets;*
- o. *Off-street parking and loading standards;*
- p. *The order in which development will likely proceed in complex, multi-use, multi- phase developments;*
- q. *The potential impact of the proposed plan on the student population of the local school district(s);*
- r. *The Ohio Environmental Protection Agency's 401 permit, and/or isolated wetland permit (if required);*
- s. ☐ *The U.S. Army Corps of Engineers 404 permit, or nationwide permit (if required).*

It is also important to evaluate the PUD portion based on the purpose and intent. Per Section 1159.02, PUD's are intended to:

- a. *Ensure that future growth and development occurs in general accordance with the Strategic Plan;*
- b. *Minimize adverse impacts of development on the environment by preserving native vegetation, wetlands and protected animal species to the greatest extent possible*
- c. *Increase and promote the use of pedestrian paths, bicycle routes and other non-vehicular modes of transportation;*
- d. *Result in a desirable environment with more amenities than would be possible through the strict application of the minimum commitment to standards of a standard zoning district;*
- e. *Provide for an efficient use of land, and public resources, resulting in co-location of harmonious uses to share facilities and services and a logical network of utilities and streets, thereby lowering public and private development costs;*
- f. *Foster the safe, efficient and economic use of land, transportation, public facilities and services;*
- g. *Encourage concentrated land use patterns which decrease the length of automobile travel, encourage public transportation, allow trip consolidation and encourage pedestrian circulation between land uses;*
- h. *Enhance the appearance of the land through preservation of natural features, the provision of underground utilities, where possible, and the provision of recreation areas and open space in excess of existing standards;*
- i. *Avoid the inappropriate development of lands and provide for adequate drainage and reduction of flood damage;*
- j. *Ensure a more rational and compatible relationship between residential and non-residential uses for the mutual benefit of all;*
- k. *Provide an environment of stable character compatible with surrounding areas; and*
- l. *Provide for innovations in land development, especially for affordable housing and infill development.*

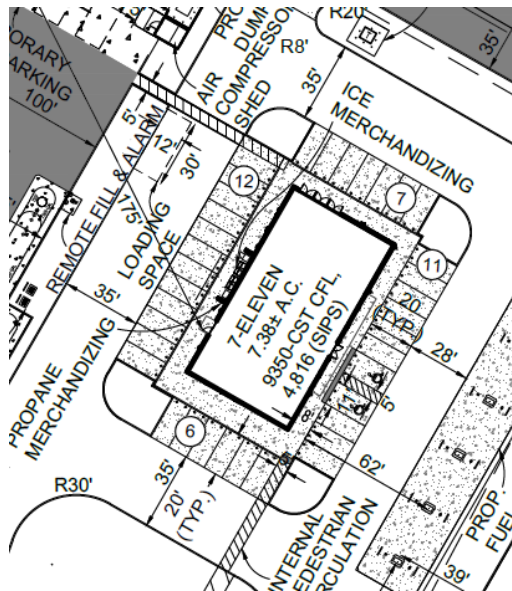
Engage New Albany Strategic Plan Recommendations

The Engage New Albany Strategic Plan recommends the following development standards for the Neighborhood Retail future land use category:

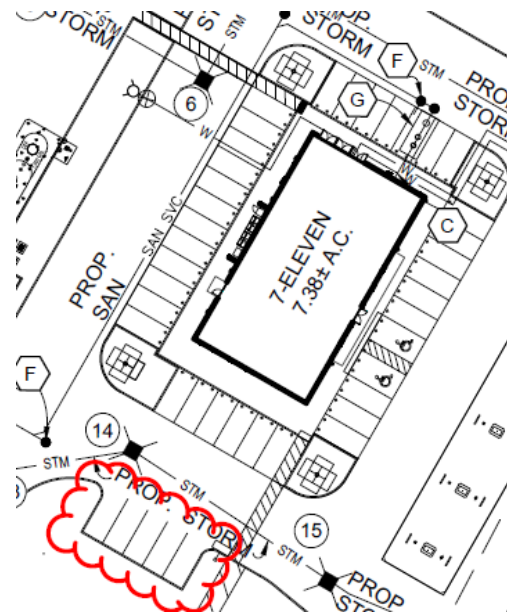
1. ☐ Parking areas should promote pedestrians by including walkways and landscaping to enhance visual aspects of the development.
2. ☐ Combined curb cuts and cross access easements are encouraged.
3. ☐ Curb cuts on primary streets should be minimized and well-organized connections should be created within and between all retail establishments.
4. ☐ Retail building entrances should connect with the pedestrian network and promote connectivity through the site.
5. ☐ Integrate outdoor spaces for food related businesses.

A. Use, Site and Layout

1. ☐ The site layout generally remains the same as previously approved aside from the additional parking spaces.



Approved



Proposed

2. ☐ Zoning text section II(C)(9) requires that the total lot coverage, which includes areas of pavement and building, to not exceed 80%. The plan meets this requirement with a proposed 43.6% lot coverage amount.

B. Access, Loading, Parking

1. ☐ The city parking code does not have parking standards for convenience stores associated with a gasoline station use. The Planning Commission should evaluate the appropriateness of the number of parking spaces provided on site.
 - ☐ The applicant is providing 41 parking spaces on site.
 - ☐ Per Codified Ordinance 1167.05(d)(8) gasoline service stations require 2 for each service bay plus 1 for each 2 gasoline dispensing units, plus 1 for each employee during main shift. This site has 10 gasoline pumps and could have up to 5 employees during the main shift resulting in 10 parking spaces being required.
 - ☐ City code does not have specific parking space requirements for convenience stores associated with a gasoline station use. The closest use within the city parking code is retail shopping centers which require one parking space for each 200 square feet of gross floor area. The convenience store is 4,816 square feet which would require 25 parking spaces. The applicant is providing 41 parking spaces which appears to accommodate both the gasoline pump and convenience store uses. This is generally consistent with other gas stations in the city such as Duke and Duchess.

C. Architectural Standards

1. ☐ No architectural changes

D. Parkland, Buffering, Landscaping, Open Space, Screening

1. ☐ No landscaping changes

E. Lighting & Signage

1. ☐ No changes to lighting or signage

IV. SUMMARY

The proposal is meeting many of the goals of the Engage New Albany Strategic Plan such as providing pedestrian access along roadways into the site and utilizing high quality building materials.

The additional parking and minimal coverage increase is not substantial. With the recommended condition of approval, the use is appropriate given the surrounding commercially zoned area.

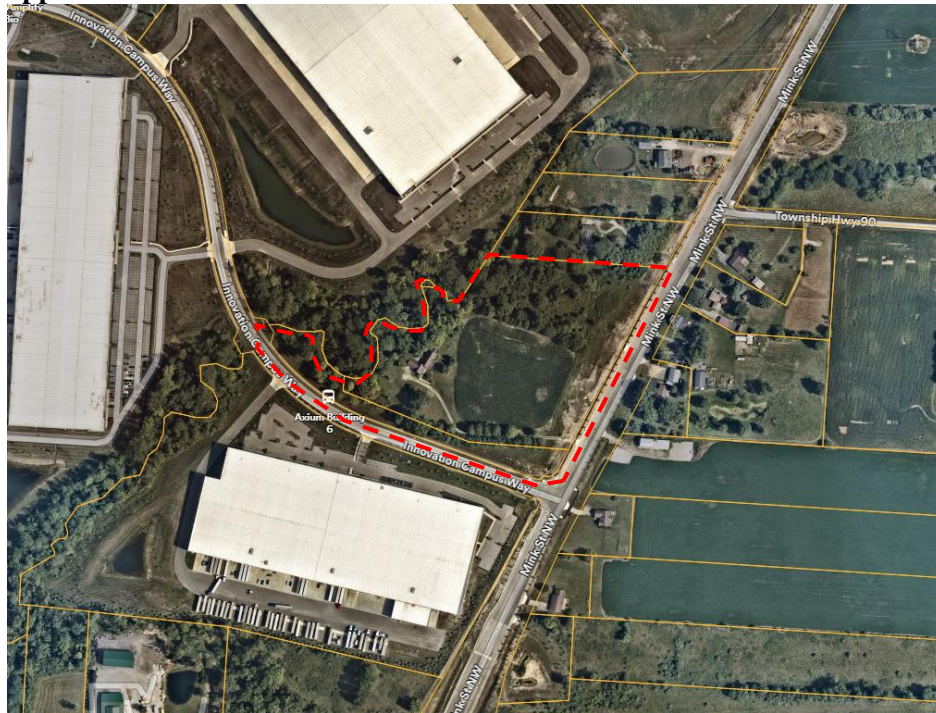
V. ACTION

Should the Planning Commission find that the application has sufficient basis for approval, the following motions would be appropriate:

Move to approve final development plan modification application FDM-19-2025, subject to the following conditions:

1. ☐ All conditions from the approved final development plan application FDP-03-2025 shall be met.

Approximate Site Location



Source: NearMap



Community Development Department

RE: City of New Albany Board and Commission Record of Action

Dear J. Carter Bean Architect,

Attached is the Record of Action for your recent application that was heard by one of the City of New Albany Boards and Commissions. Please retain this document for your records.

This Record of Action does not constitute a permit or license to construct, demolish, occupy or make alterations to any land area or building. A building and/or zoning permit is required before any work can be performed. For more information on the permitting process, please contact the Community Development Department.

Additionally, if the Record of Action lists conditions of approval these conditions must be met prior to issuance of any zoning or building permits.

Please contact our office at (614) 939-2254 with any questions.

Thank you.



Community Development Department

Decision and Record of Action

Tuesday, April 22, 2025

The New Albany Planning Commission took the following action on 04/21/2025 .

Final Development Plan Modification

Location: 1825 Mink St., New Albany, OH 43054

Applicant: J. Carter Bean Architect,

Application: PLFDM20250019

Request: Modification to the approved final development plan to allow 5 additional parking spaces for an approved Speedway gas station located at the northwest corner of Mink Street and Innovation Campus Way (PID: 093-107478-00.002).

Motion: To Approve

Commission Vote: Motion Approved with Conditions, 4-0

Result: Final Development Plan Modification, PLFDM20250019 was Approved with Conditions, by a vote of 4-0.

Recorded in the Official Journal this April 22, 2025

Condition(s) of Approval:

1. All conditions from the approved final development plan application FDP-03-2025 shall be met.

Staff Certification:

Sierra L Saumenig

Sierra Saumenig
Planner



**Planning Commission Staff Report
April 21, 2025 Meeting**

**COURTYARDS AT HAINES CREEK CLUBHOUSE
FINAL DEVELOPMENT PLAN MODIFICATION**

LOCATION:	Generally located at the northwest corner of Central College Road and Jug Street Rd NW (PID: 222-005497)
APPLICANT:	EC New Vision Ohio LLC
REQUEST:	Final Development Plan Modification
ZONING:	Courtyards at Haines Creek I-PUD Zoning District
STRATEGIC PLAN:	Residential District
APPLICATION:	FDM-21-2025

Review based on: Application materials received April 4, 2025.

Staff report completed by Jay Henderson, Planner.

I. REQUEST AND BACKGROUND

This application is for a final development plan modification for a clubhouse to be built on Reserve H of the subdivision known as “Courtyards at Haines Creek.” The change includes modifications to the clubhouse's architecture.

The Planning Commission reviewed the zoning change and preliminary development plan for the property on June 20, 2023 (ZC-07-2023), and the zoning change was adopted by City Council on July 18, 2023 (O-84-2023). The Planning Commission reviewed and approved the final development plan on March 4, 2024 (FDP-87-2023), and the final plat was approved by City Council on January 7, 2025 (FPL-85-2024).

The application is being heard by the Planning Commission because, in section VIII(B) of the Courtyard at Haines Creek zoning text states, “The private community clubhouse shall be located as generally shown on the preliminary development plan. This parcel shall be owned and maintained by the applicant (or its affiliated entities) or the HOA. The final size and configuration of the parcel and the design of the clubhouse shall be identified in an approved development plan.” The application is being heard by the Planning Commission due to the change in architecture to the clubhouse.

II. SITE DESCRIPTION & USE

The 1.5+/- acre zoning area is located in Franklin County. The site is generally located at the northwest corner of Central College Road and Jug Street Rd NW. The site is located immediately west of the Licking County line north of Agriculturally zoned and residentially used properties. Additionally, there are unincorporated residential properties to the west and north of the site.

III. EVALUATION

Staff’s review is based on New Albany's plans and studies, zoning text, and zoning regulations. Primary concerns and issues have been indicated below, with needed action or recommended action in the underlined text. The Planning Commission’s review authority is found under Chapter 1159. The property owners within 200 feet of the property in question have been notified.

The Commission should consider, at a minimum, the following (per Section 1159.08):

- a. *That the proposed development is consistent in all respects with the purpose, intent and applicable standards of the Zoning Code;*
- b. *That the proposed development is in general conformity with the Strategic Plan/Rocky Fork-Blacklick Accord or portion thereof as it may apply;*
- c. *That the proposed development advances the general welfare of the Municipality;*
- d. *That the benefits, improved arrangement and design of the proposed development justify the deviation from standard development requirements included in the Zoning Ordinance;*
- e. *Various types of land or building proposed in the project;*
- f. *Where applicable, the relationship of buildings and structures to each other and to such other facilities as are appropriate with regard to land area; proposed density may not violate any contractual agreement contained in any utility contract then in effect;*
- g. *Traffic and circulation systems within the proposed project as well as its appropriateness to existing facilities in the surrounding area;*
- h. *Building heights of all structures with regard to their visual impact on adjacent facilities;*
- i. *Front, side and rear yard definitions and uses where they occur at the development periphery;*
- j. *Gross commercial building area;*
- k. *Area ratios and designation of the land surfaces to which they apply;*
- l. *Spaces between buildings and open areas;*
- m. *Width of streets in the project;*
- n. *Setbacks from streets;*
- o. *Off-street parking and loading standards;*
- p. *The order in which development will likely proceed in complex, multi-use, multi- phase developments;*
- q. *The potential impact of the proposed plan on the student population of the local school district(s);*
- r. *The Ohio Environmental Protection Agency's 401 permit, and/or isolated wetland permit (if required);*
- s. *The U.S. Army Corps of Engineers 404 permit, or nationwide permit (if required).*

It is also important to evaluate the PUD portion based on the purpose and intent. Per Section 1159.02, PUD's are intended to:

- a. *Ensure that future growth and development occurs in general accordance with the Strategic Plan;*
- b. *Minimize adverse impacts of development on the environment by preserving native vegetation, wetlands and protected animal species to the greatest extent possible*
- c. *Increase and promote the use of pedestrian paths, bicycle routes and other non-vehicular modes of transportation;*
- d. *Result in a desirable environment with more amenities than would be possible through the strict application of the minimum commitment to standards of a standard zoning district;*
- e. *Provide for an efficient use of land, and public resources, resulting in co-location of harmonious uses to share facilities and services and a logical network of utilities and streets, thereby lowering public and private development costs;*
- f. *Foster the safe, efficient and economic use of land, transportation, public facilities and services;*
- g. *Encourage concentrated land use patterns which decrease the length of automobile travel, encourage public transportation, allow trip consolidation and encourage pedestrian circulation between land uses;*
- h. *Enhance the appearance of the land through preservation of natural features, the provision of underground utilities, where possible, and the provision of recreation areas and open space in excess of existing standards;*
- i. *Avoid the inappropriate development of lands and provide for adequate drainage and reduction of flood damage;*
- j. *Ensure a more rational and compatible relationship between residential and non-residential uses for the mutual benefit of all;*

- k. *Provide an environment of stable character compatible with surrounding areas; and*
- l. *Provide for innovations in land development, especially for affordable housing and infill development.*

Engage New Albany Strategic Plan

The site is located within the Residential District Future Land Use District. The Engage New Albany Strategic Plan lists the following development standards for the Residential District:

- Organically shaped stormwater management ponds and areas should be incorporated into the overall design as natural features and assets to the community.
- Houses should front onto public open spaces and not back onto public parks or streets.
- All or adequate amounts of open space and parkland is strongly encouraged to be provided on-site.
- A hierarchy of open spaces is encouraged. Each development should have at least one open space located near the center of the development. Typically, neighborhood parks range from half an acre to 5 acres. Multiple greens may be necessary in large developments to provide centrally located greens.
- Adequate amounts of open space and parkland are encouraged to be provided on-site.
- Rear or side-loaded garages are encouraged. When a garage faces the street, the front façade of the garage should be set back from the front façade of the house.
- Any proposed residential development outside of the Village Center shall have a base density of 1 dwelling unit per gross acre in order to preserve and protect the community's natural resources and support the overall land conservation goals of the community. A transfer of residential density can be used to achieve a gross density of 1 dwelling unit per acre.
- Private streets are at odds with many of the community's planning principles, such as: interconnectivity, a hierarchy of street typologies, and a connected community. To achieve these principles, streets within residential developments must be public.

The Engage New Albany Strategic Plan recommends the following standards as prerequisites for all development proposals in New Albany:

- Development should meet setback recommendations contained in the strategic plan.
- Streets must be public and not gated. Cul-de-sacs are strongly discouraged.
- Parks and open spaces should be provided, publicly dedicated and meet the quantity requirements established in the city's subdivision regulations (i.e. 20% gross open space and 2,400 sf of parkland dedication for each lot).
 - All or adequate amounts of open space and parkland is strongly encouraged to be provided on-site. If it cannot be provided on-site, purchasing and publicly dedicating land to expand the Rocky Fork Metro Park or park space for the Joint Parks District is an acceptable alternative.
- The New Albany Design Guidelines & Requirements for residential development must be met.
- Quality streetscape elements, including an amenity zone, street trees, and sidewalks or leisure trails, and should be provided on both sides of all public streets.
- Homes should front streets, parks and open spaces.
- A residential density of 1 dwelling unit (du) per acre is required for single-family residential and a density of 3 du per acre for age restricted housing.
 - Higher density may be allowed if additional land is purchased and deed restricted. This type of density "offset" ensures that the gross density of the community will not be greater than 1 unit per acre. Any land purchased for use as an offset, should be within the NAPLS district or within the metro park zone.
 - 3 du/acre is only acceptable if 100% age restricted. Otherwise, the federal regulations and criteria for subdivisions to qualify as age-restricted must be accounted for when calculating density (i.e. 80% age restricted and 20% non-age restricted).

- Age restriction must be recorded as a deed restriction and included as a requirement in the subdivision's zoning text.

A. Use, Site and Layout

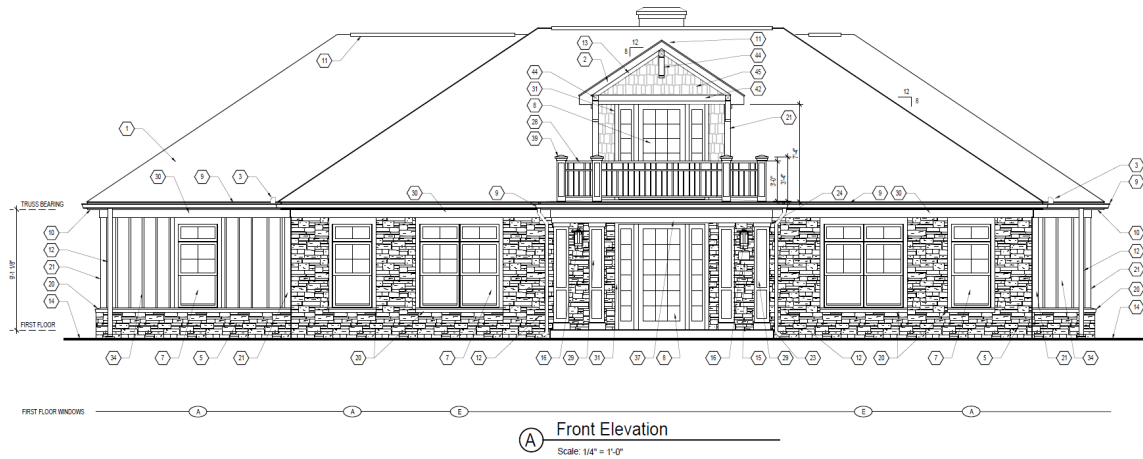
1. The zoning text section II(C) permits one private amenities center/clubhouse, which may include a fitness center, gathering spaces, outdoor pool, and/or other recreational and social facilities, amenities, and improvements serving only the residents living in this zoning district. Per the zoning text, the clubhouse shall be associated with an approved development plan, thus requiring the Planning Commission to review and approve the changes.
2. The previously approved clubhouse site design is consistent with the proposed clubhouse. It features two pickleball courts, a bocce ball court, a community garden, and a swimming pool, which were originally reviewed and approved by the Planning Commission.
3. The site is accessible from Haines Creek Drive and Wooster Drive. The site also has pedestrian access on the south and west sides of the property.
4. The proposal includes minor on-site landscaping changes and changes to the clubhouse architecture.
5. The setbacks align with the approved final development plan for the Courtyard at Haines Creek.

B. Access, Loading, Parking

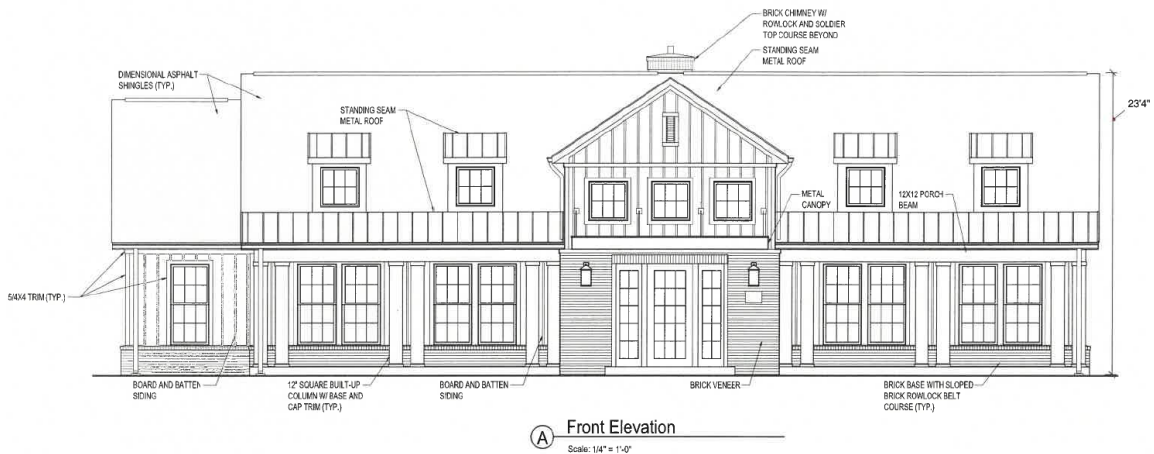
1. Per the zoning text, a parking lot may be provided near the private amenities center/clubhouse to provide for the parking needs of residents and other visitors of the residents in the community. The location of the parking lot and the number of spaces to be provided shall be determined at the time of approval of a final development plan for this zoning district.
2. Per code section 1167.05(f), the Planning Commission shall determine the number of parking spaces required for the clubhouse since it is a use not mentioned in the code.
3. The applicant proposes 23 designated parking spaces for the clubhouse, consistent with the approved final development plan.
4. Staff recommends a condition of approval to add a sidewalk connection from the community garden to the eastern leisure path (condition #1).

C. Architectural Standards

1. The applicant has proposed significant changes to the clubhouse architecture. The approved final development plan included a stone veneer face on the front elevation, which also wrapped around the building's water table.
2. The applicant has also removed the porch overhang.
3. Maximum building height: The maximum height of the private amenities' center/clubhouse shall be 35 feet as measured from the finished grade at the front door to the ridge on the roof. The proposed elevations show the clubhouse being 23 feet and 4 inches, which meets the requirement.
4. The amenities center/private clubhouse shall be 1 ½ stories in appearance or two stories in height. The architectural design and appearance of this structure shall be complimentary to and consistent with the homes in this zoning district. The proposed clubhouse is consistent with the 1 ½ story shown in the previous iteration.
5. The proposed clubhouse will be comprised of the same lap siding and board and batten siding, with brick veneer as new material. The base will be comprised of brick with a sloped rowlock belt course.
6. The City Architect has reviewed the clubhouse and recommends approval of the proposed design and materials.



Previously approved



Proposed

D. Parkland, Buffering, Landscaping, Open Space, Screening

1. Minor landscaping details have changed on site; however, staff is not concerned as the changes are not substantial.
2. All garbage cans and other waste containers shall be kept in garages or within approved screened areas. Pedestrian garbage receptacles may be located on the exterior of the amenities center/private clubhouse, provided that they are placed within or covered by an enclosure made of materials and with colors that are complimentary to the building.
3. The applicant has provided all necessary screening details for all mechanical and waste containers.

E. Lighting & Signage

1. Light poles within parking lot areas near the clubhouse shall not exceed 18 feet in height, shall be cut-off type fixtures, and be downcast. Parking lot lighting shall be from a controlled source in order to minimize light spilling beyond the boundaries of the site. These details were not provided as part of this application and will be reviewed at the time of permitting. The city recommends a condition of approval that all final lighting details be subject to staff approval (condition #2).

IV. ENGINEER'S COMMENTS

The City Engineer has reviewed the referenced plan in accordance with the engineering-related requirements of Code Section 1159.07(b)(3) and has no comments.

V. SUMMARY

The final development plan modification conforms with the residential land use density recommendations of the Engage New Albany Strategic Plan and is consistent with the zoning text requirements approved with the final development plan. The clubhouse provides the recommended amenity space within the community, maintaining the desired quality of life in the city. The changes in architectural features and details demonstrate cohesiveness with the surrounding homes. The site provides various amenities to serve the residents in the community while meeting the requirements demonstrated in the I-PUD text.

VI. ACTION

Suggested Motion for FDP-21-2025:

Move to approve FDP-21-2025 with the following conditions:

1. The applicant inserts a sidewalk connection from the community garden space to the eastern leisure path on the site.
2. That the final lighting details be subject to staff approval.

Approximate Site Location:



Source: ArcGIS



Community Development Department

RE: City of New Albany Board and Commission Record of Action

Dear EC New Vision Ohio, LC,

Attached is the Record of Action for your recent application that was heard by one of the City of New Albany Boards and Commissions. Please retain this document for your records.

This Record of Action does not constitute a permit or license to construct, demolish, occupy or make alterations to any land area or building. A building and/or zoning permit is required before any work can be performed. For more information on the permitting process, please contact the Community Development Department.

Additionally, if the Record of Action lists conditions of approval these conditions must be met prior to issuance of any zoning or building permits.

Please contact our office at (614) 939-2254 with any questions.

Thank you.



Community Development Department

Decision and Record of Action

Wednesday, April 23, 2025

The New Albany Planning Commission took the following action on 04/21/2025 .

Final Development Plan Modification

Location: PID: 222-005497

Applicant: EC New Vision Ohio, LLC,

Application: PLFDM20250021

Request: Modification to the approved final development plan for review and approval of a private clubhouse on 1.5 acres located at the northwest corner of Central College Road and Jug Street Rd NW (PID: 222-005497).

Motion: To approve

Commission Vote: Motion Approval with Conditions, 4-0

Result: Final Development Plan Modification, PLFDM20250021 was Approval with Conditions, by a vote of 4-0.

Recorded in the Official Journal this April 23, 2025

Condition(s) of Approval:

1. The applicant inserts a sidewalk connection from the community garden space to the eastern leisure path on the site.
2. That the final lighting details be subject to staff approval.

Staff Certification:

Jay Henderson
Planner



Planning Commission Staff Report April 21, 2025 Meeting

MEDICAL OFFICE BUILDING FINAL DEVELOPMENT PLAN

LOCATION: Located off the roundabout on Forest Drive, south of Johnstown Road (US-62) and north of Smith's Mill Road (PID: 222-004888)
APPLICANT: Canini & Associates
REQUEST: Final Development Plan
ZONING: Infill Planned Unit Development (I-PUD): Canini Trust Corp, subarea 8b
STRATEGIC PLAN: Employment Center
APPLICATION: FDP-20-2025

Review based on: Application materials received February 28, 2025

Staff report prepared by Kylie Blackburn, Planner

I. REQUEST AND BACKGROUND

The application is for a final development plan for two proposed medical office buildings located on the east side of the roundabout off Forest Drive south of Johnstown Road (US-62) and north of Smith's Mill Road, within the Canini Trust Corp. The development includes two medical office buildings on a 2.08-acre site.

The zoning text allows office buildings, and the permitted uses contained in the Codified Ordinances of the Village of New Albany, OCD Office Campus District, Section 1144.02, and the conditional uses contained in Section 1144.03, which includes personal services such as health offices. The applicant has applied for a variance to be heard by the Planning Commission at tonight's meeting under case VAR-24-2025 for a lot split that would create a lot with no frontage.

II. SITE DESCRIPTION & USE

The 2.08-acre site is generally located on the east side of the roundabout off Forest Drive south of Johnstown Road (US-62) and north of Smith's Mill Road, within the Canini Trust Corp. Some of the existing surrounding uses include Hampton Inn & Suites, Courtyard by Marriott, and The Estate at New Albany.

III. EVALUATION

Staff's review is based on New Albany plans and studies, zoning text, zoning regulations. Primary concerns and issues have been indicated below, with needed action or recommended action in underlined text. Planning Commission's review authority is found under Chapter 1159.

The Commission should consider, at a minimum, the following (per Section 1159.08):

- a. *That the proposed development is consistent in all respects with the purpose, intent and applicable standards of the Zoning Code;*
- b. *That the proposed development is in general conformity with the Strategic Plan/Rocky Fork-Blacklick Accord or portion thereof as it may apply;*
- c. *That the proposed development advances the general welfare of the Municipality;*

- d. *That the benefits, improved arrangement and design of the proposed development justify the deviation from standard development requirements included in the Zoning Ordinance;*
- e. *Various types of land or building proposed in the project;*
- f. *Where applicable, the relationship of buildings and structures to each other and to such other facilities as are appropriate with regard to land area; proposed density may not violate any contractual agreement contained in any utility contract then in effect;*
- g. *Traffic and circulation systems within the proposed project as well as its appropriateness to existing facilities in the surrounding area;*
- h. *Building heights of all structures with regard to their visual impact on adjacent facilities;*
- i. *Front, side and rear yard definitions and uses where they occur at the development periphery;*
- j. *Gross commercial building area;*
- k. *Area ratios and designation of the land surfaces to which they apply;*
- l. *Spaces between buildings and open areas;*
- m. *Width of streets in the project;*
- n. *Setbacks from streets;*
- o. *Off-street parking and loading standards;*
- p. *The order in which development will likely proceed in complex, multi-use, multi- phase developments;*
- q. *The potential impact of the proposed plan on the student population of the local school district(s);*
- r. *The Ohio Environmental Protection Agency's 401 permit, and/or isolated wetland permit (if required);*
- s. *The U.S. Army Corps of Engineers 404 permit, or nationwide permit (if required).*

It is also important to evaluate the PUD portion based on the purpose and intent. Per Section 1159.02, PUD's are intended to:

- a. *Ensure that future growth and development occurs in general accordance with the Strategic Plan;*
- b. *Minimize adverse impacts of development on the environment by preserving native vegetation, wetlands and protected animal species to the greatest extent possible*
- c. *Increase and promote the use of pedestrian paths, bicycle routes and other non-vehicular modes of transportation;*
- d. *Result in a desirable environment with more amenities than would be possible through the strict application of the minimum commitment to standards of a standard zoning district;*
- e. *Provide for an efficient use of land, and public resources, resulting in co-location of harmonious uses to share facilities and services and a logical network of utilities and streets, thereby lowering public and private development costs;*
- f. *Foster the safe, efficient and economic use of land, transportation, public facilities and services;*
- g. *Encourage concentrated land use patterns which decrease the length of automobile travel, encourage public transportation, allow trip consolidation and encourage pedestrian circulation between land uses;*
- h. *Enhance the appearance of the land through preservation of natural features, the provision of underground utilities, where possible, and the provision of recreation areas and open space in excess of existing standards;*
- i. *Avoid the inappropriate development of lands and provide for adequate drainage and reduction of flood damage;*
- j. *Ensure a more rational and compatible relationship between residential and non-residential uses for the mutual benefit of all;*
- k. *Provide an environment of stable character compatible with surrounding areas; and*
- l. *Provide for innovations in land development, especially for affordable housing and infill development.*

Engage New Albany Strategic Plan Recommendations

The 2020 Engage New Albany strategic plan designates the area as the Employment Center District future land use category. The strategic plan lists the following development standards for the Employment Center District land use category:

1. No freeway / pole signs are allowed.
2. Heavy landscaping is necessary to buffer these uses from adjacent residential areas.
3. Plan office buildings within context of the area, not just the site, including building height within development parcels.
4. Sites with multiple buildings should be well organized and clustered if possible.
5. All office developments are encouraged to employ shared parking or be designed to accommodate it.
6. All office developments should plan for regional stormwater management.
7. All associated mechanical operations should be concealed from the public right-of-way and screened architecturally or with landscape in an appealing manner.
8. Any periphery security should integrate with the existing landscape and maintain and enhance the character of road corridor.
9. Combined curb cuts and cross-access easements are encouraged.
10. The use of materials, colors, and texture to break up large-scale facades is required.

1. Use, Site, and Layout

1. The applicant proposes to develop two medical office buildings, the building on lot 1 will be 9,170 sq ft, and the building on lot 2 will be 5,845 sq ft. The existing total site size is 2.08 acres, which is going to be split into lot 1 totaling +/- 1.19 acres and lot 2 totaling +/- 0.89 acres
2. The variance for lot 2 to not have public street frontage is to be heard by the Planning Commission at tonight's meeting under case VAR-24-2025.
3. The proposed use is appropriate given the proximity of this site to surrounding commercial development in the area. Some of the existing surrounding uses include The Estate at New Albany, Hampton Inn, Courtyard by Marriott, Home2 Suites by Hilton, and Smith's Mill Health Campus.
4. Zoning text section 8b.01(7) requires that the total lot coverage, which includes areas of pavement and building, to not exceed 80%. The plan meets this requirement with a proposed 73% lot coverage amount.
5. The zoning text section 8b.01 requires the following setbacks:

Road	Requirement	Proposed
Forest Drive	30-foot building and 20-foot pavement setbacks	83+/- foot building [meets code] 24.13+/- foot pavement [meets code]

2. Access, Loading, Parking

1. The site is accessed from two proposed curb cuts:
 - a. One full access along Forest Drive.
 - b. One full access off of the private drive on PID: 222-004887
 - a. Before building permits, the applicant needs written consent and a cross-access easement to utilize the private drive on PID: 222-004887 for the second access (condition 1).



2. The applicant anticipates splitting the lot and developing one building on each lot. If this is done, a cross-access agreement will need to be made between the two lots (condition 2).
3. Per section 8b.02(3) of the zoning text, bicycle racks shall be provided within this subarea; this is being met with one bicycle rack on each proposed lot.
4. Codified Ordinance 1167.05(d)(5) requires a minimum of one parking space for each 200 square feet of gross floor area. Lot one's building is 9,170 square feet; therefore, 46 spaces are required, and the applicant meets this requirement with the proposed 59 spaces. Lot two's building is 5,845 square feet; therefore, 30 spaces are required, and the applicant meets this requirement with the proposed 43 spaces.
5. Per section 8b.02(5) of the zoning text, a minimum of five (5) parking spaces shall be provided for wetland park and/or trail users. These parking spaces shall be located adjacent to the entrance to the wetland park and shall be labeled as such, this is not being met.
 - o Staff recommends that these parking spaces for the wetland park be provided, subject to staff approval (condition 3).
6. Per C.O. 1167.03(a), the minimum parking space dimensions required are 9 feet wide and 19 feet long, and this requirement is met.
7. Per C.O. 1167.03(a), the minimum maneuvering lane width size is 22 feet for this development type, and this requirement is met.

3. Architectural Standards

1. The purpose of the New Albany Design Guidelines and Requirements is to help ensure that the New Albany community enjoys the highest possible quality of architectural design.
2. The zoning text contains architectural standards and is regulated by Section 6 of the Design Guidelines and Requirements (Commercial outside the Village Center).

3. The zoning text states that the maximum building height within this zoning district shall not exceed 45 feet. The proposed building height is 20 feet; therefore, this requirement is being met.
4. The applicant proposes to use brick for the majority of the building. The proposal also includes smooth cast stone. The zoning text permits the use of these materials, such as brick, pre-cast stone, wood, glass, and other synthetic materials are permitted as long as they are used appropriately. The design of the building and use of materials are appropriate and consistent with other buildings in the immediate area.
5. The zoning text states that tinted glass is permitted, but the use of mirrored or reflective glass is prohibited. The proposed windows on the two buildings will be clear/transparent glass, therefore meeting this requirement.
6. Zoning text section 8a.03(2) states that all visible elevations of a building shall receive similar treatment in style, materials, and design so that no visible side is of a lesser visual character than any other. The applicant is accomplishing this requirement by utilizing four-sided architecture.
7. A sample of a roof plan was submitted and demonstrates that all rooftop mechanical equipment will be fully screened from all public roads using a parapet wall.
8. Zoning text section 8b.03(6) states that if a flat roof is used, strong cornice lines must be integrated, and the applicant is meeting this.
9. The City Architect has reviewed the proposed architecture and has the following comments:
 - The ratio of glazing-to-solid-wall needs to be enhanced. The City Architect recommends replacing the solid panels located directly above the windows with actual transoms that are the same height as the solid panels (with a vertical separation matching the windows below). This may raise the interior first-floor ceiling height, but the exterior requires it.
 - The trim portion of the cornice design should be enhanced. The city architect pointed out that the current design might not be as strong concerning the scale of the building. The city architect gave a suggestion that the cornice could have a strong overhang to pick up on the horizontality of the entry canopy.
 - Staff recommends a condition of approval that all City Architect's comments are addressed, subject to staff approval (condition 4)

4. Parkland, Buffering, Landscaping, Open Space, Screening

1. Parking Lot Landscaping Requirement:
 - Codified Ordinance 1171.06(a)(2)(3) says that for each one hundred square feet, or fraction thereof, of parking area, a minimum total of five square feet of landscaped area shall be provided.
 - For lot 1 parking, 1,242 sq ft is required for code, and the applicant is providing 3,904 sq ft.
 - For lot 2 parking, 1,000 sq ft is required for code, and the applicant is providing 2,790 sq ft.
 - The zoning text 8b.04(4)(c) requires that the amount of interior landscaping shall be a minimum of eight percent of the total area of parking lot pavement.
 - For lot 1, parking 1,987 sq ft is required per the zoning text, and the applicant is providing 3,904 sq ft.
 - For lot 2, parking 1,599 sq ft is required per the zoning text, and the applicant is providing 2,790 sq ft.
 - Per zoning text 8b.04(4)(a), parking lots shall be screened from rights-of-way with a minimum 36-inch-high evergreen landscape hedge or wall. The landscape plan meets this requirement with the proposed 4-6-foot-high evergreen landscape hedge.
2. General Site Landscaping Requirement:
 - The zoning text 8b.04(6) requires that tree plantings shall be required within site parking and service areas. Trees shall be a minimum of 2 ½ inches in caliper at installation.

- The zoning text 8b.04(7)(a) also requires that the minimum tree size shall be no less than 2 ½ inches in caliper for street and or shade trees and 1 ½ inches in caliper for ornamental trees
 - The applicant is providing that all trees planted will be at 2 ½ inches in caliper for site parking, service, street, and buffer shade trees.
- 3. Street Tree Landscaping Requirement:
 - The zoning text section 8b.04(2) requires street trees to be planted along Forest Drive. Trees are to be a minimum of two and a half inch caliper and shall be spaced at a minimum distance of thirty feet on center, Trees may be grouped, provided that the quantity is equivalent to 1 tree per thirty feet.
 - The applicant will have 20 feet of parking frontage along Forest Drive and is providing 2 trees at 2 ½ inches in caliper, therefore meeting the requirement.
- 4. The applicant proposes to use ornamental trees for buffer trees that are required per code. The plan also proposes that the buffer ornamental trees be planted at 2.5" CAL. This requirement is met.
- 5. Staff recommends a condition of approval that all City Landscape Architect's comments are addressed, subject to staff approval (condition 5)

5. Lighting & Signage

1. The zoning text 8b.05(1)(b) requires that external building lighting shall be limited to wall-mounted sconces. No uplighting or washing of the building shall be permitted.
 - The applicant has provided images of the proposed external lighting, and they meet this requirement.
2. Per the zoning text 8b.05(1)(d), all light poles shall be black or New Albany Green with a maximum height of 20 feet, including the light fixture.
 - The applicant shows light poles will be New Albany Green and have a mounting height of 17 feet, meeting the requirement.

6. Wall Signs

The applicant has not provided a sign package at this time, but has informed staff that when signs are proposed, they will match signs of neighboring properties within the development, having sign panels with backlit or halo lettering. There will not be a monument sign on the properties.

- The city staff recommends a condition of approval that the final copy of these proposed signs and future signage is subject to staff approval (condition 6).

IV. ENGINEER'S COMMENTS

The City Engineer has reviewed the application and provided the following comments. These comments can also be found in a separate memo attached to this staff report. Staff recommends a condition of approval that the comments of the city engineer are addressed, subject to staff approval (condition 7).

1. Add the signature block and all accompanying notes to the cover sheet of the referenced document and delete what is currently shown.
2. Please show all existing easements, wetlands, and buffers shown on this exhibit on the referenced document.
3. Please show the site distance triangles in accordance with the ODOT L&D Manual on the landscape plans at the curb cuts. Add stop bars/signs and use this as the decision point. Remove impediments to motorists' view.
4. We recommend that a Licensed Landscape Architect stamp/seal the landscape plans.
5. Obtain Community Development Department approval for the photometrics plan. Footcandle intensities at the property line and the uniformity ratios are higher than what typically gets approved.
6. Provide a fire truck turning radius analysis for review and approval.
7. Record the access easement and provide a copy to the City when available.

8. We will provide additional engineering review comments once detailed plans are available.

V. SUMMARY

The proposal meets many of the goals of the Engage New Albany Strategic Plan, such as providing pedestrian access along roadways and into the site, as well as utilizing high-quality building materials that are consistent with other buildings in the immediate area. The proposed development is in an appropriate location given the context of the surrounding area development.

V. ACTION

Should the Planning Commission find that the application has sufficient basis for approval, the following motions would be appropriate:

Move to approve final development plan application **FDP-20-2025**, subject to the following conditions:

1. Before building permits, the applicant needs written consent and a cross-access easement to utilize the private drive on PID: 222-004887
2. A cross-access agreement will be needed when the lot split occurs.
3. The 5 required parking spaces stated in the zoning text for the use of the wetland park are required in the submittal process.
4. The City Architect's comments must be addressed, subject to staff approval:
 - a. The ratio of glazing-to-solid-wall needs to be enhanced. The city architect recommends replacing the solid panels located directly above the windows with actual transoms that are the same height as the solid panels (with a vertical separation matching the windows below). This may raise the interior first-floor ceiling height, but the exterior requires it.
 - b. The trim portion of the cornice design should be enhanced. The city architect pointed out that the current design might not be as strong concerning the scale of the building. The city architect gave a suggestion that the cornice could have a strong overhang to pick up on the horizontality of the entry canopy
5. The City Landscape Architect's comments must be addressed, subject to staff approval:
 - a. Shift the proposed trash enclosure to the southeast corner of the parcel. Consolidate the enclosures and provide a standard approved gate to the proposed screening wall.
 - b. Shift the proposed drive access to the south edge of the parcel.
 - c. Update the proposed site plan to include proper pedestrian connections/ walks within the interior of the site.
 - d. Revise the proposed parking lot screening application to reflect a minimum height of 36" at time of install.
 - e. Revise the proposed planting plan to provide proper screening along the north side of the parking lot.
 - f. Remove the proposed plant bed between trash enclosures to consolidate screening wall.
 - g. Per Canini Trust Corp. I-PUD Text, 8a.04 (5) Revise the proposed planting plan to provide the proper minimum deciduous shade trees in the buffer zone along Forest Drive.
 - h. New Horizon Elm is susceptible to the Elm Leaf Beetle, revise the proposed plant list to replace the New Horizon Elm with the Princeton Elm (*or similar, ensuring resistance to Dutch elm disease and the Elm leaf beetle*).
6. The final copy of the proposed signs and future signage is subject to staff approval.
7. The City Engineer's comments must be addressed, subject to staff approval;
 - o Add the signature block and all accompanying notes to the cover sheet of the referenced document and delete what is currently shown.
 - o Please show all existing easements, wetlands, and buffers shown on this exhibit on the referenced document.

- Please show the site distance triangles in accordance with the ODOT L&D Manual on the landscape plans at the curb cuts. Add stop bars/signs and use this as the decision point. Remove impediments to motorists' view.
 - We recommend that a Licensed Landscape Architect stamp/seal the landscape plans.
 - Obtain Community Development Department approval for the photometrics plan. Footcandle intensities at the property line and the uniformity ratios are higher than what typically gets approved.
 - Provide a fire truck turning radius analysis for review and approval.
 - Record the access easement and provide a copy to the city when available.
 - We will provide additional engineering review comments once detailed plans are available.
8. That the proposal is contingent upon approval of VAR-24-2025.

Approximate Site Location:



Source: Nearmap

Development Review

project name	Canini & Associates Forest Drive MOB
prepared for	City of New Albany
date	April 10, 2025
date received	March 28, 2025

COMMENTS

Site Plan

1. Remove the proposed plant bed between trash enclosures to consolidate the enclosures and provide a standard approved gate to the proposed screening wall. See diagram.
2. Update the proposed site plan to include proper pedestrian connections/ walks within the interior of the site. See diagram.

Planting Plan

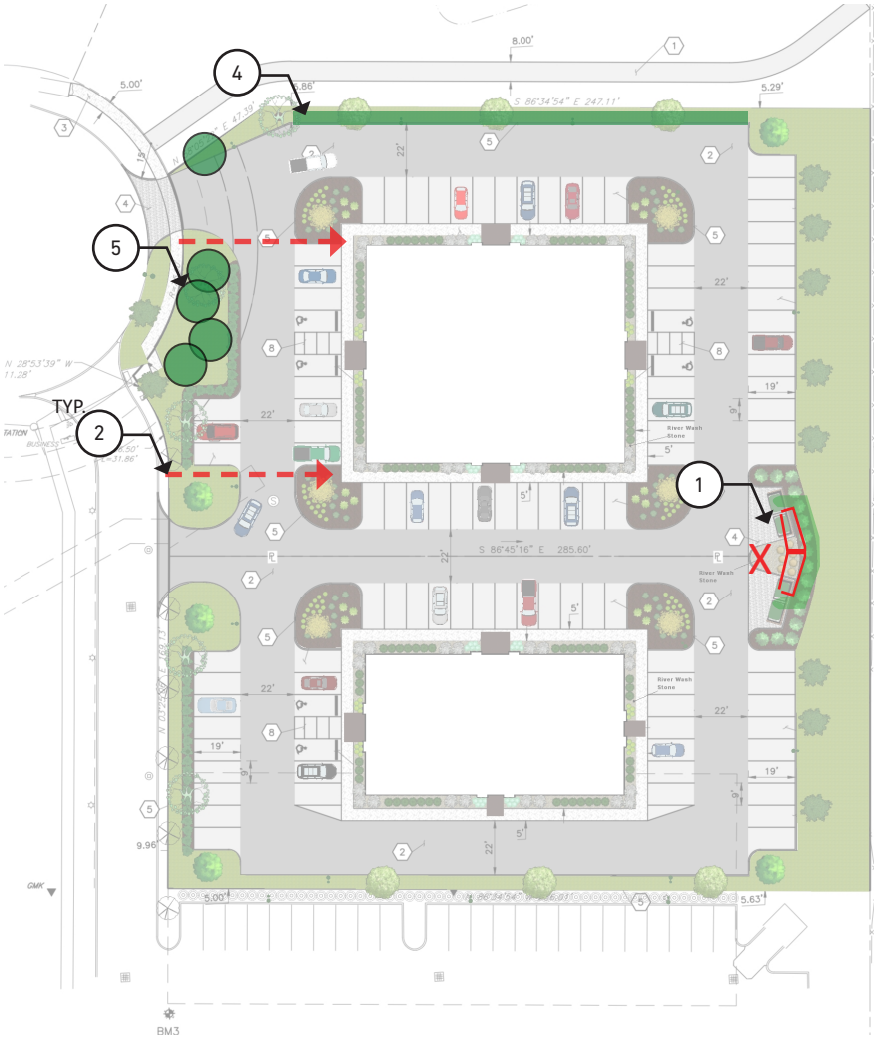
3. Revise the proposed parking lot screening application to reflect a minimum height of 36" at time of install. See diagram.
4. Revise the proposed planting plan to provide proper screening along the north side of the parking lot. See diagram.
5. Per Canini Trust Corp. I-PUD Text, 8b.04 (5) Revise the proposed planting plan to provide the proper minimum deciduous shade trees in the buffer zone along Forest Drive. See diagram.
6. New Horizon Elm is susceptible to the Elm Leaf Beetle, revise the proposed plant list to replace the New Horizon Elm with the Princeton Elm (*or similar, ensuring resistance to Dutch elm disease and the Elm leaf beetle*). See diagram.

*NOTES:

The provided diagram is for clarification and design intent purposes only. The diagram should be used to help illustrate the above comments. It is the responsibility of the design consultants to incorporate the above comments as it relates to the site and to adhere to all City requirements and subsequent code. The diagram may not be to scale.

Development Review

project name Canini & Associates Forest Drive MOB
prepared for City of New Albany
date April 10, 2025
date received March 28, 2025



		Plant Legend		
Symbol	Qty	Common	Botanical	Location
	6	Amur Maackia	Maackia Amurensis	Parking Lot Tree
	5	Princeton Sentry Ginkgo	Ginkgo biloba 'Princeton Sentry'	Parking Lot Tree
	6	Japanese Zelkova	Zelkova serrata 'Green Vase'	Parking Lot/Buffer Tree
	5	New Horizon Elm	Ulmus New Horizon	Buffer Tree
	10	Red Maple	Acer rubrum 'Red Sunset'	Street Tree
	47	Sea Green Juniper	Juniperus Chin. 'Sea Green'	Parking Screen
	48	Low Scape Chokeberry	Aronia Low Scape Mound	Landscape
	40	Bobo Panicle Hydrangea	Hydrangea paniculata 'Bobo'	Landscape
	96	Strongbox Inkberry	Ilex glabra 'Strongbox'	Landscape
	90	Variegated Liriope	Liriope muscari 'Variegata'	Landscape
	40	Blue Oat Grass	Helictotrichon sempervirens	Landscape
	40	Daylily	Heemerocallis sp.	Landscape
	18	Feather Reed Grass	Calamagrostis acutiflora	Landscape
	16	Techny Arborvitae	Thuja occidentalis 'Techny'	Dumpster Screen
	7	Zebra Grass	Miscanthus sinensis 'Strictus'	Dumpster Screen

404.741-01
April 1, 2025

To: Kylie Blackburn
City Planner

From: Matt Ferris, P.E., P.S., Consulting City Engineer
By: Jay M. Herskowitz, P.E., BCEE

Re: MOB FDP

Our review comments are as follows:

1. Refer to Exhibit A attached. Add the signature block and all accompanying notes to the cover sheet of the referenced document and delete what is currently shown.
2. Refer to Exhibit B. Please show all existing easements, wetlands and buffers shown on this exhibit on the referenced document.
3. Please show the site distance triangles in accordance with the ODOT L&D Manual on the landscape plans at the curb cuts. Add stop bars/signs and use this as the decision point. Remove impediments to motorist view.
4. We recommend that a Licensed Landscape Architect stamp/seal the landscape plans.
5. Obtain Community Development Department approval for the photometrics plan. Footcandle intensities at the property line and the uniformity ratios are higher than what typically gets approved.
6. Provide a fire truck turning radius analysis for review and approval.
7. Record the access easement and provide a copy to the City when available.
8. We will provide additional engineering review comments once detailed plans are available.

MEF/JMH

(attachments)

CC: Joshua Albright, Development Engineer



Community Development Department

RE: City of New Albany Board and Commission Record of Action

Dear Canini & Associates LLC,

Attached is the Record of Action for your recent application that was heard by one of the City of New Albany Boards and Commissions. Please retain this document for your records.

This Record of Action does not constitute a permit or license to construct, demolish, occupy or make alterations to any land area or building. A building and/or zoning permit is required before any work can be performed. For more information on the permitting process, please contact the Community Development Department.

Additionally, if the Record of Action lists conditions of approval these conditions must be met prior to issuance of any zoning or building permits.

Please contact our office at (614) 939-2254 with any questions.

Thank you.



Community Development Department

Decision and Record of Action

Wednesday, April 23, 2025

The New Albany Planning Commission took the following action on 04/21/2025 .

Final Development Plan

Location: 222-004888

Applicant: Canini & Associates LLC,

Application: PLFDP20250020

Request: Final Development Plan

Motion: To approve with conditions

Commission Vote: Motion Approval with Conditions, 4-0

Result: Final Development Plan, PLFDP20250020 was Approval with Conditions, by a vote of 4-0.

Recorded in the Official Journal this Wednesday, April 23, 2025

Condition(s) of Approval:

1. Before building permits, the applicant needs written consent and a cross-access easement to utilize the private drive on PID: 222-004887
2. A cross-access agreement will be needed when the lot split occurs.
3. A cross-access easement must be established along the entire front drive aisle to allow future connection onto parcel 222-004353 in order to meet the requirement from zoning text section 8b.02(5).
4. The City Architect's comments must be addressed, subject to staff approval:
 - The ratio of glazing-to-solid-wall needs to be enhanced. The city architect recommends replacing the solid panels located directly above the windows with actual transoms that are the same height as the solid panels (with a vertical separation matching the windows below). This may raise the interior first-floor ceiling height, but the exterior requires it.
 - The trim portion of the cornice design should be enhanced. The city architect pointed out that the current design might not be as strong concerning the scale of the building. The city architect gave a suggestion that the cornice could have a strong overhang to pick up on the horizontality of the entry canopy
5. The City Landscape Architect's comments must be addressed, subject to staff approval:
 - Shift the proposed trash enclosure to the southeast corner of the parcel. Consolidate the enclosures and provide a standard approved gate to the proposed screening wall.
 - Shift the proposed drive access to the south edge of the parcel.
 - Update the proposed site plan to include proper pedestrian connections/ walks within the interior of the site.
 - Revise the proposed parking lot screening application to reflect a minimum height of 36" at time

- of install.
 - Revise the proposed planting plan to provide proper screening along the north side of the parking lot.
 - Remove the proposed plant bed between trash enclosures to consolidate screening wall.
 - Per Canini Trust Corp. I-PUD Text, 8a.04 (5) Revise the proposed planting plan to provide the proper minimum deciduous shade trees in the buffer zone along Forest Drive.
 - New Horizon Elm is susceptible to the Elm Leaf Beetle, revise the proposed plant list to replace the New Horizon Elm with the Princeton Elm (or similar, ensuring resistance to Dutch elm disease and the Elm leaf beetle).
6. The final copy of the proposed signs and future signage is subject to staff approval.
 7. The City Engineer's comments must be addressed, subject to staff approval;
 - Add the signature block and all accompanying notes to the cover sheet of the referenced document and delete what is currently shown.
 - Please show all existing easements, wetlands, and buffers shown on this exhibit on the referenced document.
 - Please show the sight distance triangles in accordance with the ODOT L&D Manual on the landscape plans at the curb cuts. Add stop bars/signs and use this as the decision point. Remove impediments to motorists' view.
 - We recommend that a Licensed Landscape Architect stamp/seal the landscape plans.
 - Obtain Community Development Department approval for the photometrics plan. Footcandle intensities at the property line and the uniformity ratios are higher than what typically gets approved.
 - Provide a fire truck turning radius analysis for review and approval.
 - Record the access easement and provide a copy to the city when available.
 - We will provide additional engineering review comments once detailed plans are available.
 8. That the proposal is contingent upon approval of VAR-24-2025.
 9. Flip the buildings to get the parking correct with the neighbor to the south, subject to staff approval

Staff Certification:

Kylie Blackburn

Kylie Blackburn
Planner



**Board of Zoning Appeals Staff Report
April 21, 2025, Meeting**

**MEDICAL OFFICE BUILDING
LOT FRONTAGE VARIANCE**

LOCATION: Located off the roundabout on Forest Drive, south of Johnstown Road (US-62) and north of Smith's Mill Road (PID: 222-004888)
APPLICANT: Canini & Associates
REQUEST: Variance to codified ordinance 1153.04(b) to allow the creation of a lot that does not front on a public or private street.
ZONING: Infill Planned Unit Development (I-PUD): Canini Trust Corp, subarea 8b
STRATEGIC PLAN: Employment Center
APPLICATION: VAR-24-2025

Review based on: Application materials received on April 04, 2025.

Staff report prepared by Kylie Blackburn, Planner.

I. REQUEST AND BACKGROUND

The applicant requests a variance to allow for the creation of a lot that does not abut on a public or private street. The city codified ordinance 1153.04(b) states that all lots shall abut a public or private street and have adequate lot width to provide for yards and distances.

The applicant states they are requesting this variance in order to split a portion of the property to create two lots for two separate medical office buildings.

II. SITE DESCRIPTION & USE

The property is 2.08 acres in size and is currently undeveloped. The property is located on the east side of the roundabout off Forest Drive south of Johnstown Road (US-62) and north of Smith's Mill Road, within the Canini Trust Corp. The development includes two medical office buildings. The property is surrounded by similar commercial and office spaces.

The applicant has applied for a Final Development Plan to be heard by the Planning Commission at tonight's meeting under case FDP-20-2025 for the development of two medical office buildings.

III. ASSESSMENT

The application complies with application submittal requirements in C.O. 1113.03, and is considered complete. In accordance with C.O. 1113.05(b), all property owners within 200 feet of the subject property in question have been notified of the request via mail.

Criteria

The standard for granting of an area variance is set forth in the case of *Duncan v. Village of Middlefield*, 23 Ohio St.3d 83 (1986). The Board must examine the following factors when deciding whether to grant a landowner an area variance:

All of the factors should be considered and no single factor is dispositive. The key to whether an area variance should be granted to a property owner under the "practical difficulties" standard is

PC 25 0421 Medical Office Building Lot Frontage VAR-24-2025 1 of 3

whether the area zoning requirement, as applied to the property owner in question, is reasonable and practical.

1. *Whether the property will yield a reasonable return or whether there can be a beneficial use of the property without the variance.*
2. *Whether the variance is substantial.*
3. *Whether the essential character of the neighborhood would be substantially altered or adjoining properties suffer a "substantial detriment."*
4. *Whether the variance would adversely affect the delivery of government services.*
5. *Whether the property owner purchased the property with knowledge of the zoning restriction.*
6. *Whether the problem can be solved by some manner other than the granting of a variance.*
7. *Whether the variance preserves the "spirit and intent" of the zoning requirement and whether "substantial justice" would be done by granting the variance.*

Plus, the following criteria as established in the zoning code (Section 1113.06):

8. *That special conditions and circumstances exist which are peculiar to the land or structure involved and which are not applicable to other lands or structures in the same zoning district.*
9. *That a literal interpretation of the provisions of the Zoning Ordinance would deprive the applicant of rights commonly enjoyed by other properties in the same zoning district under the terms of the Zoning Ordinance.*
10. *That the special conditions and circumstances do not result from the action of the applicant.*
11. *That granting the variance requested will not confer on the applicant any special privilege that is denied by the Zoning Ordinance to other lands or structures in the same zoning district.*
12. *That granting the variance will not adversely affect the health and safety of persons residing or working in the vicinity of the proposed development, be materially detrimental to the public welfare, or injurious to private property or public improvements in the vicinity.*

IV. EVALUATION

A variance to allow the creation of a lot that does not abut on a public or private street.

The following should be considered in the board's decision:

1. The city codified ordinance 1153.04(b) states that all lots shall abut a public or private street and have adequate lot width to provide for yards and distances. The applicant requests a variance in order to allow the creation of a new lot that does not front (i.e. abut) on a public or private street.
2. The property owner requests the variance to parcel off a portion of the property. The first new proposed parcel would be the northern portion of the property at 1.19 +/- acres. The second new proposed parcel would be the southern portion of the property at 0.89 +/- acres.
3. The proposed variance meets the "spirit and intent" of the zoning requirement. The property owner states they commit to a cross-access easement running in favor of the "southern" parcel to provide it with direct access to and from Forest Drive. The city staff recommends a condition of approval requiring that the cross-access easement be recorded by the applicant with the lot being split (condition 1).
4. The variance does not appear to be substantial if the applicant is providing a cross-access easement between the north and south property. This will allow the properties to share the new driveway onto the public street, Forest Drive. The FDP does show a curb cut being added to the private driveway for the southern property, which has been conditioned in the FDP.
5. It does not appear the variance could be solved in another manner. The current parcel is on a roundabout, giving it around 30 feet of building frontage or 20 feet of parking frontage along Forest Drive. Staff recognizes the limited lot frontage as the parcel exists today,

having two properties with street frontage would be unfeasible on this site. The western side of this parcel does run along a private driveway for PID: 222-004887, which the applicant has stated they will have an agreement made to utilize this driveway once the lot split is submitted.

6. The granting of the variance would not adversely affect the delivery of government services.
7. That granting the variance will not adversely affect the health and safety of people residing or working in the vicinity of the proposed development, be materially detrimental to the public welfare, or injurious to private property or public improvements in the vicinity.
8. This type of variance has been approved before in front of the Board of Zoning Appeals in June of 2024 for a property on Smith's Mill Road, VAR-44-2024.

IV. SUMMARY

According to the property owners, the purpose of the lot split is to use the land to expand economic growth and opportunity. Although the city zoning code requires all properties to have access to public or private streets, the cross-access easement meets the spirit and intent of the code since it provides all of the properties access to a public street. If the new parcels use the proposed curb cut, this will ensure the campus design of the overall site is still achieved and therefore does not appear to be substantial.

V. ACTION

Should the Planning Commission find that the application has sufficient basis for approval, the following motions would be appropriate:

Move to approve the variance application **VAR-24-2025**, subject to the following

1. Require that the cross-access easement be recorded by the applicant with the lots.
2. Require that a cross-access easement and written consent from PID: 222-004887 be recorded by the applicant with the lot split for the use of the private driveway
3. That FDP-20-2025 is approved

Approximate Site Location:



Source: NearMap



Community Development Department

RE: City of New Albany Board and Commission Record of Action

Dear Canini & Associates LLC,

Attached is the Record of Action for your recent application that was heard by one of the City of New Albany Boards and Commissions. Please retain this document for your records.

This Record of Action does not constitute a permit or license to construct, demolish, occupy or make alterations to any land area or building. A building and/or zoning permit is required before any work can be performed. For more information on the permitting process, please contact the Community Development Department.

Additionally, if the Record of Action lists conditions of approval these conditions must be met prior to issuance of any zoning or building permits.

Please contact our office at (614) 939-2254 with any questions.

Thank you.



Community Development Department

Decision and Record of Action

Wednesday, April 23, 2025

The New Albany Planning Commission took the following action on 04/21/2025 .

Variance

Location: 222-004888

Applicant: Canini & Associates LLC,

Application: PLVARI20250024

Request: Variance to codified ordinance 1153.04(b) to allow the creation of a lot that does not front on a public or private street located on Forest Drive (PID: 222-004888).

Motion: To approve

Commission Vote: Motion Approval with Conditions, 4-0

Result: Variance, PLVARI20250024 was Approval with Conditions, by a vote of 4-0.

Recorded in the Official Journal this April 23, 2025

Condition(s) of Approval:

1. Require that the cross-access easement be recorded by the applicant with the lots.
2. Require that a cross-access easement and written consent from PID: 222-004887 be recorded by the applicant with the lot split for the use of the private driveway
3. That FDP-20-2025 is approved

Staff Certification:

Kylie Blackburn
Planner



**Planning Commission Staff Report
April 21, 2025 Meeting**

**GANTON PARKWAY WEST PHASE 3B
FINAL PLAT**

LOCATION: Various properties generally located west of the existing portion of Ganton Parkway West and south of Worthington Road
APPLICANT: City of New Albany
REQUEST: Final Plat
ZONING: Limited General Employment (L-GE)
STRATEGIC PLAN: Employment Center
APPLICATION: FPL-23-2025

Review based on: Application materials received April 7, 2025

Staff report completed by Annalise Bennett, Planner

I. REQUEST AND BACKGROUND

The application is for a final plat to dedicate right-of-way for Part B of the third phase of Ganton Parkway West. Ganton Parkway is recommended in the Engage New Albany strategic plan to facilitate additional connections within the New Albany International Business Park. This extension of Ganton Parkway West will provide access to existing and new development sites in the future and will make improvements to a small portion of Babbitt Road.

II. SITE DESCRIPTION & USE

The proposed right-of-way dedication generally extends further westward from Ganton Parkway West Phase 3A and to a small portion of Babbitt Road to the north and south, where the existing road will be improved. The property is zoned L-GE and allows the same uses as the Personal Care and Beauty Park such as office, distribution, and warehousing uses.

III. PLAN REVIEW

The Planning Commission's review authority of the final plat is found under C.O. Section 1187. Upon review of the final plat, the Commission is to make a recommendation to the City Council. The staff's review is based on city plans and studies, zoning text, and zoning regulations.

1. The Planning Commission and City Council approved a final plat for the Ganton Parkway West Phase 3A in August 2024. The right-of-way is being phased in order to allow for flexibility regarding development opportunities. Additional phases will be platted as new development occurs in the area.
2. This plat dedicates right-of-way to the city of New Albany for an extension of Ganton Parkway West. The dedication extension consists of a new public street totaling approximately 3.889 +/- acres and the improvement of approximately 1.467 +/- acres of Babbitt Road, connecting to Worthington Road/E Dublin Granville Road to the north for a total of approximately 5.356 +/- acres. No reserves are being platted, or lots are being created, within this new right-of-way extension.
3. This proposed street dedication location is also identified as a future connection in the Engage New Albany strategic plan. Similar to the development of other roads within the New Albany Business Park, this road is envisioned to ultimately connect back to Johnstown Road in the Village Center, leveraging the freeway frontage in between and providing roadway connections for future development parcels.

4. The plat dedicates 100' of right-of-way for Ganton Parkway West and 80' for Babbitt Road. The plat area is located in multiple L-GE zoning districts. The zoning texts contain provisions for the general roadway alignment shown on the plat. The city is supportive of the proposed right-of-way widths and the zoning requirements are being met.
5. The convergence of Ganton Parkway West and Babbitt Road in this general area is identified as both a minor arterial road typology and a minor collector road typology in the Engage New Albany strategic plan. The plan indicates these roads should be designed at a larger scale in order to best accommodate traffic traveling into the business park. The proposed rights-of-way for Ganton Parkway West and Babbitt Road are consistent with the recommendations in the Engage New Albany Strategic Plan (67' to 115').
6. Proposed streetscape and utility easements are shown on the plat to ensure desired street improvements can be accommodated.
7. The Ganton Parkway South zoning text requirement for all property owners with frontage along Babbitt Road to be notified via mail has been met as part of this application.

IV. ENGINEER'S COMMENTS

The city Engineer has reviewed the referenced plan in accordance with the engineering-related requirements of Code Section 1159.07(b)(3) and has no comments.

V. SUMMARY

The proposed road plat is consistent with the goals and objectives found in the Engage New Albany strategic plan for this area. The road improvements in this area will serve as critical connections within the New Albany Business Park and provide access for existing and new development sites in the future.

VI. ACTION

Should the Planning Commission find that the application has sufficient basis for approval, the following motion would be appropriate:

Move to recommend approval to City Council of application FPL-23-2025.

Approximate Street Location Shown in Red:



Source: Nearmap



Community Development Department

RE: City of New Albany Board and Commission Record of Action

Dear CITY OF NEW ALBANY,

Attached is the Record of Action for your recent application that was heard by one of the City of New Albany Boards and Commissions. Please retain this document for your records.

This Record of Action does not constitute a permit or license to construct, demolish, occupy or make alterations to any land area or building. A building and/or zoning permit is required before any work can be performed. For more information on the permitting process, please contact the Community Development Department.

Additionally, if the Record of Action lists conditions of approval these conditions must be met prior to issuance of any zoning or building permits.

Please contact our office at (614) 939-2254 with any questions.

Thank you.



Community Development Department

Decision and Record of Action

Thursday, April 24, 2025

The New Albany Planning Commission took the following action on 04/21/2025 .

Final Plat

Location: Ganton Parkway

Applicant: CITY OF NEW ALBANY,

Application: PLFPL20250023

Request: Final Plat for the dedication of public right-of-way for Ganton Parkway West Phase 3B located west of Beech Road, south of Worthington Road, and east of Babbitt Road.

Motion: To approve

Commission Vote: Motion Approved, 4-0

Result: Final Plat, PLFPL20250023 was Approved, by a vote of 4-0.

Recorded in the Official Journal this April 24, 2025

Condition(s) of Approval: n/a

Staff Certification:

Annalise Bennett
Planner



Community Development Department

Meeting Sign-in Sheet

[illegible]

Cynthia Coleman 6145 Babbitt
Cindycoleman@gmail.com
614-832-2107

Jim Doran - mgdor@insight.net.com
Michele 614-596-6078

Ron Farber - ~~kon~~
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**Planning Commission Staff Report
May 5, 2025 Meeting**

**AEP SUBSTATION
FINAL DEVELOPMENT PLAN**

LOCATION: 7375 Souder Road (PID: 222-004891)
APPLICANT: AEP, Ohio Transmission Company, Inc. c/o Amy Toohey
REQUEST: Final Development Plan
ZONING: Souder East Office, Research, & Information District I-PUD
STRATEGIC PLAN: Employment Center
APPLICATION: FDP-05-2025

Review based on: Application materials received January 23, 2025 and March 19, 2025

Staff report prepared by Sierra Saumenig, Planner 2

NOTE FROM THE LAW DIRECTOR

As noted during the April 21, 2025 Regular Meeting of the Planning Commission, from the time this matter was first tabled during the February 19, 2025 Planning Commission Meeting until the date of this Updated Staff Report, the City and AEP have exchanged correspondence and engaged in discussions with one another regarding the pending matter. While AEP indicated in correspondence dated March 14, 2025 that the construction of its Substation was “not subject to the City’s zoning process” pursuant to R.C. 49065.65, the City disputed its contention and notified it of its position that it maintained the matter was subject to the jurisdiction of the Planning Commission. As such, AEP was advised the matter would proceed before the Planning Commission.

Because of the contrary positions of the parties concerning the role of the Planning Commission in reviewing the Final Development Plan (“FDP”), the following is a summary of the position taken by the City with respect to the proposed Substation.

At the outset, it is important to recognize the proposed Substation is in an I-PUD. AEP filed a FDP application giving rise to this matter. As was shared with AEP, undoubtedly, the Planning Commission has jurisdiction as it is well-settled “a public utility which seeks to build a public utility facility, as defined in R.C. 4905.65(A)(2), and which facility meets the tests under R.C. 4905.65(B)(1), (2) and (3), must first apply for any permits regarding construction or location of the facility required by the political subdivision in which the utility proposes to build. If the subdivision will not issue the permits to the satisfaction of the utility, then the utility may resort to the courts to determine the degree of local regulation permitted under R.C. 4905.65.” *Cleveland Elec. Illum. Co. v. Lakewood*, 64 Ohio St.2d 374, 374.

Regarding the authority of the Planning Commission to regulate the proposed Substation set forth in FDP-05-2025, it may be limited by R.C. 4905.65, though. Ohio Rev. Code 4905.65, entitled Local Regulation of Public Utility Facilities, states:

(A) As used in this section:

(1) "Public utility" means any electric light company, as the same is defined in sections [4905.02](#) and [4905.03](#) of the Revised Code.

(2) "Public utility facility" means any electric line having a voltage of twenty-two thousand or more volts used or to be used by an electric light company and supporting structures, fixtures, and appurtenances connected to, used in direct connection with, or necessary for the operation or safety of such electric lines.

(3) "Local regulation" means any legislative or administrative action of a political subdivision of this state, or of an agency of a political subdivision of this state, having the effect of restricting or prohibiting the use of an existing public utility facility or facilities or the proposed location, construction, or use of a planned public utility facility or facilities.

(B) To the extent permitted by existing law a local regulation may reasonably restrict the construction, location, or use of a public utility facility, unless the public utility facility:

(1) Is necessary for the service, convenience, or welfare of the public served by the public utility in one or more political subdivisions other than the political subdivision adopting the local regulation; and

(2) Is to be constructed in accordance with generally accepted safety standards; and

(3) Does not unreasonably affect the welfare of the general public.

Nothing in this section prohibits a political subdivision from exercising any power which it may have to require, under reasonable regulations not inconsistent with this section, a permit for any construction or location of a public utility facility by a public utility in such political subdivision.

AEP is a "public utility" as defined in R.C. 4905.65(A)(1).

AEP has submitted correspondence confirming that the electric lines will have a voltage in excess of 22,000 volts. Consequently, AEP maintains the proposed Substation is a "public utility facility" as defined in R.C. 4905.65(A)(2). In email correspondence, AEP has confirmed "the substation and related facilities are, indeed, electric facilities that will operate at a voltage greater than 22 kV. Some of the equipment in the substation will be interconnected and operate at 138 kV." (Email from hgarcia1@aep.com dated April 21, 2025).

Any action of the Planning Commission would be considered a "local regulation" as defined in R.C. 4905.65(A)(3).

Consistent with R.C. 4905.65, to the extent permitted by law, the Planning Commission "may reasonably restrict the construction, location or use of a public utility facility, unless the public utility facility" satisfies three (3) criteria. (R.C. 4905.65(B)(1)-(3), underline added). The 3 criteria are outlined above. AEP has submitted information via email in response to each of the criteria.

Necessary for the Service, Convenience or Welfare of the Public (R.C. 4905.65(B)(1))

As noted in an email, AEP stated “the substation, along with the equipment in and connected to it, is necessary to provide electric service to the public, including providing electric public utility service to the people, businesses, governments, and facilities that receive their electric service from Ohio Power in the area, as well as in neighboring areas that either draw electricity through the Ohio Power distribution system locally or are part of the distribution and electric transmission network that serves customers in other counties, cities, and townships in Ohio. (Email from hgarcia1@aep.com dated April 21, 2025).

Constructed in Accordance with Generally Accepted Safety Standards (R.C. 4905.65(B)(2))

As noted in an email, AEP indicated the proposed “substation and related facilities will be constructed in accordance with generally accepted safety standards and will be built with strict adherence to requirements from the National Electrical Safety Code (NESC) and consistent with the North American Electric Reliability Corporation (NERC), which is overseen by the Federal Energy Regulatory Commission (FERC). Additionally, the substation will be connected to 138 kV transmission lines classified as major utility facilities, which are subject to regulation and requirements from the Ohio Power Siting Board (OPSB).” (Email from hgarcia1@aep.com dated April 21, 2025).

Does Not Unreasonably Affect the Welfare of the General Public (R.C. 4905.65(B)(3))

In support of its contention that it does not unreasonably affect the welfare of the General Public, AEP submitted the following in an email: “...the substation and related facilities do not unreasonably affect the welfare of the general public, as evidenced by the fact that the facilities are part of the extensive network used by Ohio Power to provide electric service to its customers and the region. The facilities are designed and will be constructed to satisfy stringent safety and engineering requirements while minimizing their impact on surrounding areas, as is common practice in the industry for constructing and maintaining electric utility facilities of this type in both urban and rural areas.” (Email from hgarcia1@aep.com dated April 21, 2025).

In the event it is determined that AEP has complied with R.C. 4905.65, the Planning Commission’s authority is limited as it may not unreasonably restrict the “construction, location, or use” of the proposed Substation.

Although its authority may be limited by R.C. 4905.65, the Planning Commission does retain the authority “to require, under reasonable regulations not inconsistent with this section, a permit for any construction or location of a public utility facility by a public utility in such political subdivision.” (R.C. 4905.65). As a result, the matter is not entirely outside the jurisdiction of the Planning Commission. In fact, courts have recognized a public utility has to “**make a sincere attempt** to comply with the regulations of the political subdivision in which it is trying to build. If the two sides cannot reach an agreement, then the utility has the option of resorting to courts.” *Cleveland Electric Illuminating Co. v. Lakewood*, 64 Ohio St.2d 374 (1980).

Consequently, while the Planning Commission may be limited in its ability to reasonably restrict the location, construction and use of a public utility facility consistent with R.C. 4905.65(B), if AEP meets the criteria set forth therein, AEP still must engage in the permitting process and “make a sincere effort” to comply. Again, R.C. 4905.65(B) pertains to local regulations and restrictions on the “location, construction and use of a public utility facility.” In the event AEP believes the Planning Commission has imposed unreasonable restrictions on it and the parties cannot reach an agreement, it has the option to pursue the matter in a court.

I. REQUEST AND BACKGROUND

This final development plan application is for a proposed AEP electrical substation located at 7375 Souder Road.

The property in question is zoned I-PUD and is located within the Souder East Office, Research, & Information District Subarea 4. The proposed use (electrical substation) is permitted as this use is identified as an essential service which is permitted in all zoning districts.

It's important to note as this use is a public utility and essential service, the applicant has broader flexibility and does not need to seek variances if a zoning requirement cannot be met per the city law director. Overall the plan is meeting the applicable zoning requirements however there is flexibility for the number of curb cuts, stormwater basin design, and street trees within a utility corridor.

II. SITE DESCRIPTION & USE

The site is generally located north of New Albany Road on the west side of Souder Road. The site is 9.50 acres and is currently undeveloped. Surrounding uses include Canine Companions to the west, undeveloped and commercial uses to the east, commercial to the south, and residential to the north.

III. EVALUATION

Staff's review is based on New Albany plans and studies, zoning text, and zoning regulations. Primary concerns and issues have been indicated below, with needed action or recommended action in underlined text. Planning Commission's review authority is found under Chapter 1159.

The Commission should consider, at a minimum, the following (per Section 1159.08):

- a. *That the proposed development is consistent in all respects with the purpose, intent and applicable standards of the Zoning Code;*
- b. *That the proposed development is in general conformity with the Strategic Plan/Rocky Fork-Blacklick Accord or portion thereof as it may apply;*
- c. *That the proposed development advances the general welfare of the Municipality;*
- d. *That the benefits, improved arrangement and design of the proposed development justify the deviation from standard development requirements included in the Zoning Ordinance;*
- e. *Various types of land or building proposed in the project;*
- f. *Where applicable, the relationship of buildings and structures to each other and to such other facilities as are appropriate with regard to land area; proposed density may not violate any contractual agreement contained in any utility contract then in effect;*
- g. *Traffic and circulation systems within the proposed project as well as its appropriateness to existing facilities in the surrounding area;*
- h. *Building heights of all structures with regard to their visual impact on adjacent facilities;*
- i. *Front, side and rear yard definitions and uses where they occur at the development periphery;*
- j. *Gross commercial building area;*
- k. *Area ratios and designation of the land surfaces to which they apply;*
- l. *Spaces between buildings and open areas;*
- m. *Width of streets in the project;*
- n. *Setbacks from streets;*
- o. *Off-street parking and loading standards;*
- p. *The order in which development will likely proceed in complex, multi-use, multi-phase developments;*
- q. *The potential impact of the proposed plan on the student population of the local school district(s);*
- r. *The Ohio Environmental Protection Agency's 401 permit, and/or isolated wetland permit (if required);*
- s. ☐ *The U.S. Army Corps of Engineers 404 permit, or nationwide permit (if required).*

It is also important to evaluate the PUD portion based on the purpose and intent. Per Section 1159.02, PUD's are intended to:

- a. *Ensure that future growth and development occurs in general accordance with the Strategic Plan;*
- b. *Minimize adverse impacts of development on the environment by preserving native vegetation, wetlands and protected animal species to the greatest extent possible*
- c. *Increase and promote the use of pedestrian paths, bicycle routes and other non-vehicular modes of transportation;*
- d. *Result in a desirable environment with more amenities than would be possible through the strict application of the minimum commitment to standards of a standard zoning district;*
- e. *Provide for an efficient use of land, and public resources, resulting in co-location of harmonious uses to share facilities and services and a logical network of utilities and streets, thereby lowering public and private development costs;*
- f. *Foster the safe, efficient and economic use of land, transportation, public facilities and services;*
- g. *Encourage concentrated land use patterns which decrease the length of automobile travel, encourage public transportation, allow trip consolidation and encourage pedestrian circulation between land uses;*
- h. *Enhance the appearance of the land through preservation of natural features, the provision of underground utilities, where possible, and the provision of recreation areas and open space in excess of existing standards;*
- i. *Avoid the inappropriate development of lands and provide for adequate drainage and reduction of flood damage;*
- j. *Ensure a more rational and compatible relationship between residential and non-residential uses for the mutual benefit of all;*
- k. *Provide an environment of stable character compatible with surrounding areas; and*
- l. *Provide for innovations in land development, especially for affordable housing and infill development.*

Engage New Albany Strategic Plan Recommendations

The Engage New Albany Strategic Plan lists the following development standards for the Employment Center future land use category:

1. ☐ No freeway/pole signs are allowed.
2. ☐ Heavy landscaping is necessary to buffer these uses from adjacent residential areas.
3. ☐ Plan office buildings within the context of the areas, not just the site, including building heights within development parcels.
4. ☐ Sites with multiple buildings should be well organized and clustered if possible.
5. ☐ All office developments are encouraged to employ shared parking or be designed to accommodate it.
6. ☐ All office developments should plan for regional stormwater management.
7. ☐ All associated mechanical operations should be concealed from the public right-of-way and screened architecturally or with landscape in an appealing manner.
8. ☐ Any periphery security should integrate with the existing landscape and maintain and enhance the character of the road.
9. ☐ Combined curb cuts and cross-access easements are encouraged.
10. ☐ The use of materials, colors and textures to break up large scale facades is required.

A. Use, Site and Layout

1. ☐ The applicant proposes to develop an AEP electrical substation that is named Souder Station. The existing total site size is 9.50 acres. The proposed use is appropriate for this location in the New Albany Business Park. It will satisfy an existing and future electrical need in the area. The use is permitted as it is an essential service that is permitted in all of the city's zoning districts.
2. ☐ The station will contain electrical equipment that is installed on a gravel pad and secured by fencing. Additionally, there are two transmission poles that connect to the station. These poles are situated on the east side, outside the station fence, and are linked to the equipment within the station, as well as AEP's existing transmission network.

3. ☐ The applicant will install a leisure trail and horse fence along Souder Road.
4. ☐ The PUD zoning text requires the following setbacks from these perimeter boundaries.
Since the site does not consist of pavement or a building, these setbacks do not apply.

Perimeter Boundary	Required Setback
Souder Road	30-foot building and pavement
Western Boundary	50-foot building and pavement
Northern Boundary	50-foot building and pavement
Southern Boundary	25-foot building and pavement

5. ☐ The applicant indicates that the onsite stormwater will be conveyed to an onsite stormwater basin on the south end of the site.

B. Access, Loading, Parking

1. ☐ The site is proposed to be accessed from two curb cuts along Souder Road including:
 - a. ☐ One full-service curb cut on the northern part of the site.
 - b. ☐ One full-service curb cut on the southern part of the site.
2. ☐ Parking is not applicable for this site as the site is not regularly occupied or staffed.

C. Parkland, Buffering, Landscaping, Open Space, Screening

1. ☐ The applicant is providing street trees along Souder Road in the tree lawn between the leisure trail and road pavement with the exception of the utility corridor.
2. ☐ The New Albany Business Park Research and Information Campus Design Guidelines require 15 trees per 100 linear feet to be planted along Souder Road however, the applicant is not able to do so due to the utility corridor. In lieu of providing this landscaping on Souder Road, staff recommend a condition of approval that the applicant include naturalized plantings in the northwest corner of the site to provide additional screening (condition #1)
3. ☐ Zoning text section H(4) requires an earthmound with landscaping along the northern property line that is shared with Parcel Number 222-000596. The earthmound shall be a minimum of 6 feet in height at its crest and shall be planted with evergreen or deciduous trees at an average rate of 12 trees per 100 lineal feet. This has already been installed and will remain in place.
4. ☐ The applicant is providing naturalized trees around the basin to help with screening.

D. Lighting & Signage

1. ☐ Zoning text section V(I) requires all parking lot lights to be cut-off and downcast, not exceeding 30 feet in height, and be black or New Albany Green.
 - o ☐ The applicant submitted a photometrics plan showing no light spillage from property lines and the mounting height of poles to be 29 feet.
2. ☐ The applicant proposes to install two address placards along the horse fence at the proposed entrances that meet the New Albany Business Park Research and Information Campus Design Guidelines.

IV. ENGINEER'S COMMENTS

The City Engineer has reviewed the application and provided the following comments. Staff recommends a condition of approval that the comments of the city engineer are addressed, subject to staff approval (condition #2).

1. Engineering staff will evaluate storm water management, water distribution, sanitary sewer collection and roadway construction related details once construction plans become available.

V. SUMMARY

Since the substation is an essential service, it will contribute economic value by meeting current and future electrical demands in the area, which is essential for the expanding business park. The

proposed development is in an appropriate location given the context of the surrounding area and serves as an important resource for the New Albany Business Park.

V. ACTION

Should the Planning Commission find that the application has sufficient basis for approval, the following motions would be appropriate:

Move to approve final development plan application FDP-05-2025, subject to the following conditions:

1. ☐ That the applicant includes naturalized plantings in the northwest corner of the site to provide additional screening
2. ☐ Engineering staff will evaluate storm water management, water distribution, sanitary sewer collection and roadway construction related details once construction plans become available.

Approximate Site Location



Source: NearMap

From: [Hector H Garcia-Santana](#)
To: [Sierra Saumenig](#); [Jennifer L Willis](#); [Amy Toohey](#); [Maggie R Beggs](#); [Ben Albrecht](#); [Christopher Christian](#); [rdove@keglerbrown.com](#); [CCunningham@keglerbrown.com](#)
Cc: [Hector H Garcia-Santana](#); [Marland L Turner](#); [Robert J Schmidt](#)
Subject: RE: AEP Follow-up and update
Date: Monday, April 21, 2025 2:32:15 PM
Attachments: [image001.png](#)

Hi Sierra,

Thank you for your note. Here is the requested confirmation and information:

The substation and related facilities are, indeed, electric facilities that will operate at a voltage greater than 22 kV. Some of the equipment in the substation will be interconnected and operate at 138 kV.

You are also correct that the substation will be directly connected to 138 kV transmission lines, and that the substation is necessary for the operation and safety of those lines, as well as for the operation and safety of the other lines connecting the substation to the electric distribution system in the area. The facilities are typical examples of electric public utility facilities and equipment, and they satisfy all the requirements of the definition of “public utility facility” under R.C. 4905.65.

More detail:

1. The substation, along with the equipment in and connected to it, is necessary to provide electric service to the public, including providing electric public utility service to the people, businesses, governments, and facilities that receive their electric service from Ohio Power in the area, as well as in neighboring areas that either draw electricity through the Ohio Power distribution system locally or are part of the distribution and electric transmission network that serves customers in other counties, cities, and townships in Ohio.
2. You are also correct that the substation and related facilities will be constructed in accordance with generally accepted safety standards and will be built with strict adherence to requirements from the National Electrical Safety Code (NESC) and consistent with the North American Electric Reliability Corporation (NERC), which is overseen by the Federal Energy Regulatory Commission (FERC). Additionally, the substation will be connected to 138 kV transmission lines classified as major utility facilities, which are subject to regulation and requirements from the Ohio Power Siting Board (OPSB).
3. Lastly, you are also correct that the substation and related facilities do not unreasonably affect the welfare of the general public, as evidenced by the fact that the facilities are part of the extensive network used by Ohio Power to provide electric service to its customers and the region. The facilities are designed and will be constructed to satisfy stringent safety and engineering requirements while minimizing their impact on surrounding areas, as is common practice in the industry for constructing and maintaining electric utility facilities of this type in both urban and rural areas.

We appreciate the opportunity to provide this additional information to confirm that, in fact, the facilities you inquired about are public utility facilities. Please let us know if you would like additional detail.

Thanks!
Hector

From: Sierra Saumenig <ssaumenig@newalbanyohio.org>
Sent: Wednesday, April 16, 2025 8:20 AM
To: Jennifer L Willis <jwillis@aep.com>; Amy J Toohey <ajtoohey@aep.com>; Maggie R Beggs <mrbeggs@aep.com>; Hector H Garcia-Santana <hgarcia1@aep.com>; Ben Albrecht <balbrecht@fisheldowney.com>; Christopher Christian <cchristian@newalbanyohio.org>; rdove@keglerbrown.com; CCunningham@keglerbrown.com
Subject: [EXTERNAL] RE: AEP Follow-up and update

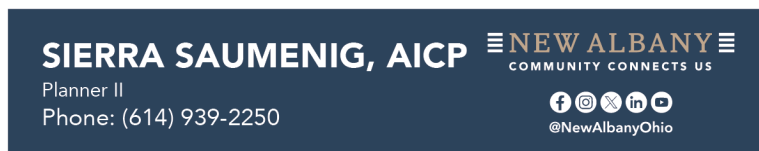
Good Morning,

I wanted to follow up to confirm that the proposed development meets the following definition as well.

R.C. 4905.65 defines a "public utility facility" as **"any electric line having a voltage of twenty-two thousand or more volts used or to be used by an electric light company and supporting structures, fixtures, and appurtenances connected to, used in direct connection with, or necessary for the operation or safety of such electric lines."**

The 3 criteria referenced in the email yesterday, if met, limit the authority of the Planning Commission.

Thank you!



From: Sierra Saumenig
Sent: Tuesday, April 15, 2025 3:37 PM
To: Jennifer L Willis <jwillis@aep.com>; Amy Toohey <ajtoohey@aep.com>; Maggie R Beggs <mrbeggs@aep.com>; Hector H Garcia-Santana <hgarcia1@aep.com>; Ben Albrecht <balbrecht@fisheldowney.com>; Christopher Christian <cchristian@newalbanyohio.org>; rdove@keglerbrown.com; CCunningham@keglerbrown.com
Subject: AEP Follow-up and update

Good Afternoon,

I just wanted to send an update about AEP. As you are aware, this will be tabled at the April 21, 2025 meeting.



Community Development Planning Application

Project Information	Site Address <u>7375 Souder Road New Albany Ohio 43054</u> Parcel Numbers <u>222-004891</u> Acres <u>9.50 acres-calculated</u> # of lots created <u>None</u>																																																																								
	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <th style="text-align: left; padding: 2px;">Choose Application Type</th> <th colspan="5" style="text-align: left; padding: 2px;">Circle all Details that Apply</th> </tr> <tr> <td style="padding: 2px;"><input type="checkbox"/> Appeal</td> <td></td><td></td><td></td><td></td><td></td> </tr> <tr> <td style="padding: 2px;"><input type="checkbox"/> Certificate of Appropriateness</td> <td></td><td></td><td></td><td></td><td></td> </tr> <tr> <td style="padding: 2px;"><input type="checkbox"/> Conditional Use</td> <td></td><td></td><td></td><td></td><td></td> </tr> <tr> <td style="padding: 2px;"><input checked="" type="checkbox"/> Development Plan</td> <td style="padding: 2px;">Preliminary</td> <td style="padding: 2px;">Final</td> <td style="padding: 2px;">Comprehensive</td> <td colspan="2" style="padding: 2px;">Amendment</td> </tr> <tr> <td style="padding: 2px;"><input type="checkbox"/> Plat</td> <td style="padding: 2px;">Preliminary</td> <td style="padding: 2px;">Final</td> <td></td><td></td><td></td> </tr> <tr> <td style="padding: 2px;"><input type="checkbox"/> Lot Changes</td> <td style="padding: 2px;">Combination</td> <td style="padding: 2px;">Split</td> <td colspan="3" style="padding: 2px;">Adjustment</td> </tr> <tr> <td style="padding: 2px;"><input type="checkbox"/> Minor Commercial Subdivision</td> <td></td><td></td><td></td><td></td><td></td> </tr> <tr> <td style="padding: 2px;"><input type="checkbox"/> Vacation</td> <td style="padding: 2px;">Easement</td> <td></td> <td colspan="3" style="padding: 2px;">Street</td> </tr> <tr> <td style="padding: 2px;"><input checked="" type="checkbox"/> Variance</td> <td></td><td></td><td></td><td></td><td></td> </tr> <tr> <td style="padding: 2px;"><input type="checkbox"/> Extension Request</td> <td></td><td></td><td></td><td></td><td></td> </tr> <tr> <td style="padding: 2px;"><input type="checkbox"/> Zoning</td> <td colspan="2" style="padding: 2px;">Amendment (rezoning)</td> <td colspan="3" style="padding: 2px;">Text Modification</td> </tr> </table>	Choose Application Type	Circle all Details that Apply					<input type="checkbox"/> Appeal						<input type="checkbox"/> Certificate of Appropriateness						<input type="checkbox"/> Conditional Use						<input checked="" type="checkbox"/> Development Plan	Preliminary	Final	Comprehensive	Amendment		<input type="checkbox"/> Plat	Preliminary	Final				<input type="checkbox"/> Lot Changes	Combination	Split	Adjustment			<input type="checkbox"/> Minor Commercial Subdivision						<input type="checkbox"/> Vacation	Easement		Street			<input checked="" type="checkbox"/> Variance						<input type="checkbox"/> Extension Request						<input type="checkbox"/> Zoning	Amendment (rezoning)		Text Modification		
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Description of Request: <u>AEP, a public utility provider, is proposing to construct a new electrical station at 7375 Souder Road.</u>																																																																									
Contacts	Property Owner's Name: <u>AEP, Ohio Transmission Company, Inc.</u> Address: <u>8500 Smiths Mill Road</u> City, State, Zip: <u>New Albany, Ohio 43054</u> Phone number: <u>614 477-5410</u> Fax: _____ Email: <u>jlwalker2@aep.com</u>																																																																								
	Applicant's Name: <u>Jennifer Walker/Amy Toohey</u> Address: <u>8500 Smiths Mill Road</u> City, State, Zip: <u>New Albany, Ohio 43054</u> Phone number: <u>614 477-5410/614 565 1480</u> Fax: _____ Email: <u>jlwaker2@aep.com/ajtoohey@aep.com</u>																																																																								
Signature	Site visits to the property by City of New Albany representatives are essential to process this application. The Owner/Applicant, as signed below, hereby authorizes Village of New Albany representatives, employees and appointed and elected officials to visit, photograph and post a notice on the property described in this application. I certify that the information here within and attached to this application is true, correct and complete.																																																																								
	<table style="width: 100%;"> <tr> <td style="width: 30%;">Signature of Owner</td> <td style="width: 40%; text-align: center;"><u>Jennifer Walker</u></td> <td style="width: 30%;">Date: <u>2-7-2025</u></td> </tr> <tr> <td>Signature of Applicant</td> <td style="text-align: center;"><u>Jennifer Walker</u></td> <td>Date: <u>2-7-2025</u></td> </tr> </table>	Signature of Owner	<u>Jennifer Walker</u>	Date: <u>2-7-2025</u>	Signature of Applicant	<u>Jennifer Walker</u>	Date: <u>2-7-2025</u>																																																																		
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Fees & Submittal Information

Appeal		250.00	
Certificate of Appropriateness			
ARB – single and two family residential		100.00	
ARB – All other residential or commercial		300.00	
ARB - Signage		75.00	
Conditional Use		600.00	
Development Plan – Preliminary PUD or Comprehensive			
Planning fee	First 10 acres	750.00	
	Each additional 5 acres or part thereof	50.00 / each	
Engineering fee	1-25 lots	155.00 / each	
	Minimum fee	1000.00	
Engineering fee	26 – 50 lots	3875.00	
	Each additional lot over 26	75.00 / each	
Engineering fee	Over 51 lots	5750.00	
	Each additional lot over 51	50.00 / each	
Development Plan – Final PUD			
Planning fee	First 10 acres	650.00	X
	Each additional 5 acres or part thereof	50.00	
Engineering fee	1-25 lots		
	(minimum fee \$1,000.00)	155.00 / each	
Engineering fee	26 – 50 lots	3875.00	
	Each additional lot over 26	75.00 / each	
Engineering fee	Over 51 lots	5750.00	
	Each additional lot over 51	50.00 / each	
Development Plan – Non-PUD		300.00	
Development Plan / Text Amendment		600.00	
Plat – Road Preliminary			
Planning fee		350.00	
Engineering fee	no lots on either side of street	1.00 / LF	
	lots on one side of street	.50 / LF	
	Minimum fee	1,000.00	
Plat – Road Final			
Planning fee		350.00	
Engineering fee	no lots on either side of street	1.00 / LF	
	lots on one side of street	.50 / LF	
	Minimum fee	1,000.00	
Plat – Subdivision Preliminary			
Planning		650.00	
	Plus each lot	50.00 / each	
Engineering fee	1-25 lots		
	(minimum fee \$1,000.00)	155.00 / each	
Engineering fee	26 – 50 lots	3875.00	
	Each lot over 26	75.00 / each	
Engineering fee	Over 51 lots	5750.00	
	Each lot over 51	50.00 / each	

Fees & Submittal Requirements

Plat – Subdivision Final			
Planning		650.00	_____
	Plus each lot	15.00 / each	_____
Engineering fee	1-25 lots		
	(minimum fee \$1,000.00)	155.00 /each	_____
Engineering fee	26-50 lots	3875.00	_____
	Each lot over 26	75.00 / each	_____
Engineering fee	Over 51 lots	5750.00	_____
	Each lot over 51	50.00 / each	_____
Lot Changes		200.00	_____
Minor Commercial Subdivision		200.00	_____
Vacation (Street or Easement)		1200.00	_____
Variance			
	Non-single family, commercial, subdivision, multiple properties	600.00	_____
	Single Family residence	250.00	_____
	In conjunction with Certification of Appropriateness	100.00	_____
Extension Request		0.00	_____
Zoning			
	Rezoning - First 10 acres	700.00	_____
	Each additional 5 acres or part thereof	50.00 / each	_____
	Rezoning to Rocky Fork Blacklick Accord	250.00	_____
	Text Modification	600.00	_____
Easement Encroachment		800.00	_____

DRAWING INDEX:

CU-CS01-S01	COVER SHEET	SHEET 1 OF 11
CU-EC02-S01	EXISTING CONDITIONS	SHEET 2 OF 11
CU-EC02-S02	DEMOLITION PLAN	SHEET 3 OF 11
CU-SL01-S01	SITE DIMENSION PLAN	SHEET 4 OF 11
CU-GP01-S01	GRADING / EROSION & SEDIMENT CONTROL PLAN	SHEET 5 OF 11
CU-GS01-S01	CROSS SECTIONS & DETAILS	SHEET 6 OF 11
CU-RP01-S01	ACCESS ROAD PROFILES & DETAILS	SHEET 7 OF 11
CU-ED02-S01	EROSION & SEDIMENT CONTROL DETAILS - SHEET 1	SHEET 8 OF 11
CU-ED02-S02	EROSION & SEDIMENT CONTROL DETAILS - SHEET 2	SHEET 9 OF 11
CU-GN02-S01	CITY OF NEW ALBANY STANDARD NOTES - SHEET 1	SHEET 10 OF 11
CU-GN02-S02	CITY OF NEW ALBANY STANDARD NOTES - SHEET 2	SHEET 11 OF 11

OWNER / APPLICANT:

AMERICAN ELECTRIC POWER
OHIO TRANSMISSION COMPANY
8500 SMITHS MILL ROAD
NEW ALBANY, OHIO 43054
CIVIL ENGINEER: KOKOU EKLOU
216-804-6741 CELL
KEKLOU@AEP.COM

SURVEY:

BAIR, GOODIE AND ASSOCIATES, INC.
153 NORTH BROADWAY STREET
NEW PHILADELPHIA, OHIO 44663
330-343-3499 OFFICE
330-343-9505 FAX

BENCHMARKS:

BM-300
1-1/2-INCH ALUMINUM MONUMENT IN CONCRETE (FOUND)
ELEV.: 1051.13'
NORTHING: 767,905.37'
EASTING: 1,882,961.68'

GEOTECHNICAL REPRESENTATIVE:

S&ME, INC.
6190 ENTERPRISE COURT
DUBLIN, OHIO 43016
614-793-2226 OFFICE
614-980-1093 MOBILE
WWW.SMEINC.COM

FLOODPLAIN NOTE:

SUBJECT PROPERTY IS LOCATED IN ZONE X (AREAS DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOODPLAIN) IN ACCORDANCE WITH THE FEMA FLOOD INSURANCE RATE MAP: FRANKLIN COUNTY, OHIO (AND INCORPORATED AREAS), PANEL 206 OF 465, MAP NUMBER: 39049C0206K, EFFECTIVE DATE: JUNE 17, 2008.

CITY OF NEW ALBANY APPROVALS

THE SIGNATURES BELOW SIGNIFY ONLY CONCURRENCE WITH THE GENERAL PURPOSE OF THIS PROJECT. ALL TECHNICAL DETAILS REMAIN THE RESPONSIBILITY OF THE ENGINEER AT BAIR, GOODIE & ASSOCIATES. THE EXTENT OF THE CITY ENGINEER REVIEW AND APPROVAL IS BASED ONLY ON COMPLIANCE WITH CITY ORDINANCE 1181, 1183, 1187, AND OTHER APPLICABLE CITY POLICIES.

FINANCE DIRECTOR, CITY OF NEW ALBANY, OHIO _____ DATE _____

CITY ENGINEER, CITY OF NEW ALBANY, OHIO _____ DATE _____

CITY MANAGER, CITY OF NEW ALBANY, OHIO _____ DATE _____

AMERICAN ELECTRIC POWER SOUDER STATION

7375 SOUDER ROAD
NEW ALBANY, OH 43054

LOCATED IN
LOT 16, SECTION 8, FIRST QUARTER OF T-2, R-16,
UNITED STATES MILITARY LANDS,
CITY OF NEW ALBANY,
COUNTY OF FRANKLIN,
STATE OF OHIO



LOCATION MAP

0 1/2 1
SCALE: 1" = 1/2 MILE

APPROVED NOTICE OF INTENT (NOI)

OHIO EPA FACILITY PERMIT NUMBER : 4GC10228*AG

POST CONSTRUCTION WATER QUALITY

A PERMANENT DRY EXTENDED DETENTION BASIN WILL BE CONSTRUCTED AS PART OF THIS PROJECT TO PROVIDE WATER QUALITY TREATMENT ONCE ALL DISTURBED AREAS HAVE BEEN SUCCESSFULLY VEGETATED .

GENERAL NOTES:

(1.) All work shall be performed in accordance with the appropriate articles of the AEP "Technical Specification for Substation and Switching Station Construction" #SS-160102 (**Specification**) and "Site Preparation Guidelines" #SS-710000.

(2.) The Cut and Fill Earthwork Quantities shown below reflect the minimum earthwork required. This calculation does not include 4" of #57 stone (by others) above the station pad or the removal of topsoil. The actual depth of stripping may increase under the station pad depending on site conditions.

(3.) All soft, wet, organic, or otherwise unsuitable material shall be removed and replaced in accordance with the **Specification**.

(4.) All disturbed areas that will not be stoned shall be seeded in accordance with the **Specification**.

(5.) Side slopes shall have a maximum gradient of three horizontal to one vertical unless otherwise noted.

(6.) The station pad area shall be covered with 8 inches of ODOT #304 aggregate to 5 feet outside the station fence followed by 4 inches of #57 stone (by others) to 4 feet outside the station fence.

(7.) The elevations shown for all graded areas are final elevations (top of pad #304). The contour interval shown is one foot.

(8.) All trees and shrubs located within the construction limits shall be removed. Trees and shrubs are not shown on the grading plan for clarity.

(9.) All debris shall be removed from the site.

(10.) All grading work shall be within property lines or the right of way line. Do not disturb adjacent properties.

(11.) Contractor is responsible for construction and maintenance of all erosion control measures.

(12.) Contractor initiated changes shall be submitted in writing to the owners representative for approval prior to fabrication or construction.

(13.) Scales as noted on the details are shown for 30"x46" size sheets.

(14.) Locations of all existing utilities shown on the plan are approximate. Contractor shall verify all existing utility locations prior to construction. Repair of any damaged utility shall be the responsibility of the contractor. Prior to excavation work, contractor shall have all utilities marked in the field.

(15.) The delineated wetlands as shown on these plans were provided by AEP.

(16.) A pre-construction meeting with the City of New Albany is required prior to any earth moving activity.

(17.) All concrete culverts to be tested per City of Columbus Construction and Material Specifications (CMSC) standards.

CONSTRUCTION LIMIT AREA / DISTURBED AREA = 7.38 ACRES (321,381 SQ. FT.)

STATION PAD AREA = 5.08 ACRES (221,375 SQ. FT.)

ESTIMATED QUANTITIES:

NOTE: QUANTITIES SHOWN ARE PROVIDED FOR REFERENCE ONLY. CONTRACTOR SHALL PERFORM THEIR OWN QUANTITY ESTIMATES.

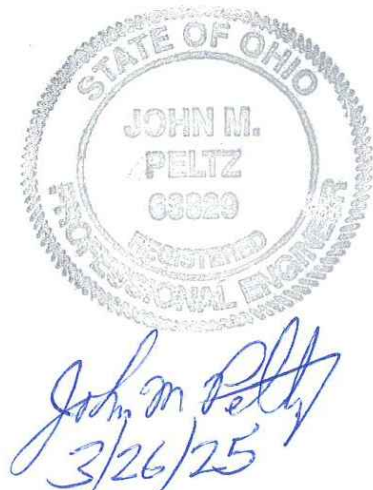
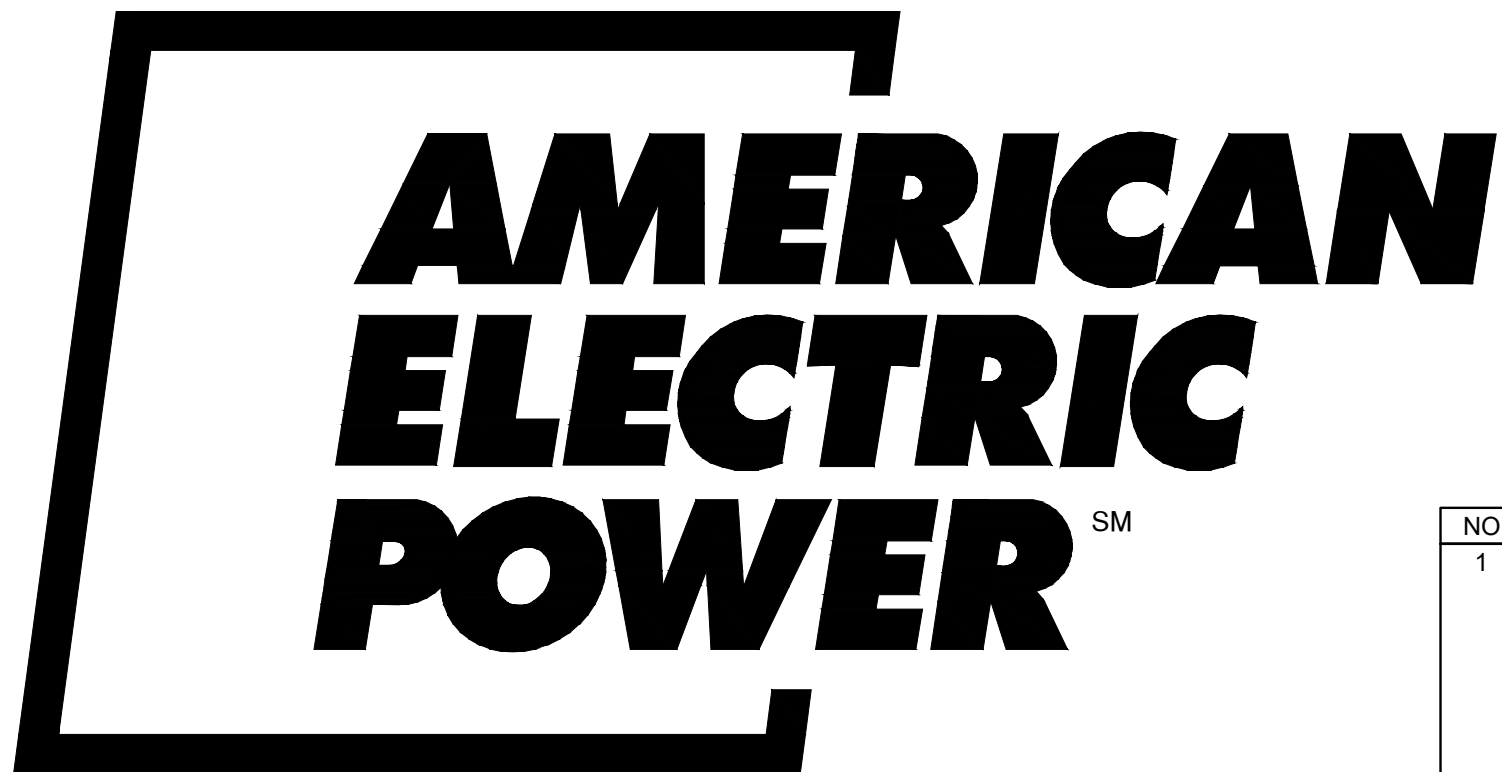
EARTHWORK:

AREA OF DISTURBANCE	7.38 AC.
CLEARING AND GRUBBING	7.38 AC.
TOPSOIL REMOVAL (ASSUMED 12" THICK)	11,900 C.Y.
TOPSOIL PLACEMENT (4" TO BE SPREAD IN AREAS TO BE SEEDED)	700 C.Y.
TOPSOIL (TO BE PLACED IN NORTH BERM)	1,220 C.Y.
TOPSOIL (EXCESS)	9,980 C.Y.
CUT* (INCLUDES 20% SWELL FACTOR)	20,700 C.Y.
FILL* (INCLUDES 30% COMPACTION)	12,000 C.Y.
EXPORT CUT	8,700 C.Y.

*CUT / FILL QUANTITIES DO NOT INCLUDE STRIPPING OR PLACEMENT OF TOPSOIL.

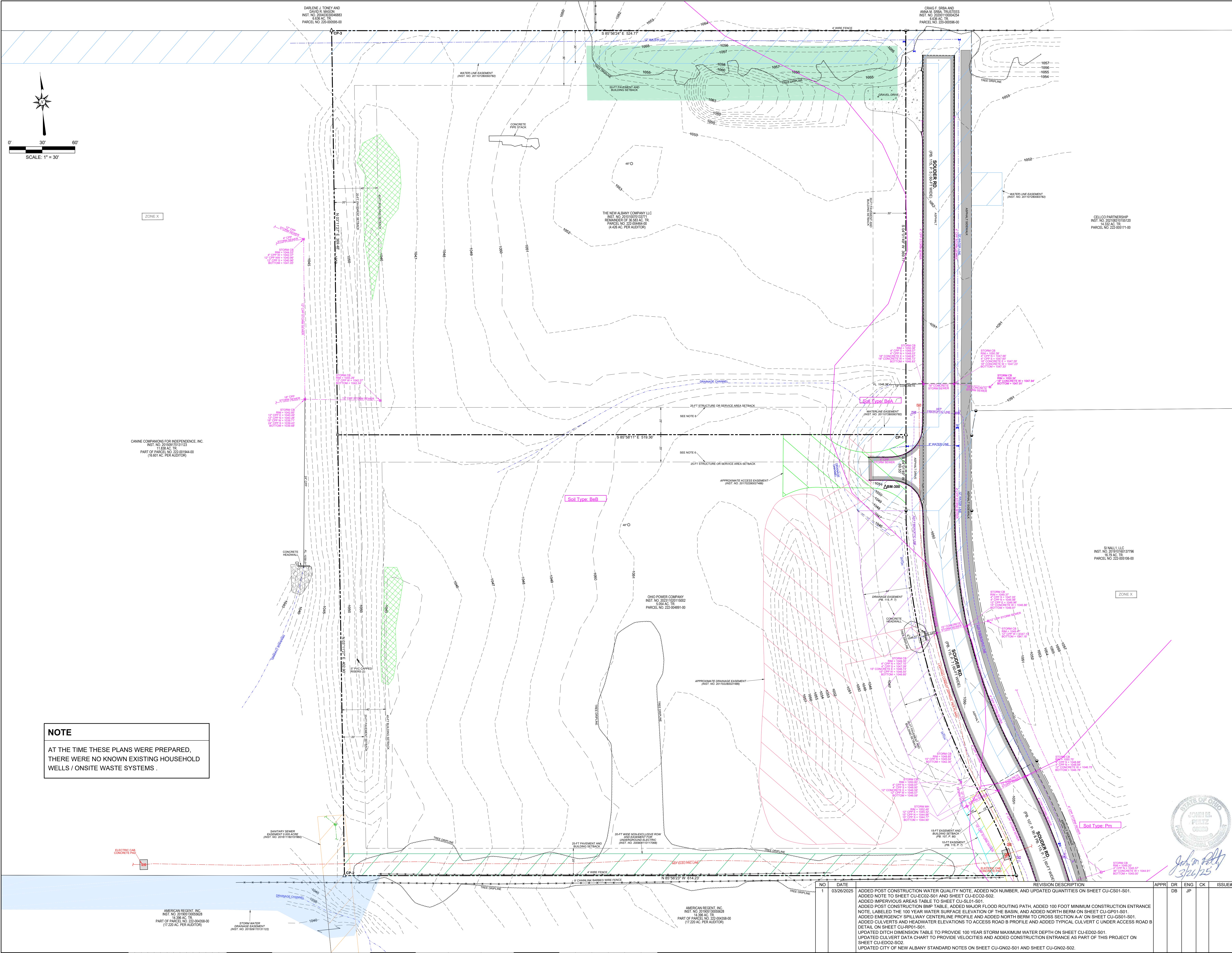
MATERIALS:

ODOT #304 AGGREGATE, 8" THICK FOR STATION PAD AND ASPHALT ACCESS ROADS AND 4-1/2" THICK FOR STONE ACCESS ROADS	11,200 TONS
AASHTO #57 WASHED LESTONE, 4" THICK FOR STATION PAD (BY OTHERS)	4,060 TONS
ODOT #2 AGGREGATE, 4-1/2" THICK FOR STONE ACCESS ROADS	50 TONS
ASPHALT PAVEMENT, 6" THICK FOR ACCESS ROADS (INSTALL PER ODOT SPECIFICATIONS)	55 C.Y.
ODOT TYPE (D) RIPRAP	2,100 TONS
AAHHTO #57 WASHED STONE FOR CULVERT INSTALLATION	560 TONS
ITEM 912 BACKFILL (TYPE I), 8 " THICK OVER CULVERT C	3 C.Y.
CHAIN LINK FENCE & APPURTENANCES	1,840 LIN. FT.
MIRAFI 600X GEOTEXTILE FABRIC FOR ACCESS ROAD	590 S.Y.
30 MIL HDPE OR PVC GEOMEMBRANE LINER	2,760 S.Y.
ODOT TYPE B GEOTEXTILE FABRIC FOR ROCK-LINED DITCHES	1,290 S.Y.
FILTER SOCK (12"Ø MIN.)	2,420 LIN. FT.
24" SWINGING GATE	2 EA.
4' X 4' AA-S133B CATCH BASIN	1 EA.
12" HDPE PIPE	80 LIN. FT.
12" REINFORCED CONCRETE PIPE (RCP)	80 LIN. FT.
18" REINFORCED CONCRETE PIPE (RCP)	60 LIN. FT.
FAIRCLOTH SKIMMER	1 EA.
SEEDING & MULCHING	1.33 AC.
CONCRETE WASHOUT	1 EA.
CONSTRUCTION ENTRANCE	1 EA.



NO	DATE	REVISION DESCRIPTION	APPR	DR	ENG	CK	ISSUE#
1	03/26/2025	ADDED POST CONSTRUCTION WATER QUALITY NOTE, ADDED NOI NUMBER, AND UPDATED QUANTITIES ON SHEET CU-CS01-S01. ADDED NOTE TO SHEET CU-EC02-S01 AND SHEET CU-EC02-S02. ADDED IMPERVIOUS AREAS TABLE TO SHEET CU-SL01-S01. ADDED POST CONSTRUCTION BMP TABLE, ADDED MAJOR FLOOD ROUTING PATH, ADDED 100 FOOT MINIMUM CONSTRUCTION ENTRANCE NOTE, LABELED THE 100 YEAR WATER SURFACE ELEVATION OF THE BASIN, AND ADDED NORTH BERM ON SHEET CU-GP01-S01. ADDED EMERGENCY SPILLWAY CENTERLINE PROFILE AND ADDED NORTH BERM TO CROSS SECTION A-A ON SHEET CU-GS01-S01. ADDED CULVERTS AND HEADWATER ELEVATIONS TO ACCESS ROAD B PROFILE AND ADDED TYPICAL CULVERT C UNDER ACCESS ROAD B DETAIL ON SHEET CU-RP01-S01. UPDATED DITCH DIMENSION TABLE TO PROVIDE 100 YEAR STORM MAXIMUM WATER DEPTH ON SHEET CU-ED02-S01. UPDATED CULVERT DATA CHART TO PROVIDE VELOCITIES AND ADDED CONSTRUCTION ENTRANCE AS PART OF THIS PROJECT ON SHEET CU-ED02-S02. UPDATED CITY OF NEW ALBANY STANDARD NOTES ON SHEET CU-GN02-S01 AND SHEET CU-GN02-S02.					

BAIR GOODIE	BAIR, GOODIE AND ASSOCIATES, INC. 153 NORTH BROADWAY STREET NEW PHILADELPHIA, OH 44663 TEL: 330.343.3499 FAX: 330.343.9505 WWW.BAIRGOODIE.COM	UNDERGROUND UTILITIES TWO WORKING DAYS CALL BEFORE YOU DIG Call: 800-362-2754 (Toll Free) OHIO UTILITIES PROTECTION SERVICE
OLD DWG #:	STD DWG #:	
*THIS DRAWING IS THE PROPERTY OF AMERICAN ELECTRIC POWER AND IS LOANED UPON CONDITION THAT IT IS NOT TO BE COPIED OR REPRODUCED, IN WHOLE OR IN PART, OR USED FOR FURNISHING INFORMATION TO ANY PERSON WITHOUT THE WRITTEN CONSENT OF AMERICAN ELECTRIC POWER, OR FOR ANY PURPOSE DETRIMENTAL TO THEIR INTEREST, AND IS TO BE RETURNED UPON REQUEST.		
AEP OHIO TRANSMISSION COMPANY, INC.		
NEW ALBANY OHIO		
COVER SHEET		
SCALE: AS NOTED	DR: DB/GA	ENG: JP/BGA
WOF: T10593117002	APPD: JP/BGA	CH: JP/BGA
1 RIVERSIDE PLAZA COLUMBUS, OH 43215	DWG. NO. CU-CS01-S01	R 1 E V



NOTE

AT THE TIME THESE PLANS WERE PREPARED, THERE WERE NO KNOWN EXISTING HOUSEHOLD WELLS / ONSITE WASTE SYSTEMS .

SURVEY CONTROL POINT DATA			
POINT NO.	DESCRIPTION	NORTHING	EASTING
BM-300	1-1/2 INCH ALUMINUM MONUMENT IN CONCRETE (FOUND)	767902.37	1882961.89
CP-1	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)	767962.05	1882963.94
CP-2	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)	767589.36	1882443.43
CP-3	13/16 INCH ID IRON PIPE WITH "EMT INC." CAP (FOUND)	768357.51	1882486.40

LEGEND - EXISTING FEATURES	
	SUBJECT PROPERTY
	PROPERTY LINE PER COUNTY
	PARCEL LINE
	MINIMUM SETBACK LINE (AS NOTED)
	EDGE OF ROAD
	CONCRETE CURB
	EDGE OF GRAVEL / DRIVE
	FENCE
	1FT CONTOUR
	5FT CONTOUR
	UNDERGROUND ELECTRIC LINE
	ELECTRIC VAULT (UNLESS OTHERWISE NOTED)
	UNDERGROUND FIBEROPTIC LINE
	FIBEROPTIC VAULT
	FIBEROPTIC MARKER
	UNDERGROUND PIPELINE
	PIPELINE MARKER
	UNDERGROUND WATER LINE
	WATER VALVE
	FIRE HYDRANT
	UNDERGROUND SANITARY SEWER
	SANITARY MANHOLE
	STORM MANHOLE
	CATCH BASIN
	CORRUGATED PLASTIC PIPE
	CULVERT
	FLOW LINE ELEVATION
	DRAINAGE CHANNEL / DITCH
	EDGE OF WATER
	TREE DRIPLINE
	TREE
	CONTROL POINT / BENCHMARK (AS NOTED)
	13/16-INCH ID IRON PIPE WITH "EMT INC." CAP (FOUND)
	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)
	DELINEATED WETLAND
	RIPRAP
	GRAVEL
	CONCRETE
	ASPHALT
	WATER LINE EASEMENT (INST. NO. 2010728003782)
	APPROXIMATE ACCESS EASEMENT (INST. NO. 201702280027486)
	APPROXIMATE DRAINAGE EASEMENT (INST. NO. 201702280027486)
	10-FT EASEMENT (PB. 115, P. 7)
	44-FT DRAINAGE EASEMENT (PB. 115, P. 7)
	15-FT EASEMENT AND BUILDING SETBACK (PB. 107, P. 90)
	SANITARY SEWER EASEMENT 0.005 ACRE (INST. NO. 201611160157860)
	20-FT WIDE NON-EXCLUSIVE ROW AND EASEMENT FOR UNDERGROUND ELECTRIC (INST. NO. 200908110117068)
	LANDSCAPING IN SETBACK FROM NORTHERN PROPERTY LINE: SEE SOUDER EAST RAI DISTRICT SUBAREA 4 ZONING TEXT ITEM H(4) DATED JULY 9, 2008
	STORM WATER DRAINAGE EASEMENT (INST. NO. 201509170131122)

LEGEND - FEMA FLOOD INSURANCE RATE MAP FEATURES	
ZONE X	ZONE X - OTHER AREAS, AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN.

LEGEND - SOIL TYPES	
BeA	Bennington silt loam, 0 to 2 percent slopes
BeB	Bennington silt loam, 2 to 6 percent slopes
Pm	Pewamo silty clay loam, low carbonate till, 0 to 2 percent slopes

- NOTES**
- 1) BEARINGS ARE ORIENTED TO THE STATE PLANE COORDINATE SYSTEM: OHIO SOUTH (3402); HORIZONTAL DATUM: NAD83 (2011); U.S. SURVEY FOOT; VERTICAL DATUM: NAVD83.
 - 2) UTILITIES LOCATED IN THE FIELD BY OBSERVED EVIDENCE COMBINED WITH SOURCE INFORMATION FROM PLANS AND MARKINGS (IF PROVIDED). HOWEVER, LACKING EXCAVATION, THE EXACT LOCATION OF UNDERGROUND FEATURES CANNOT BE ACCURATELY, COMPLETELY, AND RELIABLY DEPICTED. IN ADDITION, IN SOME JURISDICTIONS, 811 OR OTHER SIMILAR UTILITY LOCATE REQUESTS FROM SURVEYORS MAY BE IGNORED OR RESULT IN AN INCOMPLETE RESPONSE.
 - 3) DOCUMENTS USED: TAX MAPS, RECORDS AS NOTED, AND PLATS OF SURVEYS.
 - 4) SUBJECT PROPERTY IS LOCATED IN ZONE X AREAS DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOODPLAIN IN ACCORDANCE WITH THE FEMA FLOOD INSURANCE RATE MAP, FRANKLIN COUNTY, OHIO (AND INCORPORATED AREAS), PANEL 206 OF 465, MAP NUMBER: 39040C0206K, EFFECTIVE DATE: JUNE 17, 2008.
 - 5) THE APPROXIMATE LOCATION OF AN ACCESS EASEMENT AND DRAINAGE EASEMENT ARE DEPICTED AS SHOWN ON EXHIBITS A-1 AND A-2 OF INST. NO. 201702280027486.
 - 6) THE CITY OF NEW ALBANY HAS CONFIRMED THAT IN THE EVENT THE TWO PARCELS SHOWN AS THE "SUBJECT PROPERTY" COME UNDER COMMON OWNERSHIP OR CONTROL, THE MINIMUM SETBACKS SHALL NO LONGER APPLY TO THE COMMON PARCEL LINE AS NOTED AND SHOWN HEREON.
 - 7) EXISTING CONDITIONS SURVEY COMPLETED ON FEBRUARY 13, 2024.

BAIR GOODIE

BAIR, GOODIE AND ASSOCIATES, INC.
153 NORTH BROADWAY STREET
NEW PHILADELPHIA, OH 44663
TEL: 330.343.3499 FAX: 330.343.9505
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AEP OHIO TRANSMISSION COMPANY, INC.

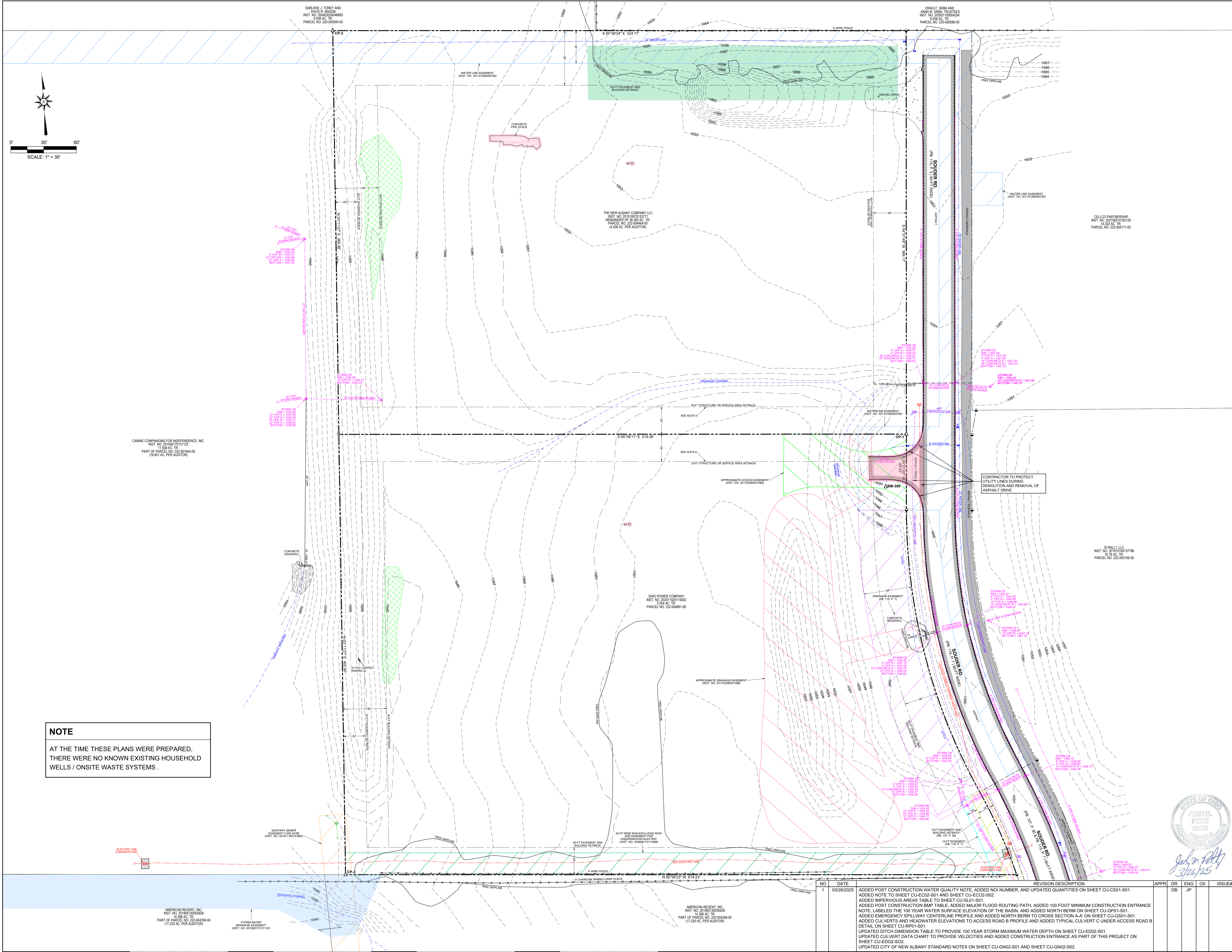
SOUDER STATION

NEW ALBANY OHIO

EXISTING CONDITIONS

SCALE: 1" = 30'

DR: DB/GA	ENG: JP/BGA	CH: JP/BGA
WO#: T10593117002	APPD: JP/BGA	DATE: 09/11/2024
1 RIVERSIDE PLAZA COLUMBUS, OH 43215	DWG. NO.: CU-EC02-S01	R 1



NOTE

AT THE TIME THESE PLANS WERE PREPARED, THERE WERE NO KNOWN EXISTING HOUSEHOLD WELLS / ONSITE WASTE SYSTEMS .

SURVEY CONTROL POINT DATA			
POINT NO.	DESCRIPTION	NORTHING	EASTING
BM-300	1-1/2-INCH ALUMINUM MONUMENT IN CONCRETE (FOUND)	767955.37	1882961.68
CP-1	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)	767952.99	1882963.94
CP-2	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)	767956.38	1882443.43
CP-3	13/16-INCH ID IRON PIPE WITH "EMHT INC." CAP (FOUND)	768357.51	1882486.40

LEGEND - EXISTING FEATURES	
---	SUBJECT PROPERTY
---	PROPERTY LINE PER COUNTY
---	PARCEL LINE
---	MINIMUM SETBACK LINE (AS NOTED)
---	EDGE OF ROAD
---	CONCRETE CURB
---	EDGE OF GRAVEL / DRIVE
---	FENCE
---	1FT CONTOUR
---	5FT CONTOUR
---	UNDERGROUND ELECTRIC LINE
---	ELECTRIC VAULT (UNLESS OTHERWISE NOTED)
---	UNDERGROUND FIBEROPTIC LINE
---	FIBEROPTIC VAULT
---	FIBEROPTIC MARKER
---	UNDERGROUND PIPELINE
---	PIPELINE MARKER
---	UNDERGROUND WATER LINE
---	WATER VALVE
---	FIRE HYDRANT
---	UNDERGROUND SANITARY SEWER
---	SANITARY MANHOLE
---	UNDERGROUND STORM SEWER
---	STORM MANHOLE
---	CATCH BASIN
---	CORRUGATED PLASTIC PIPE
---	CULVERT
---	FLOW LINE ELEVATION
---	DRAINAGE CHANNEL / DITCH
---	EDGE OF WATER
---	TREE DRIPLINE
---	CONTROL POINT / BENCHMARK (AS NOTED)
---	13/16-INCH ID IRON PIPE WITH "EMHT INC." CAP (FOUND)
---	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)
---	DELINEATED WETLAND
---	RIPRAP
---	GRAVEL
---	CONCRETE
---	ASPHALT
---	WATER LINE EASEMENT (INST. NO. 20107280093782)
---	APPROXIMATE ACCESS EASEMENT (INST. NO. 201702280027486)
---	APPROXIMATE DRAINAGE EASEMENT (INST. NO. 201702280027486)
---	10-FT EASEMENT (PB. 115, P. 7)
---	44-FT DRAINAGE EASEMENT (PB. 115, P. 7)
---	15-FT EASEMENT AND BUILDING SETBACK (PB. 107, P. 90)
---	SANITARY SEWER EASEMENT 0.005 ACRE (INST. NO. 201611160157860)
---	20-FT WIDE NON-EXCLUSIVE ROW AND EASEMENT FOR UNDERGROUND ELECTRIC (INST. NO. 200908110117068)
---	LANDSCAPING IN SETBACK FROM NORTHERN PROPERTY LINE: SEE SOUDER EAST R&I DISTRICT SUBAREA 4 ZONING TEXT ITEM H(4) DATED JULY 9, 2008.
---	STORM WATER DRAINAGE EASEMENT (INST. NO. 201509170131122)

LEGEND - PROPOSED FEATURES	
---	PROPOSED DEMOLITION / REMOVE ITEMS

- NOTES**
- 1) BENCHMARKS ARE ORIENTED TO THE STATE PLANE COORDINATE SYSTEM: OHIO SOUTH (3402); HORIZONTAL DATUM: NAD83 (2011); U.S. SURVEY FOOT; VERTICAL DATUM: NAVD83.
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 - 3) DOCUMENTS USED: TAX MAPS, RECORDS AS NOTED, AND PLATS OF SURVEYS.
 - 4) SUBJECT PROPERTY IS LOCATED IN ZONE X AREAS DETERMINED TO BE OUTSIDE OF THE 0.2% ANNUAL CHANCE FLOODPLAIN IN ACCORDANCE WITH THE FEMA FLOOD INSURANCE RATE MAP: FRANKLIN COUNTY, OHIO (AND INCORPORATED AREAS), PANEL 206 OF 465, MAP NUMBER: 30049C0205K, EFFECTIVE DATE: JUNE 17, 2008.
 - 5) THE APPROXIMATE LOCATION OF AN ACCESS EASEMENT AND DRAINAGE EASEMENT ARE DEPICTED AS SHOWN ON EXHIBITS A-1 AND A-2 OF INST. NO. 201702280027486.
 - 6) THE CITY OF NEW ALBANY HAS CONFIRMED THAT IN THE EVENT THE TWO PARCELS SHOWN AS THE "SUBJECT PROPERTY" COME UNDER COMMON OWNERSHIP OR CONTROL, THE MINIMUM SETBACKS SHALL NO LONGER APPLY TO THE COMMON PARCEL LINE AS NOTED AND SHOWN HEREON.
 - 7) EXISTING CONDITIONS SURVEY COMPLETED ON FEBRUARY 13, 2024.

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AEP OHIO TRANSMISSION COMPANY, INC.

SOUDER STATION

NEW ALBANY OHIO

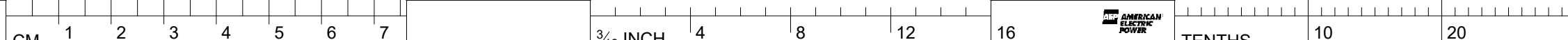
DEMOLITION PLAN

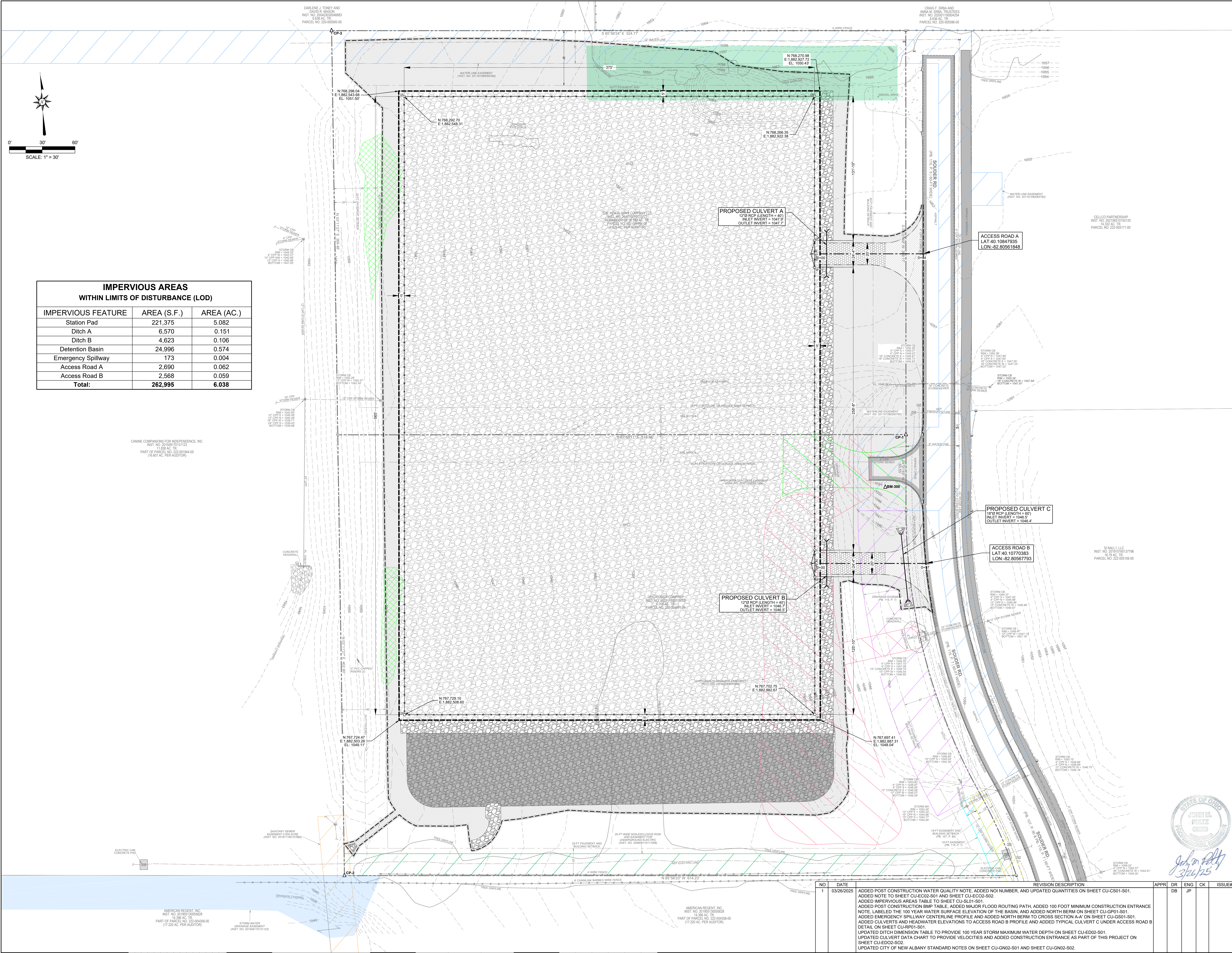
SCALE: 1" = 30'

DR: DR/BGA
WOF: T10593117002
1 RIVERSIDE PLAZA
COLUMBUS, OH 43215

ENG: JPB/GA
APPD: JPB/GA
DWG. NO.: CU-EC02-S02

CH: JPB/GA
DATE: 09/11/2024
R
1





IMPERVIOUS AREAS WITHIN LIMITS OF DISTURBANCE (LOD)		
IMPERVIOUS FEATURE	AREA (S.F.)	AREA (AC.)
Station Pad	221,375	5.082
Ditch A	6,570	0.151
Ditch B	4,623	0.106
Detention Basin	24,996	0.574
Emergency Spillway	173	0.004
Access Road A	2,690	0.062
Access Road B	2,568	0.059
Total:	262,995	6.038

SURVEY CONTROL POINT DATA			
POINT NO.	DESCRIPTION	NORTHING	EASTING
CP-1	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)	787992.37	1882981.68
CP-2	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)	787992.09	1882983.94
CP-3	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)	787988.38	1882443.43
CP-4	13/16-INCH ID IRON PIPE WITH "EMT INC." CAP (FOUND)	788387.51	1882486.40

LEGEND - EXISTING FEATURES	
	SUBJECT PROPERTY
	PROPERTY LINE
	PARCEL LINE
	MINIMUM SETBACK LINE (AS NOTED)
	EDGE OF ROAD
	CONCRETE CURB
	EDGE OF GRAVEL / DRIVE
	FENCE
	1049 FT CONTOUR
	1050 FT CONTOUR
	250 SFT CONTOUR
	UNDERGROUND ELECTRIC LINE
	ELECTRIC VAULT (UNLESS OTHERWISE NOTED)
	UNDERGROUND FIBEROPTIC LINE
	FIBEROPTIC MARKER
	UNDERGROUND PIPELINE
	PIPELINE MARKER
	UNDERGROUND WATER LINE
	WATER VALVE
	FIRE HYDRANT
	UNDERGROUND SANITARY SEWER
	SANITARY MANHOLE
	UNDERGROUND STORM SEWER
	STORM MANHOLE
	CATCH BASIN
	CORRUGATED PLASTIC PIPE
	CULVERT
	FLOW LINE ELEVATION
	DRAINAGE CHANNEL / DITCH
	EDGE OF WATER
	TREE DRIPLINE
	TREE
	CONTROL POINT / BENCHMARK (AS NOTED)
	13/16-INCH ID IRON PIPE WITH "EMT INC." CAP (FOUND)
	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)
	DELINEATED WETLAND
	RIPRAP
	GRAVEL
	CONCRETE
	ASPHALT
	WATER LINE EASEMENT (INST. NO. 20110728003782)
	APPROXIMATE ACCESS EASEMENT (INST. NO. 201702280027486)
	APPROXIMATE DRAINAGE EASEMENT (INST. NO. 201702280027486)
	10-FT EASEMENT (PB. 115, P. 7)
	44-FT DRAINAGE EASEMENT (PB. 115, P. 7)
	15-FT EASEMENT AND BUILDING SETBACK (PB. 107, P. 90)
	SANITARY SEWER EASEMENT 0.005 ACRE (INST. NO. 20161110157800)
	20-FT WIDE NON-EXCLUSIVE ROW AND EASEMENT FOR UNDERGROUND ELECTRIC (INST. NO. 20090811017068)
	LANDSCAPING IN SETBACK FROM NORTHERN PROPERTY LINE: SEE SOUDER EAST RAIL DISTRICT SUBAREA 4 ZONING TEXT ITEM H(4) DATED JULY 9, 2008.
	STORM WATER DRAINAGE EASEMENT (INST. NO. 20150817013122)

LEGEND - PROPOSED FEATURES	
	PROPOSED EDGE OF PAD
	PROPOSED FENCE
	PROPOSED CORNER POST / GATE POST
	PROPOSED CENTERLINE OF ACCESS ROAD
	PROPOSED EDGE OF ACCESS ROAD
	PROPOSED CONSTRUCTION LIMITS - 7.38 ACRES
	PROPOSED CULVERT
	PROPOSED SWINGING GATE
	PROPOSED STATION PAD STONE
	PROPOSED STONE ACCESS ROAD
	PROPOSED ASPHALT ACCESS ROAD
	PROPOSED SEEDING AREA
	PROPOSED 30 MIL HDPE OR PVC GEOMEMBRANE LINER
	PROPOSED ODOT TYPE (D) RIPRAP

- NOTES**
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AEP OHIO TRANSMISSION COMPANY, INC.

NEW ALBANY

OHIO

SITE DIMENSION PLAN

SCALE: 1" = 30'

DR: DB/GA

ENG: JP/BGA

CH: JP/BGA

WOF: T10593117002

APPD: JP/BGA

DATE: 09/11/2024

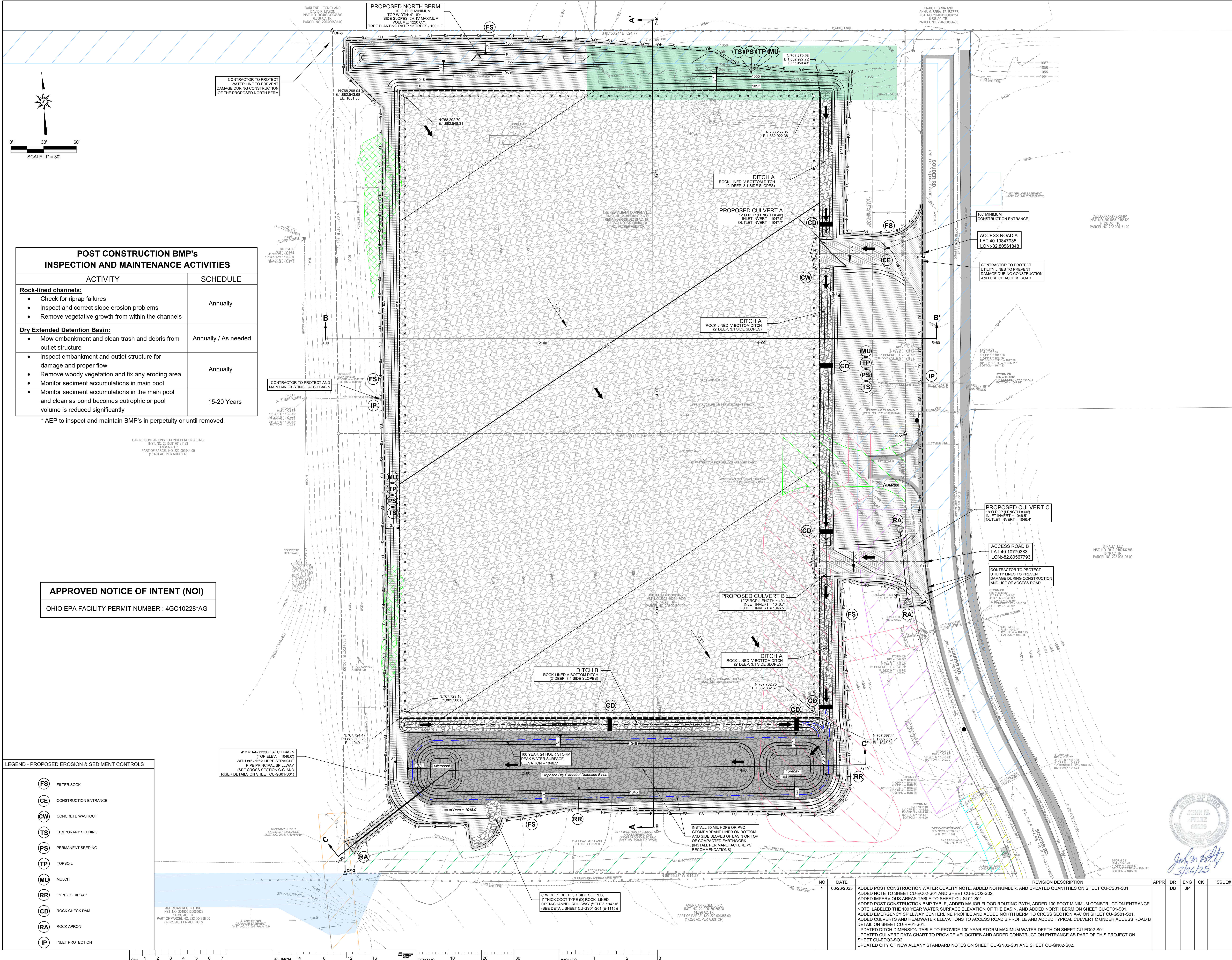
1 RIVERSIDE PLAZA
COLUMBUS, OH 43215

DWG. NO.: CU-SL01-S01

R 1

V 1

(E-1113)



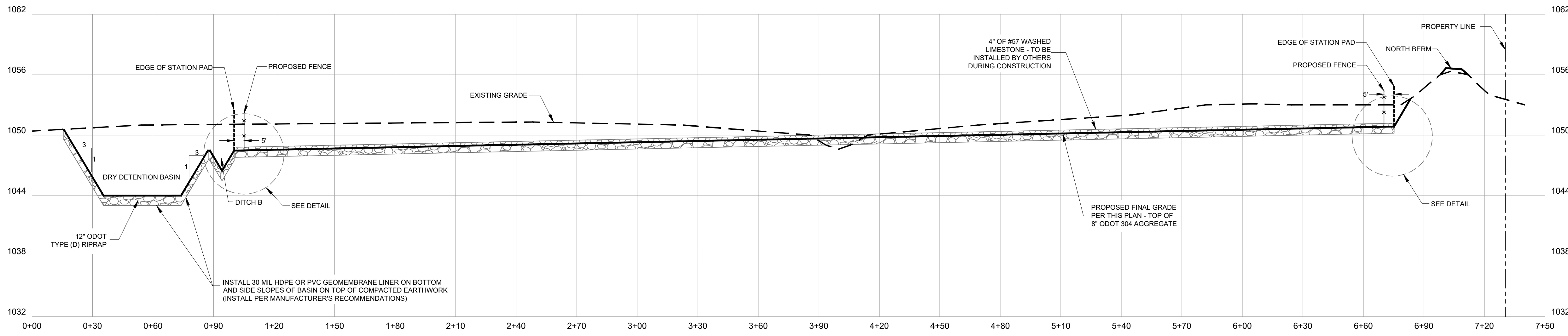
SURVEY CONTROL POINT DATA				NORTHING	EASTING	ELEVATION
BM-300	1-1/2-INCH ALUMINUM MONUMENT IN CONCRETE (FOUND)			767905.37	1862061.88	1051.13
CP-1	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)			767962.05	1862863.94	N/A
CP-2	IRON PIN WITH "CENTRAL SURV. CO. LTD" CAP (FOUND)			767966.36	1862443.43	N/A
CP-3	13/16-INCH ID IRON PIPE WITH "EMT INC." CAP (FOUND)			768027.31	1862466.40	N/A

LEGEND - EXISTING FEATURES	
	SUBJECT PROPERTY
	PROPERTY LINE
	PROPERTY LINE PER COUNTY
	PARCEL LINE
	MINIMUM SETBACK LINE (AS NOTED)
	EDGE OF ROAD
	CONCRETE CURB
	EDGE OF GRAVEL / DRIVE
	1'FT CONTOUR
	5'FT CONTOUR
	UNDERGROUND ELECTRIC LINE
	ELECTRIC VAULT (UNLESS OTHERWISE NOTED)
	UNDERGROUND FIBEROPTIC LINE
	FIBEROPTIC VAULT
	FIBEROPTIC MARKER
	UNDERGROUND PIPELINE
	PIPELINE MARKER
	UNDERGROUND WATER LINE
	WATER VALVE
	FIRE HYDRANT
	UNDERGROUND SANITARY SEWER
	SANITARY MANHOLE
	UNDERGROUND STORM SEWER
	STORM MANHOLE
	CATCH BASIN
	CORRUGATED PLASTIC PIPE CULVERT
	FLOW LINE ELEVATION
	DRAINAGE CHANNEL / DITCH
	EDGE OF WATER
	TREE DRIPLINE
	TREE
	CONTROL POINT / BENCHMARK (AS NOTED)
	13/16-INCH ID IRON PIPE WITH "EMT INC." CAP (FOUND)
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	WATER LINE EASEMENT (INST. NO. 20110728003782)
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	20-FT WIDE NON-EXCLUSIVE ROW AND EASEMENT FOR UNDERGROUND ELECTRIC (INST. NO. 20090810117068)
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	STORM WATER DRAINAGE EASEMENT (INST. NO. 201509170131122)

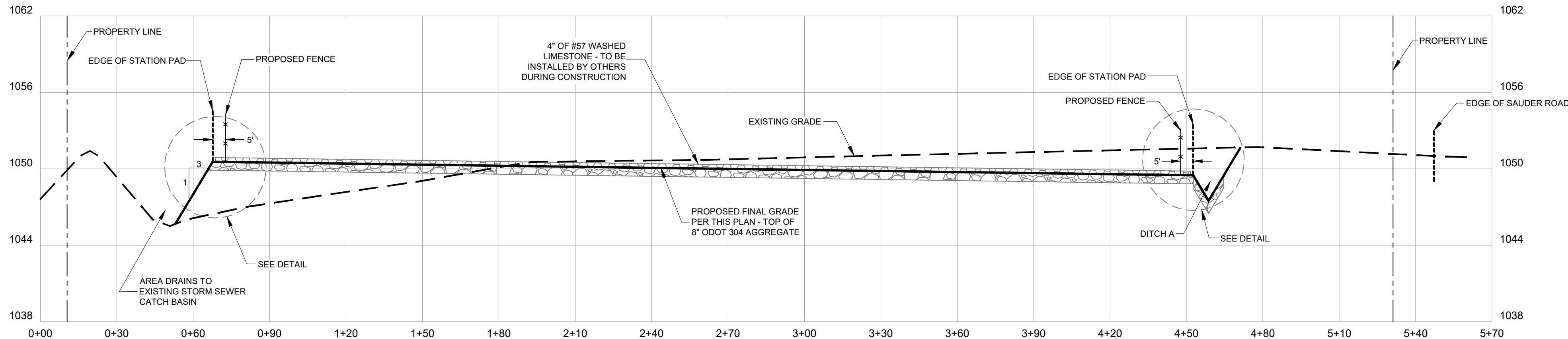
LEGEND - PROPOSED FEATURES	
	PROPOSED 1' CONTOUR
	PROPOSED 5' CONTOUR
	PROPOSED EDGE OF PAD
	PROPOSED FENCE
	PROPOSED CORNER POST / GATE POST
	PROPOSED CENTERLINE OF ACCESS ROAD
	PROPOSED EDGE OF ACCESS ROAD
	PROPOSED ROCK-LINED DITCH
	PROPOSED CONSTRUCTION LIMITS - 7.38 ACRES
	PROPOSED CULVERT
	PROPOSED SWINGING GATE
	PROPOSED ROCK CHECK DAM
	PROPOSED SLOPE INDICATOR
	PROPOSED FILTER SOCK
	PROPOSED CONSTRUCTION ENTRANCE AHEAD SIGN
	MAJOR FLOOD ROUTING PATH
	PROPOSED STATION PAD STONE
	PROPOSED STONE ACCESS ROAD
	PROPOSED ASPHALT ACCESS ROAD
	PROPOSED SEEDING AREA
	PROPOSED 30 MIL HDPE OR PVC GEOMEMBRANE LINER
	PROPOSED ODOT TYPE (D) RIPRAP

NOTES	
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7) EXISTING CONDITIONS SURVEY COMPLETED ON FEBRUARY 13, 2024.	
8) FILTER SOCK WILL BE PLACED AT THE ACTUAL CONSTRUCTION LIMITS AND IS SHOWN OFFSET ON THE PLANS FOR CLARITY ONLY.	
9) IF REGULATORY AGENCIES REQUIRE SILT FENCE INSTEAD OF FILTER SOCK, INSTALL PER STANDARD DETAILS.	

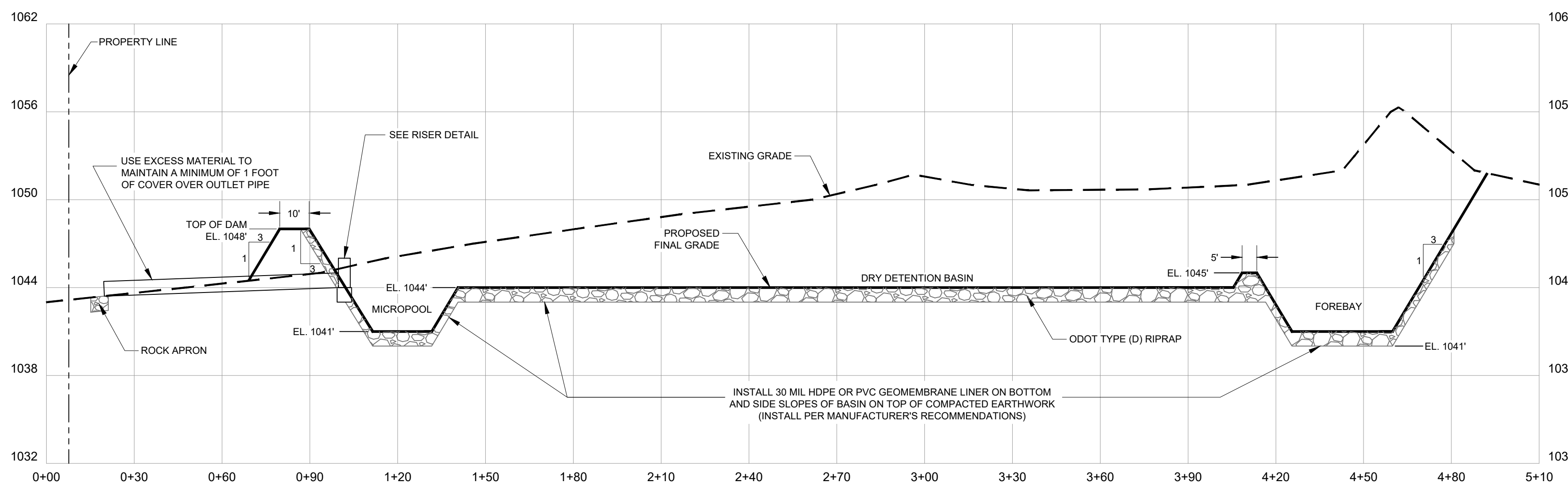
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OLD DWG #:	STD DWG #:	
AEP OHIO TRANSMISSION COMPANY, INC.		
SOUDER STATION		
NEW ALBANY OHIO		
GRADING / EROSION & SEDIMENT CONTROL PLAN		
SCALE: 1" = 30'	DR: DB/GA	ENG: JP/BGA
	WOF: T10593117002	APPD: JP/BGA
	1 RIVERSIDE PLAZA	DWG. NO. CU-GP01-S01
	COLUMBUS, OH 43215	
		R 1
		V 1



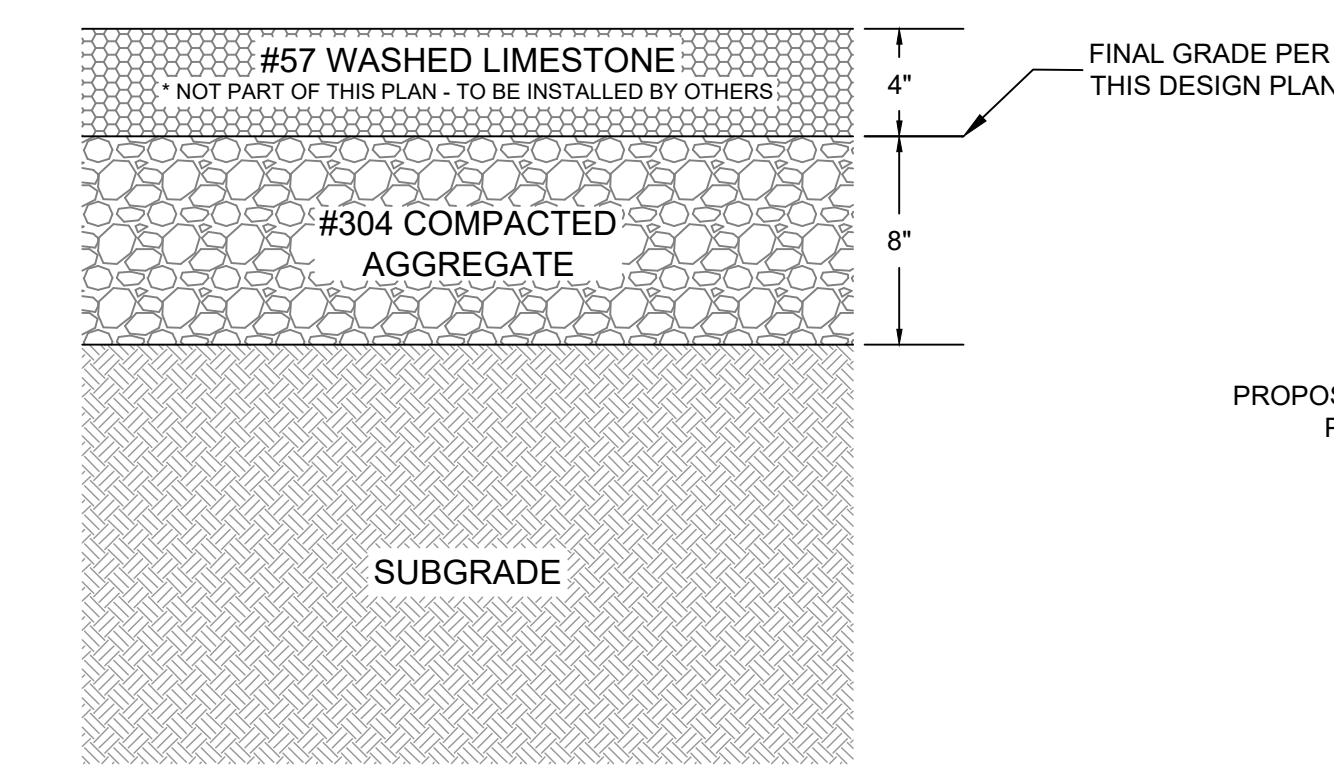
CROSS-SECTION A-A'
HORIZONTAL SCALE: 1" = 30'
VERTICAL SCALE: 1" = 6'



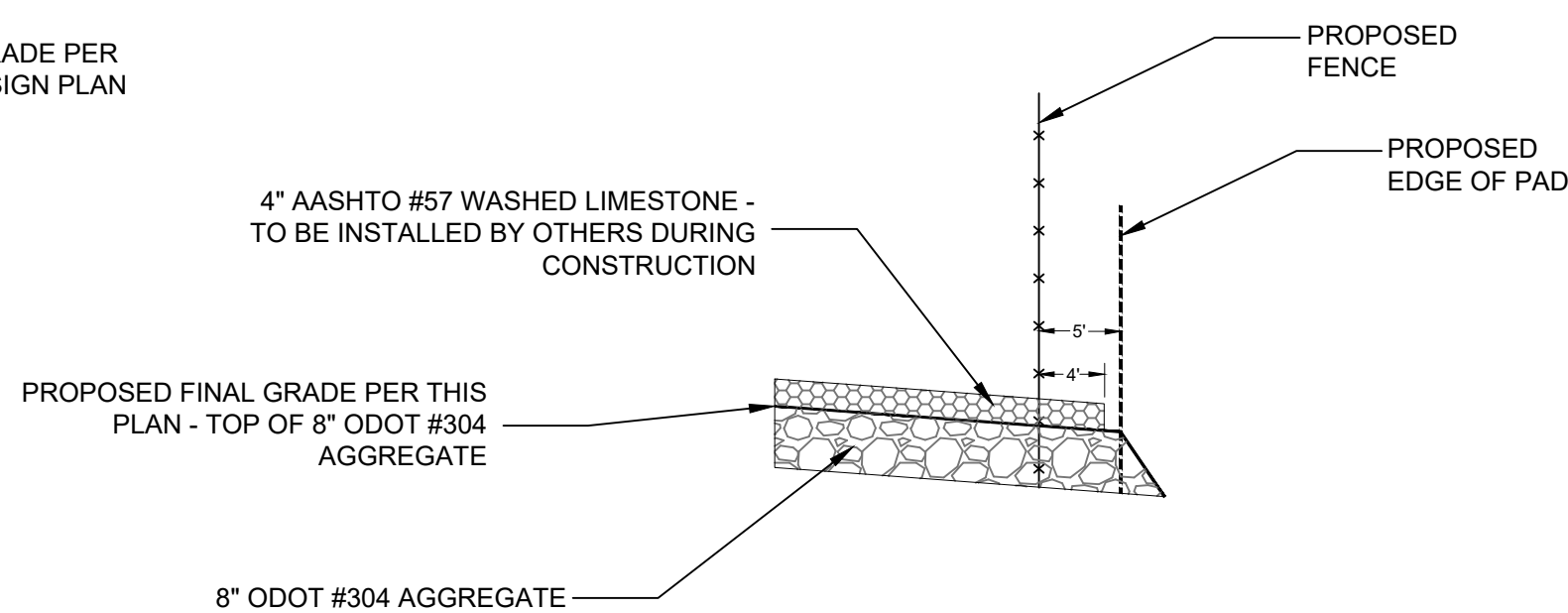
CROSS-SECTION B-B'
HORIZONTAL SCALE: 1" = 30'
VERTICAL SCALE: 1" = 6'



CROSS-SECTION C-C'
HORIZONTAL SCALE: 1" = 30'
VERTICAL SCALE: 1" = 6'



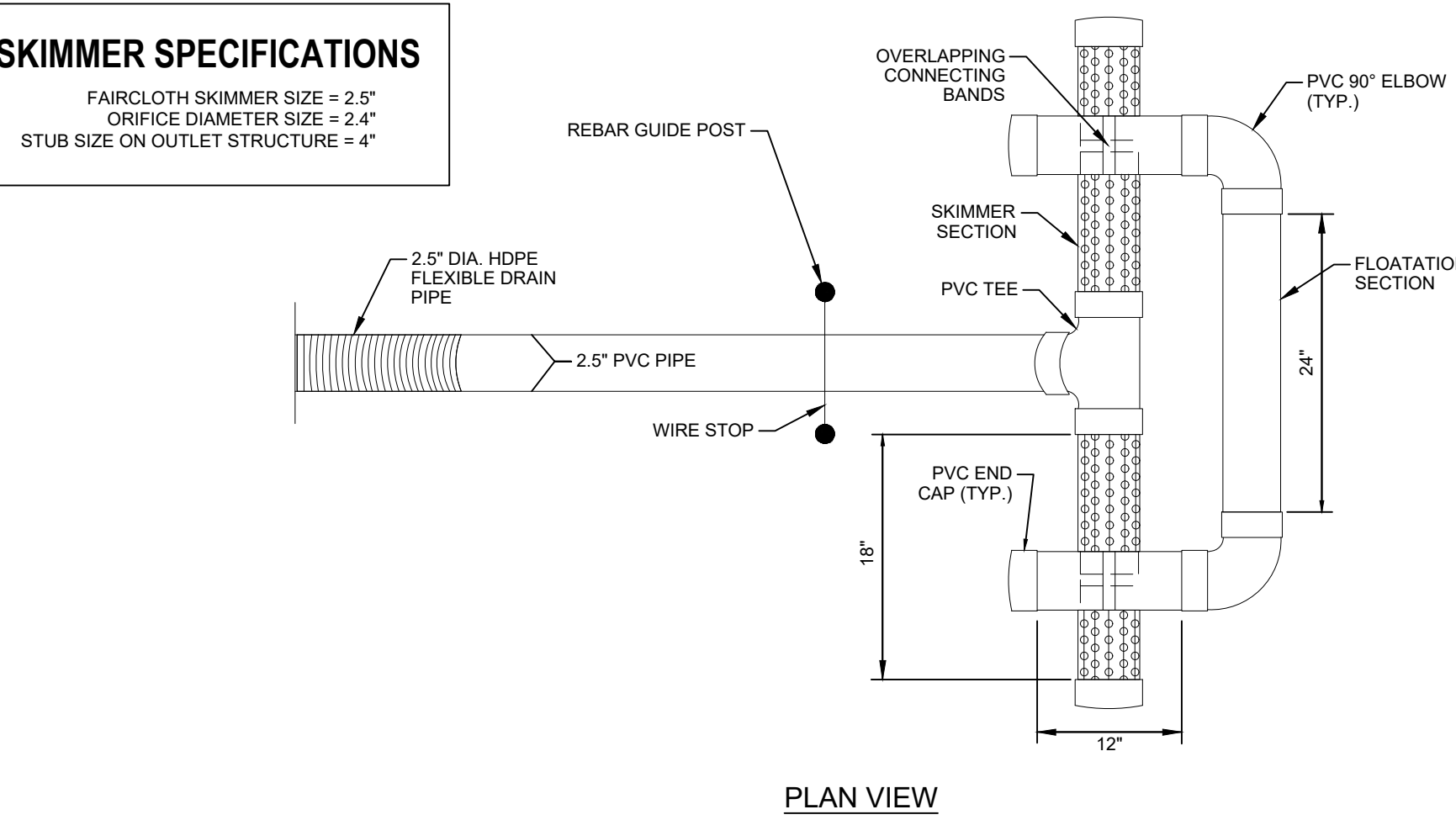
TYPICAL STATION PAD DETAIL
NOT TO SCALE



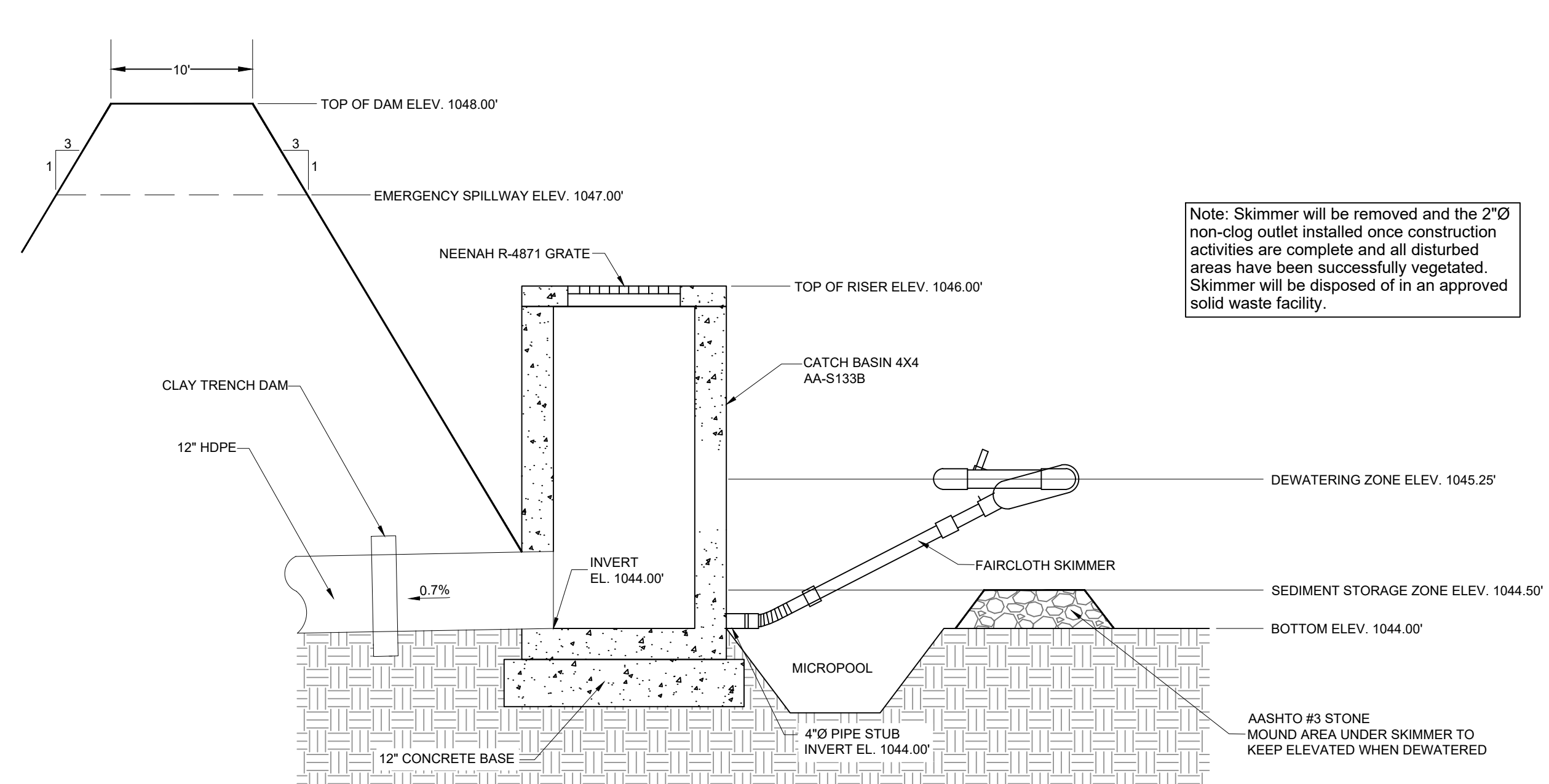
TYPICAL EDGE OF STATION PAD DETAIL
NOT TO SCALE

SKIMMER SPECIFICATIONS
FAIRCLOTH SKIMMER SIZE = 2.5"
ORIFICE DIAMETER SIZE = 2.4"
STUB SIZE ON OUTLET STRUCTURE = 4"

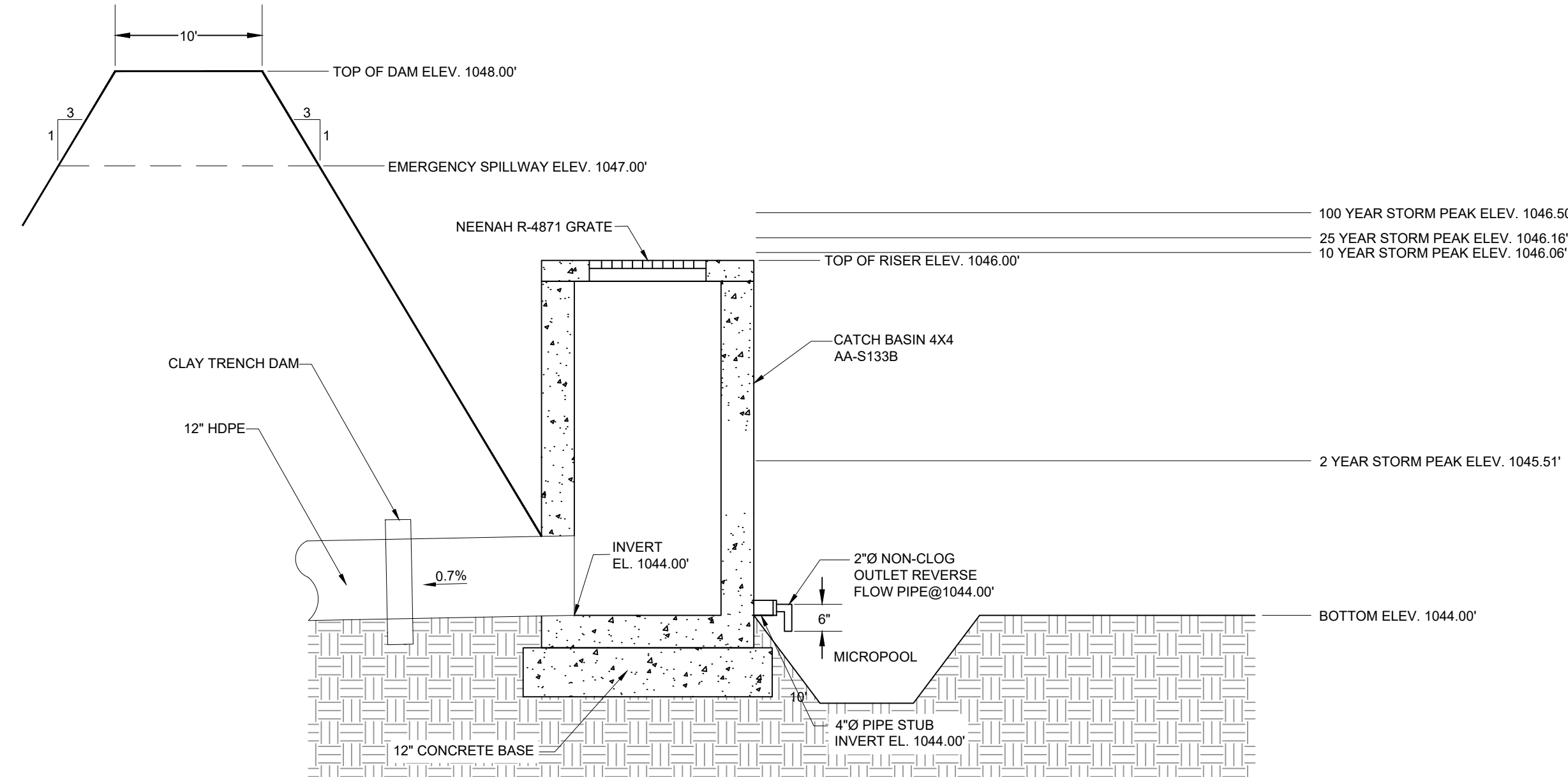
Note: If an alternative skimmer is to be used the Contractor is required to submit for review and approval to the Engineer shop drawings and supporting calculations which show the specified skimmer meets the required 48-hour drawdown time.



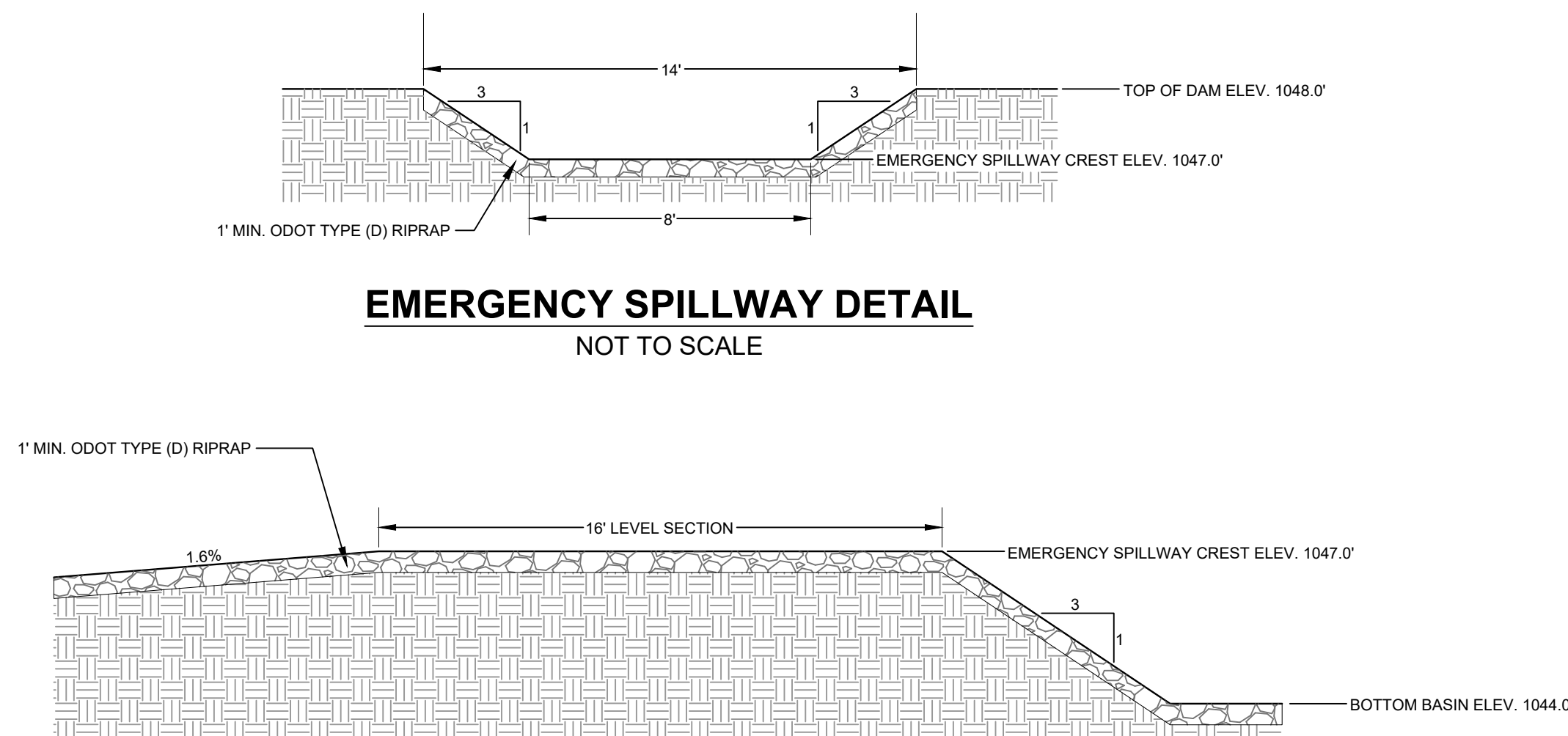
SKIMMER DETAIL
NOT TO SCALE



TEMPORARY SEDIMENT BASIN RISER DETAIL
NOT TO SCALE



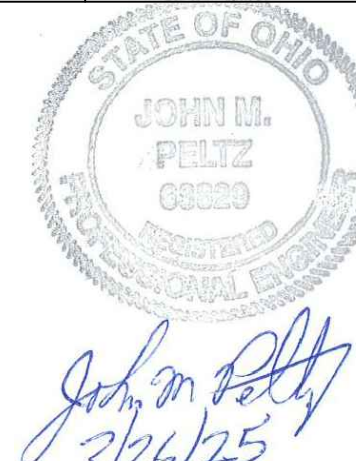
PERMANENT DRY DETENTION BASIN RISER DETAIL
NOT TO SCALE



EMERGENCY SPILLWAY CENTERLINE PROFILE
NOT TO SCALE

DETENTION BASIN - TEMPORARY SEDIMENT CONTROL STRUCTURE SCHEDULE					
TRIBUTARY ACREAGE (AC)	DISTURBED ACREAGE (AC)	REQUIRED BASIN DEWATERING VOLUME (1800 C.F./AC) (AC-FT)	PROVIDED BASIN DEWATERING VOLUME (AC-FT)	REQUIRED SEDIMENT STORAGE VOLUME (1000 C.F. / DISTURBED A.C.) (AC-FT)	PROVIDED SEDIMENT STORAGE VOLUME (AC-FT)
6.38	6.27	0.26	0.57	0.14	0.16

Sediment Basin Required Dewatering Volume Drawdown = 48 Hrs.
Sediment Basin Provided Dewatering Volume Drawdown > 48hrs.



BAIR GOODIE

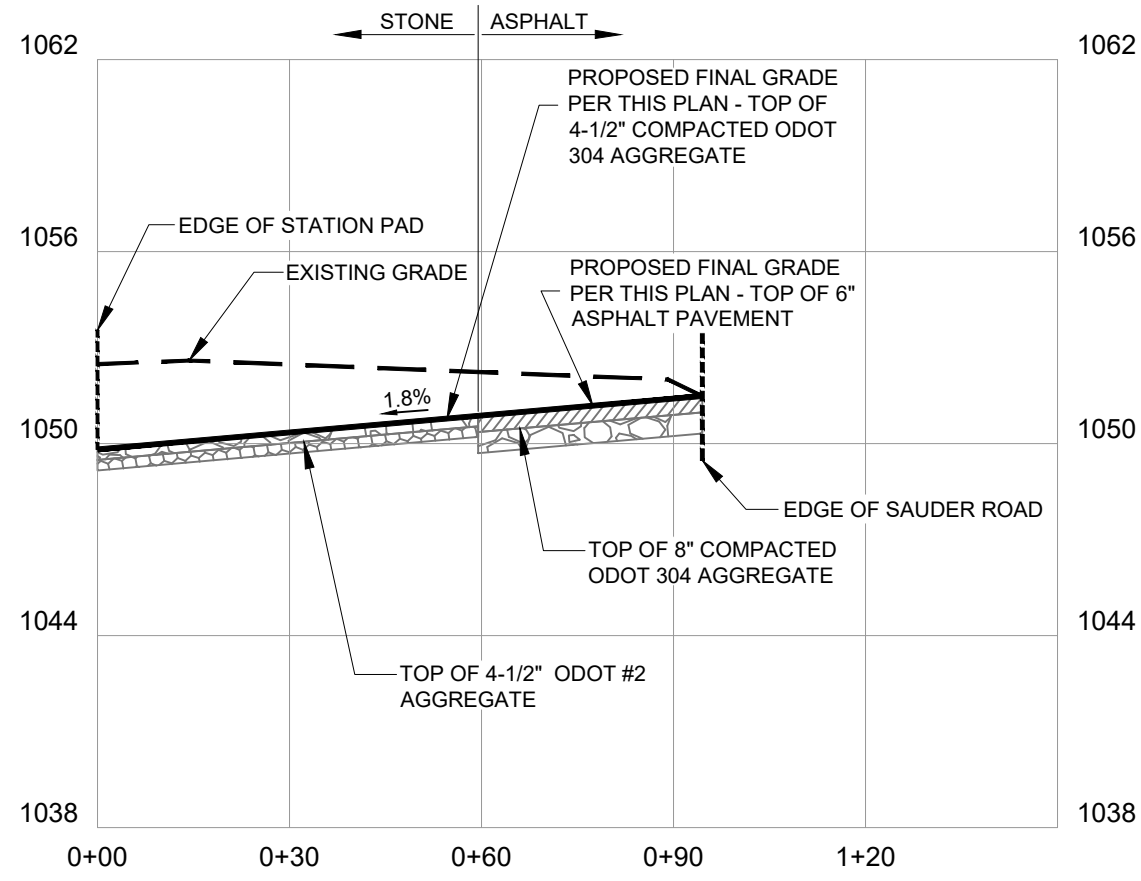
BAIR, GOODIE AND ASSOCIATES, INC.
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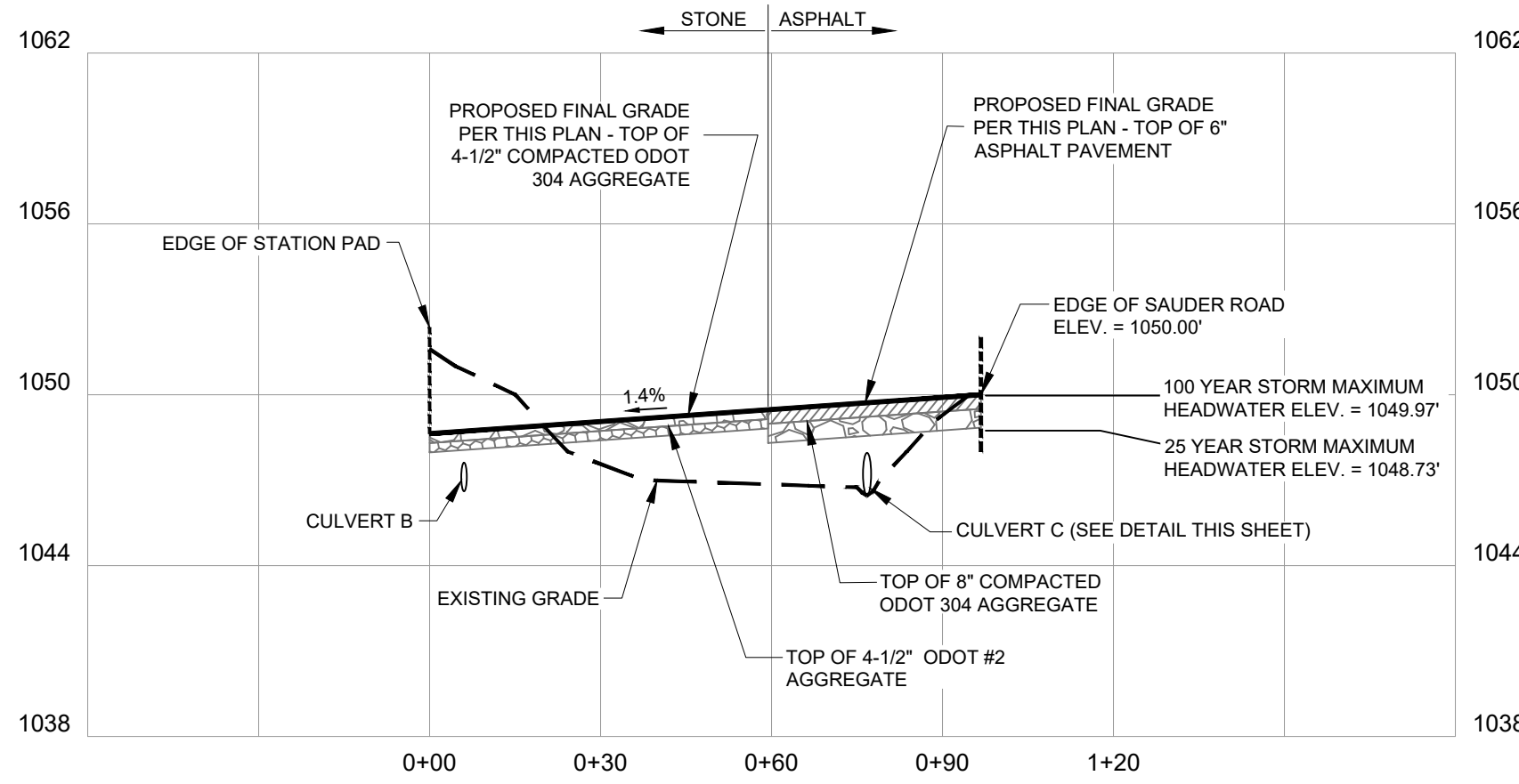
OLD DWG #:
DATE:
AEP OHIO TRANSMISSION COMPANY, INC.
SOUDER STATION
NEW ALBANY, OHIO

CROSS SECTIONS & DETAILS
SCALE: 1" = 30'
DR: DB/BGA
ENG: JP/BGA
CH: JP/BGA
WOF: T10593117002
APPD: JP/BGA
DATE: 09/11/2024
1 RIVERSIDE PLAZA
COLUMBUS, OH 43215
DWG. NO.: CU-GS01-S01
REV: 1

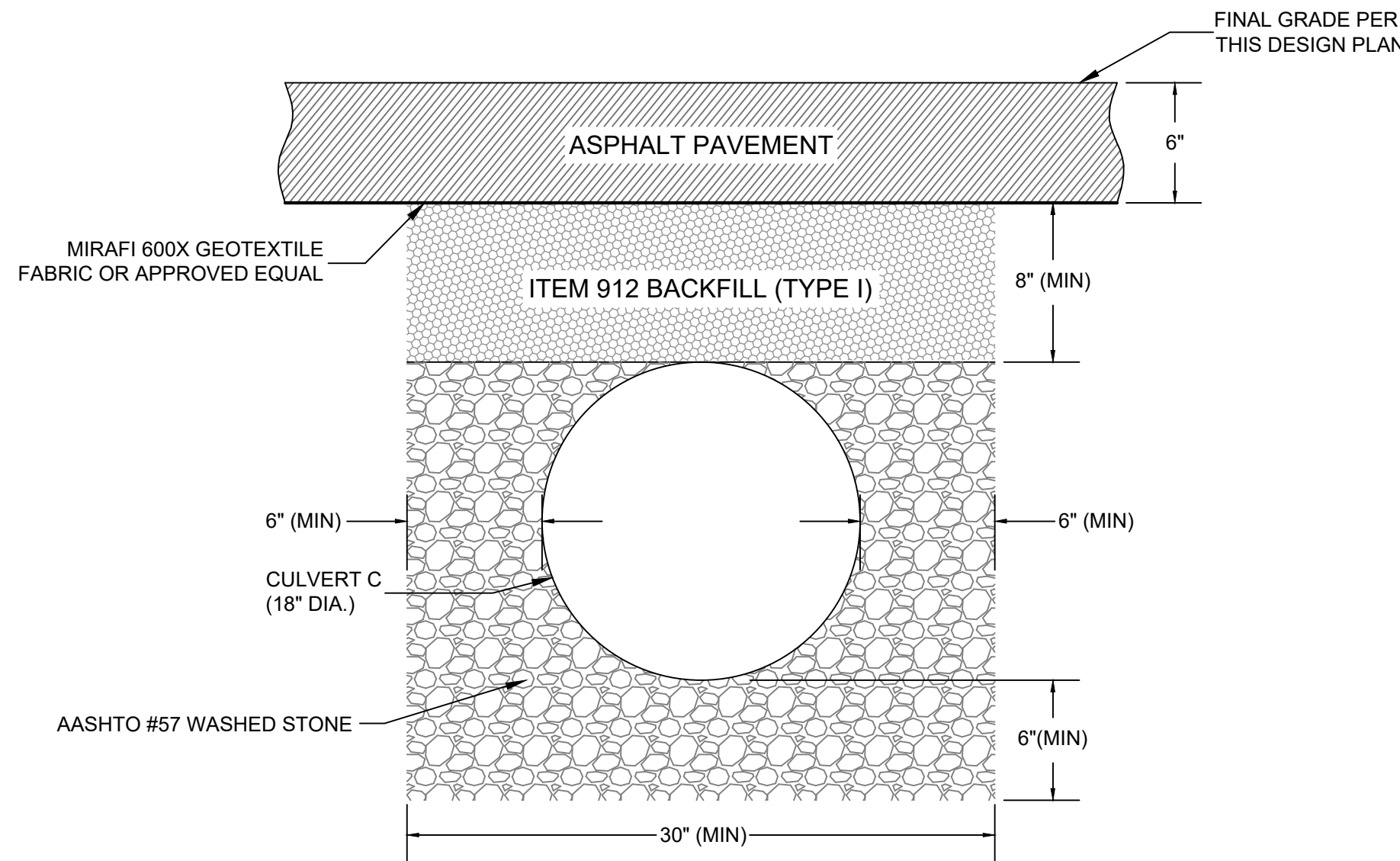




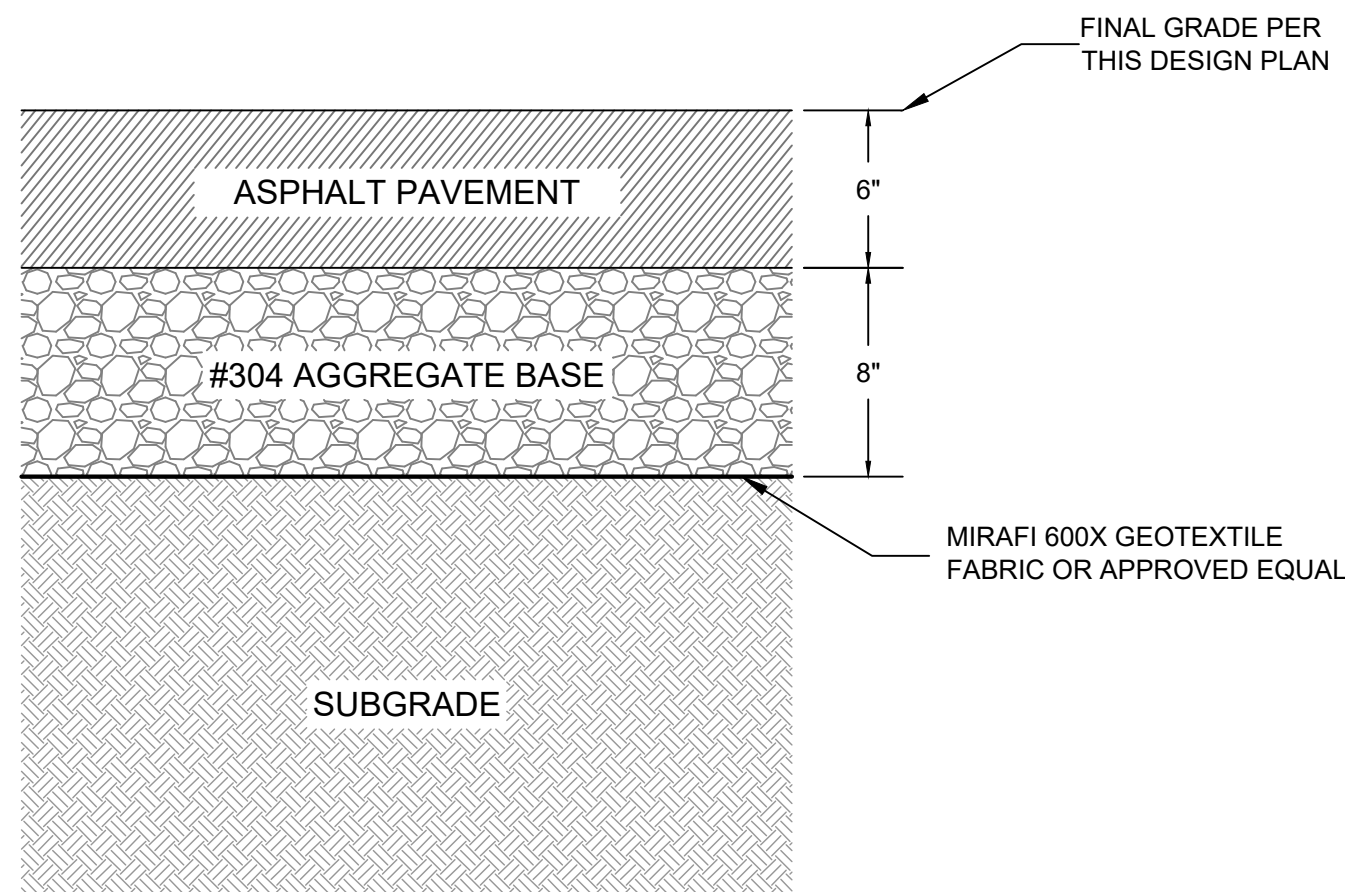
ACCESS ROAD A
HORIZONTAL SCALE: 1" = 30'
VERTICAL SCALE: 1" = 6'



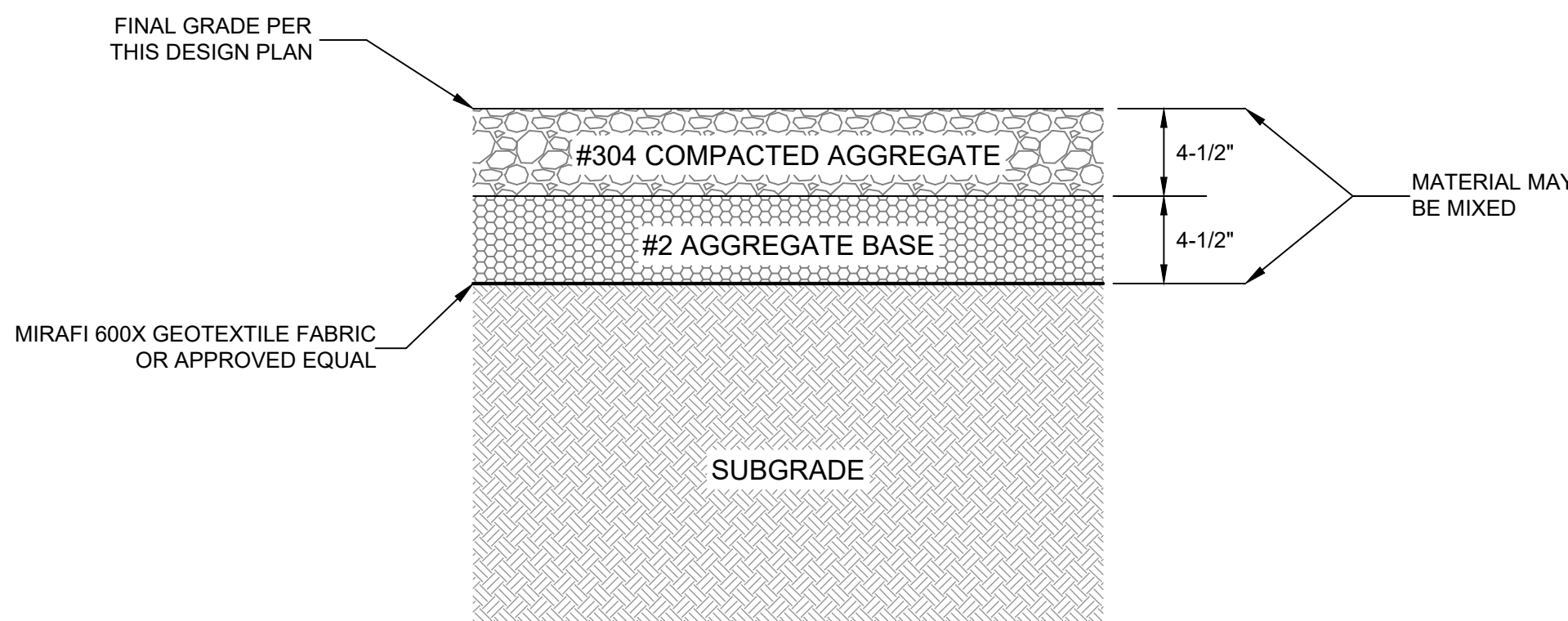
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HORIZONTAL SCALE: 1" = 30'
VERTICAL SCALE: 1" = 6'



**TYPICAL CULVERT C UNDER
ACCESS ROAD DETAIL**
NOT TO SCALE



**TYPICAL ASPHALT
ACCESS ROAD DETAIL**
NOT TO SCALE



**TYPICAL STONE
ACCESS ROAD DETAIL**
NOT TO SCALE



NO	DATE	REVISION DESCRIPTION	APPR	DR	ENG	CK	ISSUE#
1	03/26/2025	ADDED POST CONSTRUCTION WATER QUALITY NOTE, ADDED NOI NUMBER, AND UPDATED QUANTITIES ON SHEET CU-CS01-S01. ADDED NOTE TO SHEET CU-EC02-S01 AND SHEET CU-ED02-S02. ADDED IMPERVIOUS AREAS TABLE TO SHEET CU-SL01-S01. ADDED POST CONSTRUCTION BMP TABLE, ADDED MAJOR FLOOD ROUTING PATH, ADDED 100 FOOT MINIMUM CONSTRUCTION ENTRANCE NOTE, LABELED THE 100 YEAR WATER SURFACE ELEVATION OF THE BASIN, AND ADDED NORTH BERM ON SHEET CU-GP01-S01. ADDED EMERGENCY SPILLWAY CENTERLINE PROFILE AND ADDED NORTH BERM TO CROSS SECTION A-A' ON SHEET CU-GS01-S01. ADDED CULVERTS AND HEADWATER ELEVATIONS TO ACCESS ROAD B PROFILE AND ADDED TYPICAL CULVERT C UNDER ACCESS ROAD B DETAIL ON SHEET CU-RP01-S01. UPDATED DITCH DIMENSION TABLE TO PROVIDE 100 YEAR STORM MAXIMUM WATER DEPTH ON SHEET CU-ED02-S01. UPDATED CULVERT DATA CHART TO PROVIDE VELOCITIES AND ADDED CONSTRUCTION ENTRANCE AS PART OF THIS PROJECT ON SHEET CU-ED02-S02. UPDATED CITY OF NEW ALBANY STANDARD NOTES ON SHEET CU-GN02-S01 AND SHEET CU-GN02-S02.		DB	JP		

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BAIR, GOODIE AND ASSOCIATES, INC.
153 NORTH BROADWAY STREET
NEW PHILADELPHIA, OH 44663
TEL: 330.343.3499 FAX: 330.343.9505
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ADDITIONAL BMP'S

OPEN BURNING

NO MATERIALS MAY BE BURNED WHICH CONTAIN RUBBER, ASPHALT, OR PETROLEUM PRODUCTS SUCH AS TIRES, CARS, AUTO PARTS, PLASTICS OR PLASTIC COATED WIRE (SEE OAC 3745-9). OPEN BURNING IS NOT ALLOWED IN RESTRICTED AREAS. RESTRICTED AREAS ARE DEFINED AS:

1. WITHIN CORPORATION LIMITS.
2. WITHIN 1,000 FEET OF A MUNICIPAL CORPORATION.
3. WITHIN A ONE MILE ZONE OUTSIDE OF A CORPORATION OF 10,000 OR MORE.

OUTSIDE THE RESTRICTED AREA, NO OPEN BURNING CAN TAKE PLACE WITHIN 1,000 FEET OF AN INHABITED BUILDING LOCATED OFF THE PROPERTY WHERE THE FIRE IS SET. OPEN BURNING IS PERMISSIBLE IN A RESTRICTED AREA FOR THE FOLLOWING ACTIVITIES: HEATING TAR, WELDING AND ACETYLENE TORCHES, SMUDGE POTS AND SIMILAR OCCUPATIONAL NEEDS, AND HEATING OR WARMTH FOR OUTDOOR BARBECUES. OUTSIDE OF RESTRICTED AREAS, OPEN BURNING IS PERMISSIBLE FOR LANDSCAPE WASTES (PLANT MATERIAL, WITH PRIOR WRITTEN PERMISSION FROM OHIO EPA), AND AGRICULTURAL WASTES (MATERIAL GENERATED BY CROP, HORTEICULTURAL, OR LIVESTOCK PRODUCTION PRACTICES).

DUST CONTROL / SUPPRESSANTS

DUST CONTROL IS REQUIRED TO PREVENT NUISANCE CONDITIONS. DUST CONTROLS MUST BE USED IN ACCORDANCE WITH MANUFACTURERS SPECIFICATION AND NOT BE APPLIED IN A MANNER WHICH WOULD RESULT IN A DISCHARGE TO WATERS OF THE STATE. ISOLATION DISTANCES FROM BRIDGES, CATCH BASINS, AND OTHER DRAINAGEWAYS MUST BE OBSERVED (EXCLUDING WATER) AND NOT OCCUR WHEN PRECIPITATION IS IMMINENT. AS NOTED IN THE SHORT TERM FORECAST, USE OF OIL MAY NOT BE APPLIED FOR DUST CONTROL. CHEMICAL STABILIZERS MAY ONLY BE USED WITH AEP PERMISSION.

AIR PERMITTING REQUIREMENTS

ALL CONTRACTORS AND SUB CONTRACTORS MUST BE MADE AWARE THAT CERTAIN ACTIVITIES ASSOCIATED WITH CONSTRUCTION WILL REQUIRE AIR PERMITS. ACTIVITIES INCLUDING BUT NOT LIMITED TO MOBILE CONCRETE BATCH PLANTS, MOBILE ASPHALT PLANTS, CONCRETE CRUSHERS, LARGE GENERATORS, ETC., WILL REQUIRE SPECIFIC OHIO EPA AIR PERMITS FOR INSTALLATION AND OPERATION. NOTIFICATION FOR RESTORATION AND DEMOLITION MUST BE SUBMITTED TO OHIO EPA FOR ALL COMMERCIAL SITES TO DETERMINE IF ASBESTOS CORRECTIVE ACTIONS ARE REQUIRED.

WASTE DISPOSAL

THE CONTRACTOR SHALL PROVIDE LITTER CONTROL AND COLLECTION OF MATERIALS WITHIN THE PROJECT BOUNDARIES DURING CONSTRUCTION. ALL FERTILIZER, HYDROCARBON, OR OTHER CHEMICAL CONTAINERS SHALL BE DISPOSED OF BY THE CONTRACTOR IN ACCORDANCE WITH THE EPA'S STANDARD PRACTICES. NO SOLID MATERIAL INCLUDING BUILDING AND CONSTRUCTION MATERIAL SHALL BE DISPOSED OF, DISCHARGED OR BURIED ON-SITE.

OFFSITE VEHICLE TRACKING

LOADED HAUL TRUCKS SHALL BE COVERED WITH A TARP/AULIN. EXCESS DIRT MATERIAL ON THE ROADS SHALL BE REMOVED IMMEDIATELY. HAULING ON UNPAVED SURFACES SHALL BE MONITORED TO MINIMIZE DUST AND CONTROL EROSION. HAUL ROADS SHALL BE WATERED OR OTHER CONTROLS PROVIDED AS NECESSARY TO REDUCE DUST AND CONTROL SEDIMENTS.

SANITARY WASTE

THE CONTRACTOR SHALL PROVIDE PORTABLE SANITARY WASTE FACILITIES. THESE FACILITIES SHALL BE COLLECTED OR EMPTIED BY A LICENSED SANITARY WASTE MANAGEMENT CONTRACTOR AS REQUIRED BY STATE REGULATIONS.

FERTILIZERS AND PESTICIDES

FERTILIZERS SHALL BE APPLIED AT A RATE SPECIFIED BY THE SPECIFICATIONS OR THE MANUFACTURER. THE APPLICATION OF FERTILIZERS SHALL BE ACCOMPLISHED IN A MANNER AS DESCRIBED BY THE SPECIFICATION OR MANUFACTURER TO ENSURE THE PROPER INSTALLATION AND TO AVOID OVER FERTILIZING. PESTICIDES ARE NOT ANTICIPATED FOR THIS PROJECT.

ESTABLISH PROPER EQUIPMENT/VEHICLE FUELING AND MAINTENANCE PRACTICES

EQUIPMENT FUELING AND MAINTENANCE, OIL CHANGING, ETC., SHALL BE PERFORMED AWAY FROM THE WATERCOURSES, DITCHES, OR STORM DRAINS. IN AN AREA DESIGNATED FOR THAT PURPOSE, THE DESIGNATED AREA SHALL BE EQUIPPED FOR RECYCLING OIL AND CATCHING SPILLS. SECONDARY CONTAINMENT SHALL BE PROVIDED FOR ALL FUEL OIL STORAGE TANKS. THESE AREAS MUST BE INSPECTED EVERY SEVEN DAYS AND WITHIN 24 HOURS OF A 1/8 INCH OR GREATER RAIN EVENT TO ENSURE THERE ARE NO EXPOSED MATERIALS WHICH WOULD CONTAMINATE STORM WATER.

SPILL PREVENTION CONTROL PLAN

SITE OPERATORS MUST BE AWARE THAT SPILL PREVENTION CONTROL AND COUNTERMEASURES (SPCC) REQUIREMENTS APPLY. AN SPCC PLAN IS REQUIRED FOR SITES WITH ONE SINGLE ABOVE GROUND STORAGE TANK OF 1,320 GALLONS OR MORE, OR 42,000 GALLONS OF UNDERGROUND STORAGE. SITES THAT HAVE BEEN CONTAMINATED MUST BE DISPOSED OF IN ACCORDANCE WITH SECTION "CONTAMINATED SOILS" FOUND BELOW.

SPILLS ON PAVEMENT SHALL BE ABSORBED WITH SAWDUST, CAT LITTER OR OTHER ABSORBENT MATERIAL AND DISPOSED OF WITH THE TRASH AT A LICENSED SANITARY LANDFILL. HAZARDOUS OR INDUSTRIAL WASTES SUCH AS SOLVENTS, GASOLINE, OIL-BASED PAINTS, AND CEMENT CURING COMPOUNDS REQUIRE SPECIAL HANDLING. SPILLS SHALL BE REPORTED TO THE OHIO EPA (1-800-368-6070). SPILLS OF 25 GALLONS OR MORE OF PETROLEUM PRODUCTS SHALL BE REPORTED TO THE OHIO EPA, THE LOCAL FIRE DEPARTMENT, AND THE LOCAL EMERGENCY PLANNING COMMITTEE WITHIN 30 MINUTES OF THE DISCOVERY OF THE RELEASE. ALL SPILLS WHICH RESULT IN CONTACT WITH WATERS OF THE STATE MUST BE REPORTED TO OHIO EPA'S HOTLINE.

CONTAMINATED SOILS

IF SUBSTANCES SUCH AS OIL, DIESEL FUEL, HYDRAULIC FLUID, ANTIFREEZE, ETC., ARE SPILLED, LEAKED, OR RELEASED ONTO THE SOIL, THE SOIL SHOULD BE DUG UP AND DISPOSED OF AT A LICENSED SANITARY LANDFILL OR OTHER APPROVED PETROLEUM CONTAMINATED SOIL REMEDIATION FACILITY (NOT A CONSTRUCTION/DEMOLITION DEBRIS LANDFILL). PLEASE BE AWARE THAT STORM WATER RUN OFF ASSOCIATED WITH CONTAMINATED SOILS ARE NOT BEING AUTHORIZED UNDER OHIO EPA'S GENERAL STORMWATER PERMIT ASSOCIATED WITH CONSTRUCTION ACTIVITIES. IN THE EVENT THERE ARE LARGE EXTENSIVE AREAS OF CONTAMINATED SOILS, ADDITIONAL MEASURES ABOVE AND BEYOND THE CONDITIONS OF OHIO EPA'S GENERAL CONSTRUCTION STORMWATER PERMIT WILL BE REQUIRED. DEPENDING ON THE EXTENT OF CONTAMINATION, ADDITIONAL TREATMENT AND/OR CORRECTIVE DISPOSAL MAY BE REQUIRED. ALL STORMWATER DISCHARGES ASSOCIATED WITH CONTAMINATED SOILS MUST BE AUTHORIZED UNDER AN ALTERNATIVE NPDES PERMIT.

FILTER SOCK (FS)

DESCRIPTION

FILTER SOCKS ARE SEDIMENT-TRAPPING DEVICES USING COMPOST INSERTED INTO A FLEXIBLE, PERMEABLE TUBE WITH A PNEUMATIC BLOWER DEVICE OR EQUIVALENT. FILTER SOCKS TRAP SEDIMENT BY FILTERING WATER PASSING THROUGH THE BERM AND ALLOWING WATER TO POND, CREATING A SETTLING OF SOILS.

SPECIFICATIONS FOR FILTER SOCK

MATERIALS: COMPOST USED FOR FILTER SOCKS SHALL BE WEED, PATHOGEN AND INSECT FREE AND FREE OF ANY RESIDUE. CONTAMINANTS OR OTHER MATERIALS TOXIC TO PLANT GROWTH. THEY SHALL BE DERIVED FROM A WELL-DECOMPOSED SOURCE OF ORGANIC MATTER AND CONSIST OF A PARTICLE RANGING FROM 3/8" TO 2".

2. FILTER SOCKS SHALL BE 3 OR 5 MIL CONTINUOUS, TUBULAR, HOPE 3/8" KNITTED MESH NETTING MATERIAL, FILLED WITH COMPOST PASSING THE ABOVE SPECIFICATIONS FOR COMPOST PRODUCTS.

INSTALLATION:

3. FILTER SOCKS WILL BE PLACED ON A LEVEL LINE ACROSS SLOPES, GENERALLY PARALLEL TO THE BASE OF THE SLOPE OR OTHER AFFECTED AREA. ON SLOPES APPROACHING 2:1, ADDITIONAL SOCKS SHALL BE PROVIDED AT THE TOP AND AS NEEDED MID-SLOPE.

4. FILTER SOCKS INTENDED TO BE LEFT AS A PERMANENT FILTER OR PART OF THE NATURAL LANDSCAPE SHALL BE SEEDED AT THE TIME OF INSTALLATION FOR ESTABLISHMENT OF PERMANENT VEGETATION.

5. FILTER SOCKS ARE NOT TO BE USED IN CONCENTRATED FLOW SITUATIONS OR IN RUNOFF CHANNELS.

MAINTENANCE:

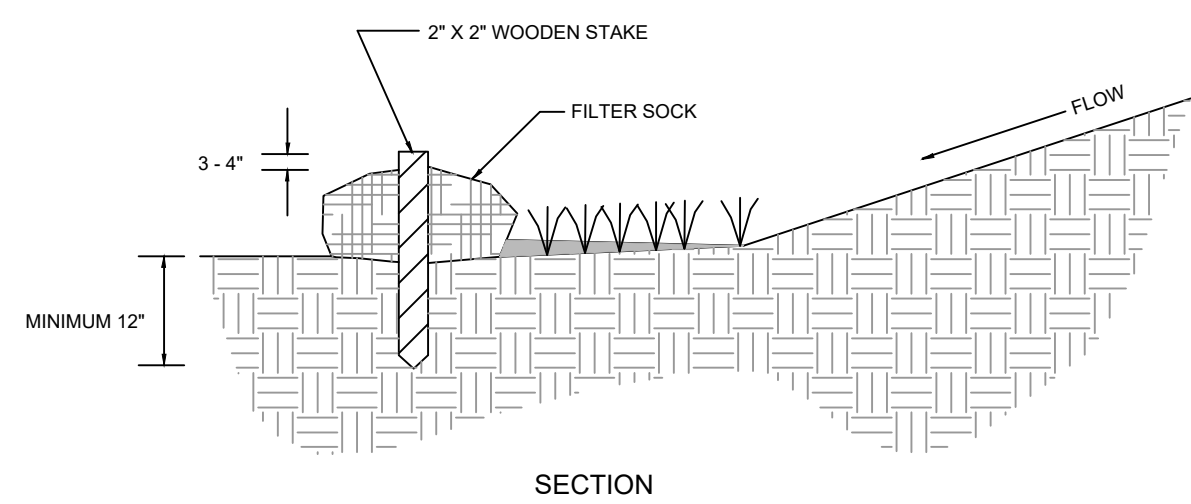
6. ROUTINELY INSPECT FILTER SOCKS AFTER EACH SIGNIFICANT RAIN, MAINTAINING FILTER SOCKS IN A FUNCTIONAL CONDITION AT ALL TIMES.

7. REMOVE SEDIMENTS COLLECTED AT THE BASE OF THE FILTER SOCKS WHEN THEY REACH 1/3 OF THE EXPOSED HEIGHT OF THE PRACTICE.

8. WHERE THE FILTER SOCK DETERIORATES OR FAILS, IT WILL BE REPAIRED OR REPLACED WITH A MORE EFFECTIVE ALTERNATIVE.

9. REMOVAL - FILTER SOCKS WILL BE DISPERSED ON SITE WHEN NO LONGER REQUIRED IN SUCH A WAY AS TO FACILITATE AND NOT OBSTRUCT SEEDLINGS.

NOTE: IF REGULATORY AGENCIES REQUIRE SALT FENCE INSTEAD OF FILTER SOCK, INSTALL PER STANDARD DETAILS.



TEMPORARY ROLLED EROSION CONTROL PRODUCTS (EROSION CONTROL MATTING) (EM)

DESCRIPTION

A TEMPORARY ROLLED EROSION CONTROL PRODUCT (RECP) IS A DEGRADABLE MANUFACTURED MATERIAL USED TO STABILIZE EASILY ERODED AREAS WHILE VEGETATION BECOMES ESTABLISHED. TEMPORARY ROLLED EROSION CONTROL PRODUCTS ARE DEGRADABLE PRODUCTS COMPOSED OF BIOLOGICALLY, PHOTO CHEMICALLY OR OTHERWISE DEGRADABLE MATERIALS. TEMPORARY RECP'S CONSIST OF EROSION CONTROL NETTING, OPEN WEAVE TEXTILES, AND EROSION CONTROL, BLANKETS AND MATTINGS. THESE PRODUCTS REDUCE SOIL EROSION AND ASSIST VEGETATIVE GROWTH BY PROVIDING TEMPORARY COVER FROM THE EROSION ACTION OF RAINFALL AND RUNOFF WHILE PROVIDING SOIL-SEED CONTACT.

SPECIFICATIONS FOR TEMPORARY ROLLED EROSION CONTROL PRODUCT

1. CHANNEL/SLOPE SOIL PREPARATION: GRADE AND COMPACT AREA OF INSTALLATION, PREPARING SEEDED BY COVERING 2" OF TOP SOIL WITH FINE GRADE. INCORPORATE AMENDMENTS SUCH AS LIME AND FERTILIZER INTO SOIL, REMOVE ALL ROCKS, CLODS, VEGETATION OR OTHER DEBRIS SO THAT INSTALLED RECP WILL HAVE DIRECT CONTACT WITH THE SOIL SURFACE.

2. CHANNEL/SLOPE SEEDING: APPLY SEED TO SOIL SURFACE PRIOR TO INSTALLATION, ALL CHECK SLOTS, ANCHOR TRENCHES, AND OTHER DISTURBED AREAS MUST BE RESEED. REFER TO THE PERMANENT SEEDING SPECIFICATION FOR SEEDING RECOMMENDATIONS.

SLOPE INSTALLATION

3. EXCAVATE TOP AND BOTTOM TRENCHES (12"X6"). INTERMITTENT EROSION CHECK SLOTS (8"X6") MAY BE REQUIRED BASED ON SLOPE LENGTH. EXCAVATE TOP ANCHOR TRENCH 2'X2' OVER CREST OF THE SLOPE.

4. IF INTERMITTENT EROSION CHECK SLOTS ARE REQUIRED, INSTALL RECP IN 6"X6" SLOT AT A MAXIMUM OF 30' CENTERS ON THE MID POINT OF THE SLOPE. RECP SHOULD BE STAPLED INTO TRENCH ON 12' CENTERS.

5. INSTALL RECP IN TOP ANCHOR TRENCH, ANCHOR ON 12' SPACINGS, BACKFILL AND COMPACT SOIL.

6. UNROLL RECP DOWN SLOPE WITH ADJACENT ROLLS OVERLAPPED A MINIMUM OF 3". ANCHOR THE BEAM EVERY 18' AND LAY THE RECP LOOSE TO MAINTAIN DIRECT SOIL CONTACT, DO NOT PULL TAUGHT.

7. OVERLAP ROLLS A MINIMUM OF 12" WITH UP-SLOPE RECP ON TOP FOR A SINGLE EFFECT. BEGIN ALL NEW ROLLS IN AN INTERMITTENT CHECK SLOT IF REQUIRED. DOUBLE ANCHOR ACROSS ROLL VERY 12'.

8. INSTALL RECP IN BOTTOM ANCHOR TRENCH (12"X6"), ANCHOR EVERY 12'. PLACE ALL OTHER STAPLES THRU OUT SLOPE AT 1 TO 2.5 PER SQUARE YARD DEPENDENT ON SLOPE. REFER TO MANUFACTURER'S ANCHOR GUIDE.

CHANNEL INSTALLATION

9. EXCAVATE INITIAL ANCHOR TRENCH (12"X6") ACROSS THE LOWER END OF THE PROJECT AREA.

10. EXCAVATE INTERMITTENT CHECK SLOTS (8"X6") ACROSS THE CHANNEL AT 30' INTERVALS ALONG THE CHANNEL.

11. EXCAVATE LONGITUDINAL CHANNEL ANCHOR SLOTS (4"X4") ALONG BOTH SIDES OF THE CHANNEL TO BURY THE EDGES. WHENEVER POSSIBLE EXTEND THE RECP 2'-3' ABOVE THE CREST OF CHANNEL SIDE SLOPES.

12. INSTALL RECP IN INITIAL ANCHOR TRENCH (DOWNSTREAM) ANCHOR EVERY 12', BACKFILL AND COMPACT SOIL.

13. ROLL OUT RECP BEGINNING IN THE CENTER OF THE CHANNEL TOWARD THE INTERMITTENT CHECK SLOT. DO NOT PULL TAUGHT. UNROLL ADJACENT ROLLS UPSTREAM WITH A 3" MINIMUM OVERLAP (ANCHOR EVERY 18') AND UP EACH CHANNEL SIDE SLOPE.

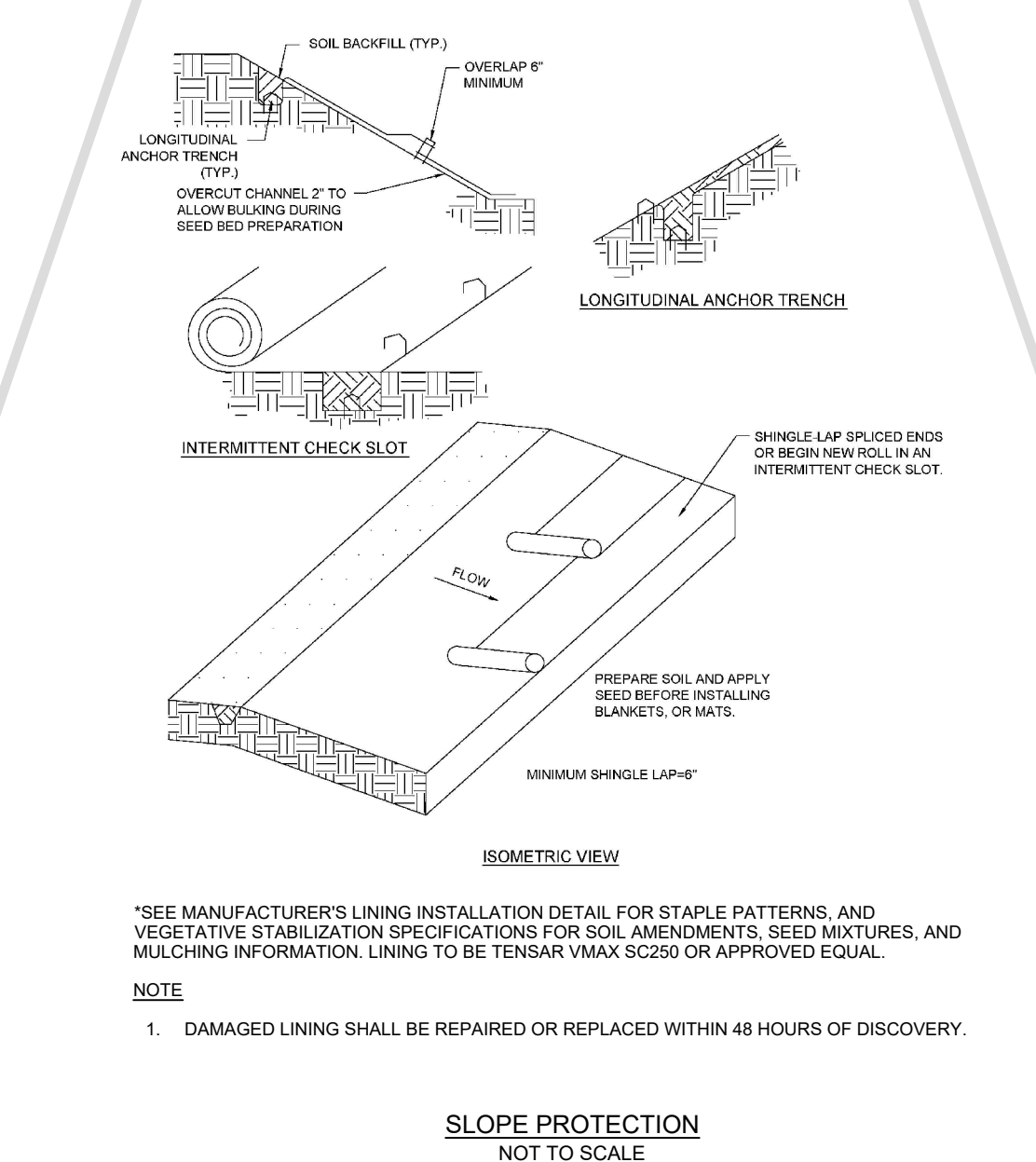
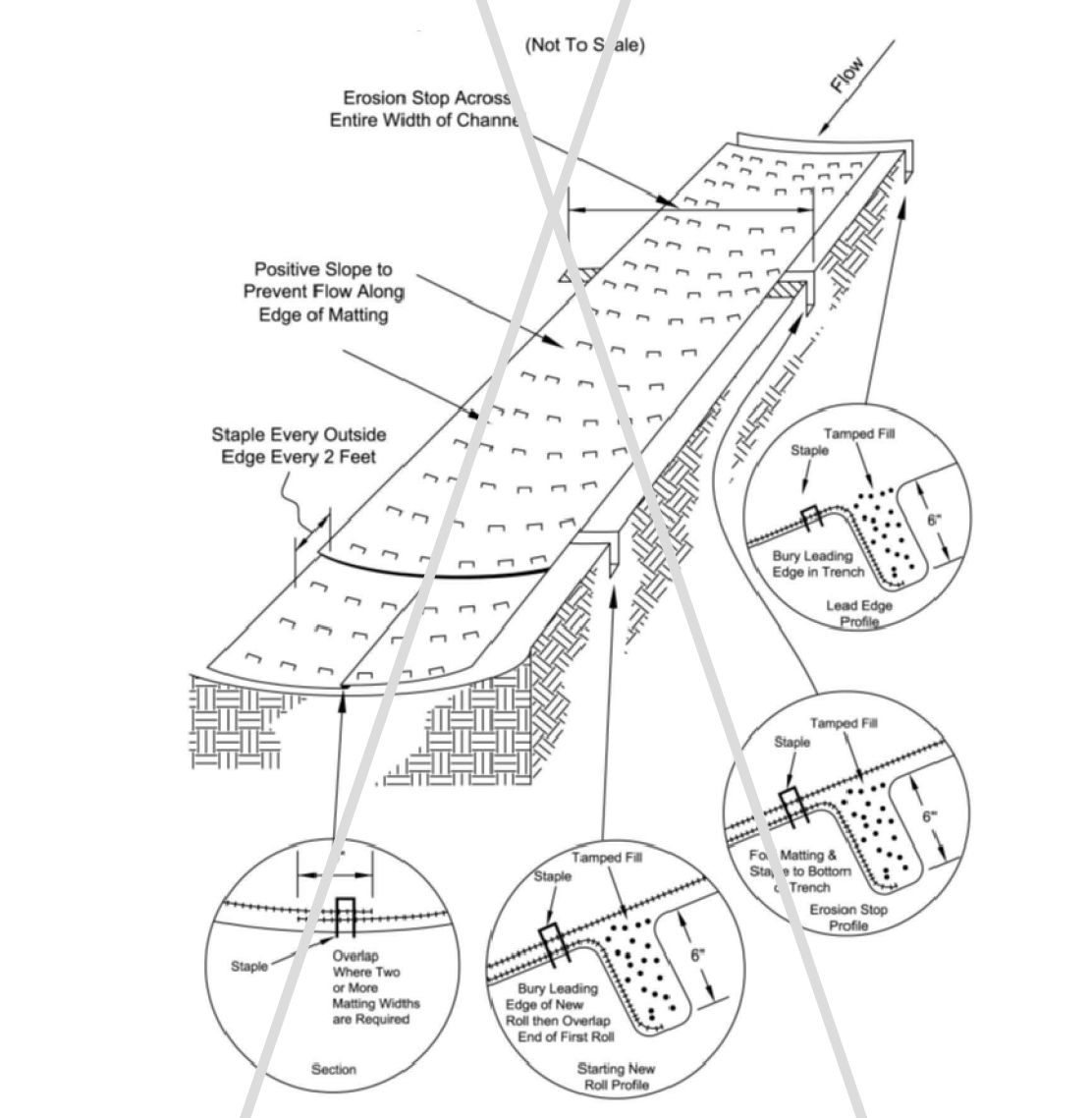
14. AT TOP OF CHANNEL SIDE SLOPES INSTALL RECP IN THE LONGITUDINAL ANCHOR SLOTS, ANCHORS EVERY 18'.

15. INSTALL RECP IN INTERMITTENT CHECK SLOTS, LAY INTO TRENCH AND SECURE WITH ANCHORS EVERY 12', BACKFILL WITH SOIL AND COMPACT.

16. OVERLAP ROLLS A MINIMUM OF 12" WITH UPSTREAM RECP ON TOP FOR A SINGLE EFFECT. BEGIN ALL NEW ROLLS IN AN INTERMITTENT CHECK SLOT, DOUBLE ANCHORED EVERY 12'.

17. INSTALL UPSTREAM END IN A PERMANENT ANCHOR TRENCH (12"X6"), ANCHOR EVERY 12', BACKFILL AND COMPACT.

18. COMPLETE ANCHORING THROUGHOUT CHANNEL AT 2.5 PER SQUARE YARD USING SUITABLE GROUND ANCHORING DEVICES (SHAPED WIRE STAPLES, METAL GEOTEXTILE TIES, PLASTIC STAPLES, AND TRIANGULAR WOODEN STAKES). ANCHORS SHOULD BE OF SUFFICIENT LENGTH TO EASILY PULL OUT. LONGER ANCHORS MAY BE REQUIRED IN LOOSE SANDY OR CLAYEY SOILS.



TYPICAL COMPOST SOCK CHECK DAM (CS)

1. COMPOST SOCK NETTING SHALL USE A KNITTED MESH FABRIC WITH 1/8" - 3/8 INCH OPENINGS, AND COMPOST MEDIA WITH PARTICLE SIZES 90% + 3 INCHES, AND 60% + 3/8 INCHES (CONFORMING TO MEDIA DESCRIBED IN CHAPTER 6 OF THE CONR RAINWATER AND LAND DEVELOPMENT MANUAL).

2. COMPOST SOCK CHECK DAMS SHALL BE USED IN AREAS THAT DRAIN 5 ACRES OR LESS.

3. SEDIMENT SHALL BE REMOVED FROM BEHIND THE SOCK WHEN IT REACHES 1/2 THE HEIGHT OF THE CHECK DAM.

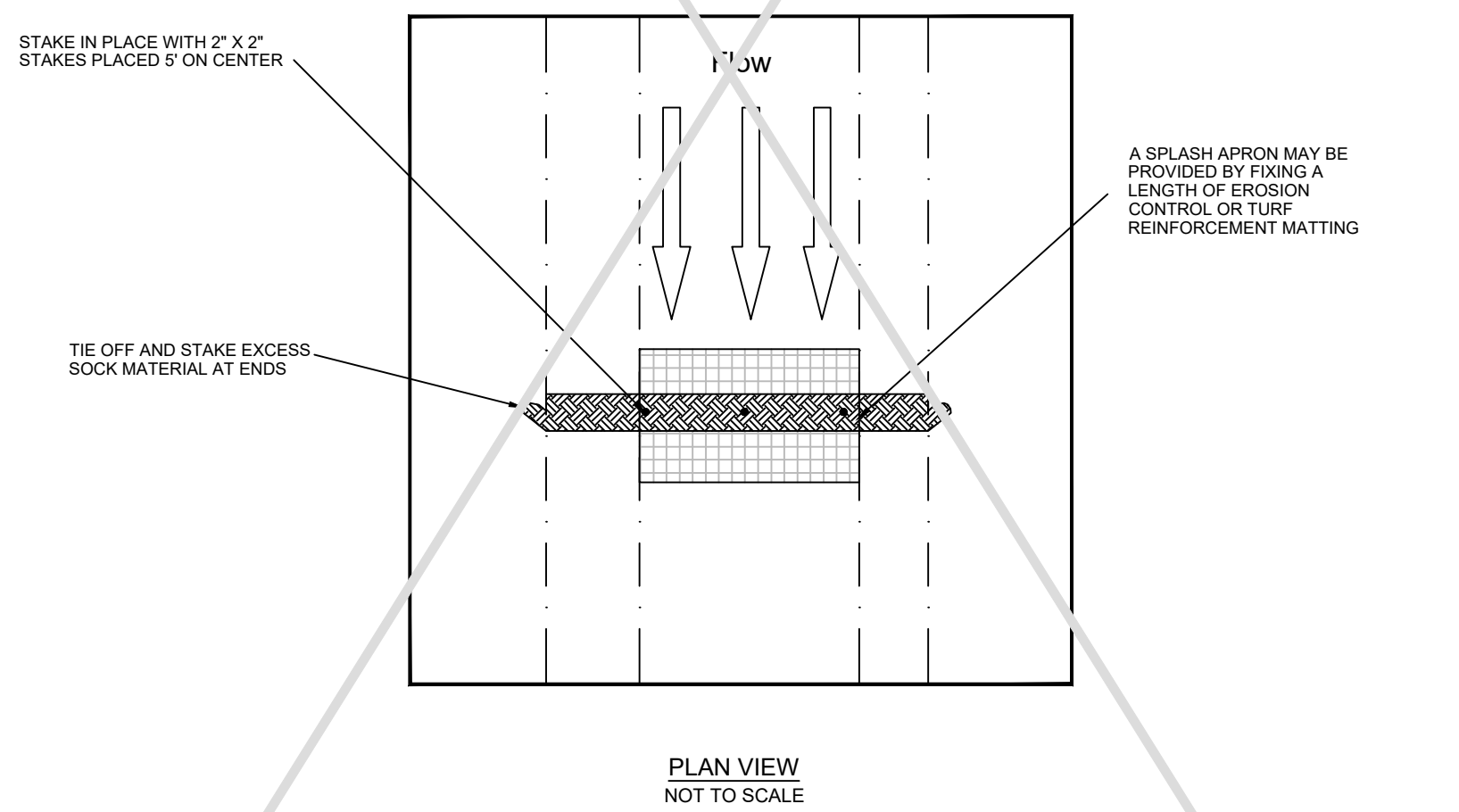
4. COMPOST SOCK CHECK DAMS SHALL BE CONSTRUCTED WITH 12, 18, OR 24 INCH DIAMETER COMPOST SOCKS, AND SHALL COMPLETELY COVER THE WIDTH OF THE CHANNEL. THE MIDPOINT OF THE COMPOST SOCK CHECK DAM SHALL BE A MINIMUM OF 6 INCHES LOWER THAN THE SIDES IN ORDER TO DIRECT FLOW ACROSS THE CENTER AND AWAY FROM THE CHANNEL SIDES. FILTER SOCK CHECK DAMS SHALL BE FILLED TO A DENSITY SUCH THAT THEY SHALL REACH THEIR INTENDED HEIGHT (DIAMETERS) AFTER INSTALLATION AND USE. THEY SHALL BE CONSIDERED UNSUITABLE AND IN NEED OF REPLACEMENT AFTER FALLING BELOW 80% OF THEIR MINIMUM REQUIRED HEIGHT (DIAMETER).

5. ALTHOUGH NO TRENCHING IS NECESSARY, COMPOST SOCK CHECK DAMS SHALL BE PLACED ON A GRADE/SURFACE WHERE CONSISTENT CONTACT WITH THE SOIL SURFACE IS MADE WITHOUT WOODEN STAKES, RILLS, GULLIES, STONES OR OTHER IRREGULARITIES.

6. PLACE COMPOST SOCK CHECK DAMS SO THAT THE ENDS EXTEND TO THE TOP OF BANK, STAKING FOR COMPOST SOCK CHECK DAMS SHALL USE 2 INCH X 2 INCH WOODEN STAKES, PLACED 5 FOOT ON CENTER. STAKE LENGTH SHALL ALLOW THEM TO BE DRIVEN 12 INCHES INTO EXISTING SOIL AND ALLOW AT LEAST 2 INCHES ABOVE THE SOCK.

7. SPACE COMPOST SOCK CHECK DAMS SO THAT THE TOE OF THE UPSTREAM DAM IS AT THE SAME ELEVATION OR LOWER (ELEVATION AS THE TOP OF THE DOWNSTREAM COMPOST SOCK CHECK DAM AT THE CENTER OF THE CHANNEL). THIS WILL BE INFLUENCED BY THE HEIGHT OF THE SOCK AND GRADIENT OF THE WATERWAY.

8. A SPLASH APRON MAY BE NEEDED WHERE FLOWS OVER THE SOCK MAY ERODE THE CHANNEL AND UNDERCUT THE COMPOST SOCK CHECK DAM. CREATE THE APRON BY DRIVING A LENGTH OF TEMPORARY ROLLED EROSION CONTROL PRODUCT (RECP - EROSION CONTROL MATTING) OR TURF REINFORCEMENT MATTING STARTING UPSTREAM OF THE SOCK A DISTANCE, EQUAL TO THE SOCK HEIGHT AND EXTENDING A LENGTH 1 TO 2 TIMES THE HEIGHT OF THE COMPOST SOCK CHECK DAM. SEE CHAPTER 7 OF THE CONR RAINWATER AND LAND DEVELOPMENT MANUAL FOR INFORMATION REGARDING THESE MATERIALS. MATERIALS USED SHOULD BE ABLE TO BE LEFT IN PLACE (E.G. BIODEGRADABLE/PHOTODEGRADABLE TYP) WITHOUT CREATING PROBLEMS FOR FUTURE MOVING OR MAINTENANCE OF THE CHANNEL.



TYPICAL ROCK CHECK DAM (CD)

1. THE CHECK DAM SHALL BE CONSTRUCTED OF 4-8 INCH DIAMETER STONE, PLACED SO THAT IT COMPLETELY COVERS THE WIDTH OF THE CHANNEL. ODOT TYPE (D) STONE IS ACCEPTABLE, BUT SHOULD BE UNDERLAIN WITH A GRAVEL FILTER CONSISTING OF ODOT NO. 3 OR 4 OR SUITABLE FILTER FABRIC.

2. MAXIMUM HEIGHT OF CHECK DAM SHALL NOT EXCEED 3.0 FEET.

3. THE MIDPOINT OF THE ROCK CHECK DAM SHALL BE A MINIMUM OF 6 INCHES LOWER THAN THE SIDES IN ORDER TO DIRECT ACROSS THE CENTER AND AWAY FROM THE CHANNEL SIDES.

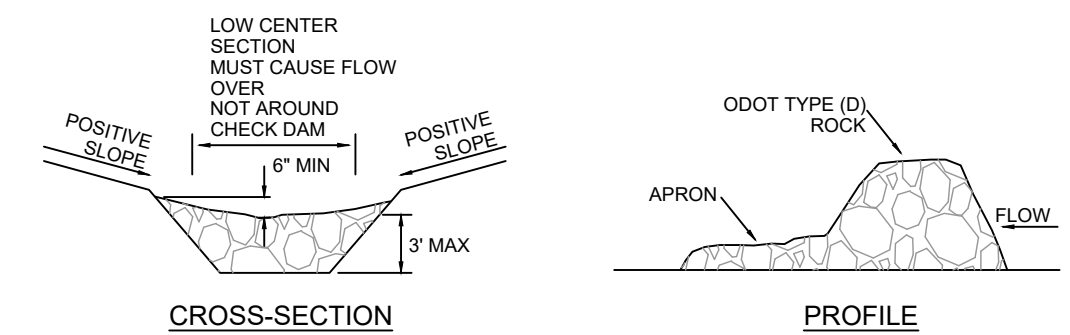
4. THE BASE OF THE CHECK DAM SHALL BE ENTRENCHED APPROXIMATELY 6 INCHES.

5. SPACING OF CHECK DAMS SHALL BE IN A MANNER SUCH THAT THE TOE OF THE UPSTREAM DAM IS AT THE SAME ELEVATION AS THE TOP OF THE DOWNSTREAM DAM.

6. A SPLASH APRON SHALL BE CONSTRUCTED WHERE CHECK DAMS ARE EXPECTED TO BE IN USE FOR AN EXTENDED PERIOD OF TIME. A STONE APRON SHALL BE CONSTRUCTED IMMEDIATELY DOWNSTREAM OF THE CHECK DAM TO PREVENT FLOWS FROM UNDERCUTTING THE STRUCTURE. THE APRON SHOULD BE 6 INCHES THICK AND ITS LENGTH TWO TIMES THE HEIGHT OF THE DAM.

7. STONE PLACEMENT SHALL BE PERFORMED EITHER BY HAND OR MECHANICALLY AS LONG AS THE CENTER OF CHECK DAM IS LOWER THAN THE SIDES AND EXTENDS ACROSS ENTIRE CHANNEL.

8. SIDE SLOPES SHALL BE A MINIMUM OF 2:1.



TYPICAL ROCK CHECK DAM
NOT TO SCALE

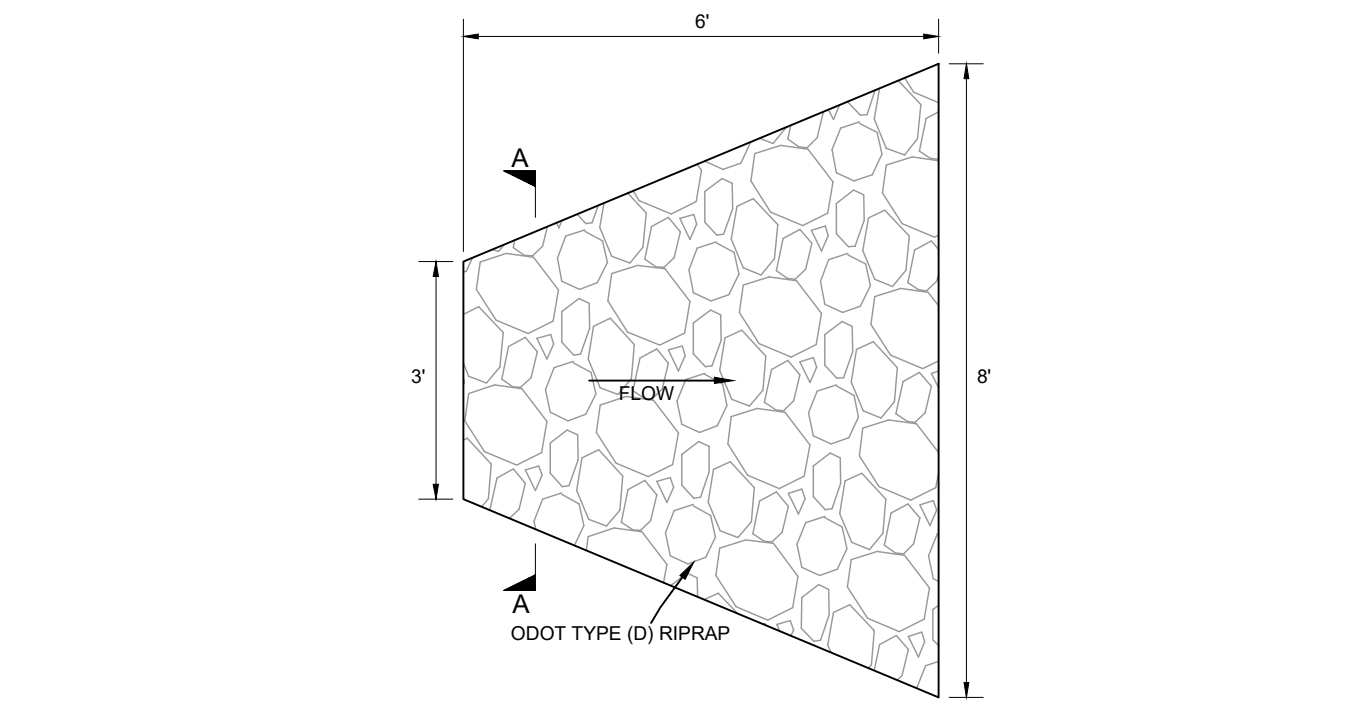
INLET PROTECTION (IP)

STORM DRAIN INLET PROTECTION DEVICES REMOVE SEDIMENT FROM STORM WATER BEFORE IT ENTERS STORM SEWERS AND DOWNSTREAM AREAS. INLET PROTECTION DEVICES ARE SEDIMENT BARRIERS THAT MAY BE CONSTRUCTED OF WASHED GRAVEL, OR CRUSHED STONE, GEOTEXTILE FABRICS AND OTHER MATERIALS THAT ARE SUPPORTED AROUND OR ACROSS STORM DRAIN INLETS.

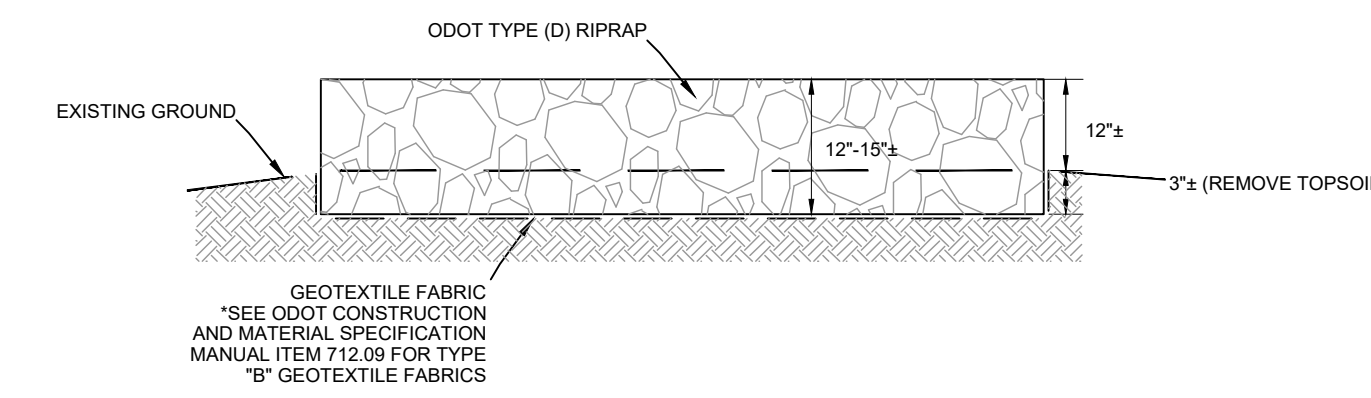
INLET PROTECTION IS INSTALLED TO CAPTURE SOME SEDIMENT AND REDUCE THE MAINTENANCE OF STORM SEWERS AND OTHER UNDERGROUND PIPING SYSTEMS PRIOR TO THE SITE BEING STABILIZED. DUE TO THEIR POORER EFFECTIVENESS, INLET PROTECTION IS CONSIDERED A SECONDARY SEDIMENT CONTROL. TO BE USED IN CONJUNCTION WITH OTHER MORE EFFECTIVE CONTROLS.

NO	DATE	REVISION DESCRIPTION	APPR	DR	ENG	CK	ISSUE#
1	03/26/2025	ADDED POST CONSTRUCTION WATER QUALITY NOTE, ADDED NOI NUMBER, AND UPDATED QUANTITIES ON SHEET CU-CS01-S01. ADDED NOTE TO SHEET CU-EC02-S01 AND SHEET CU-EC02-S02. ADDED IMPERVIOUS AREAS TABLE TO SHEET CU-SLO1-S01. ADDED POST CONSTRUCTION BMP TABLE. ADDED MAJOR FLOOD ROUTING PATH. ADDED 100 FOOT MINIMUM CONSTRUCTION ENTRANCE NOTE, LABELED THE 100 YEAR WATER SURFACE ELEVATION OF THE BASIN, AND ADDED NORTH BERM ON SHEET CU-SP01-S01. ADDED EMERGENCY SPILLWAY CENTERLINE PROFILE AND ADDED NORTH BERM TO CROSS SECTION A-A' ON SHEET CU-GS01-S01. ADDED CULVERTS AND HEADWATER ELEVATIONS TO ACCESS ROAD B PROFILE AND ADDED TYPICAL CULVERT C UNDER ACCESS ROAD B DETAIL ON SHEET CU-RP01-S01. UPDATED DITCH DIMENSION TABLE TO PROVIDE 100 YEAR STORM MAXIMUM WATER DEPTH ON SHEET CU-ED02-S01. UPDATED CULVERT DATA CHART TO PROVIDE VELOCITIES AND ADDED CONSTRUCTION ENTRANCE AS PART OF THIS PROJECT ON SHEET CU-ED02-S02. UPDATED CITY OF NEW ALBANY STANDARD NOTES ON SHEET CU-GN02-S01 AND SHEET CU-GN02-S02.		DB	JP		

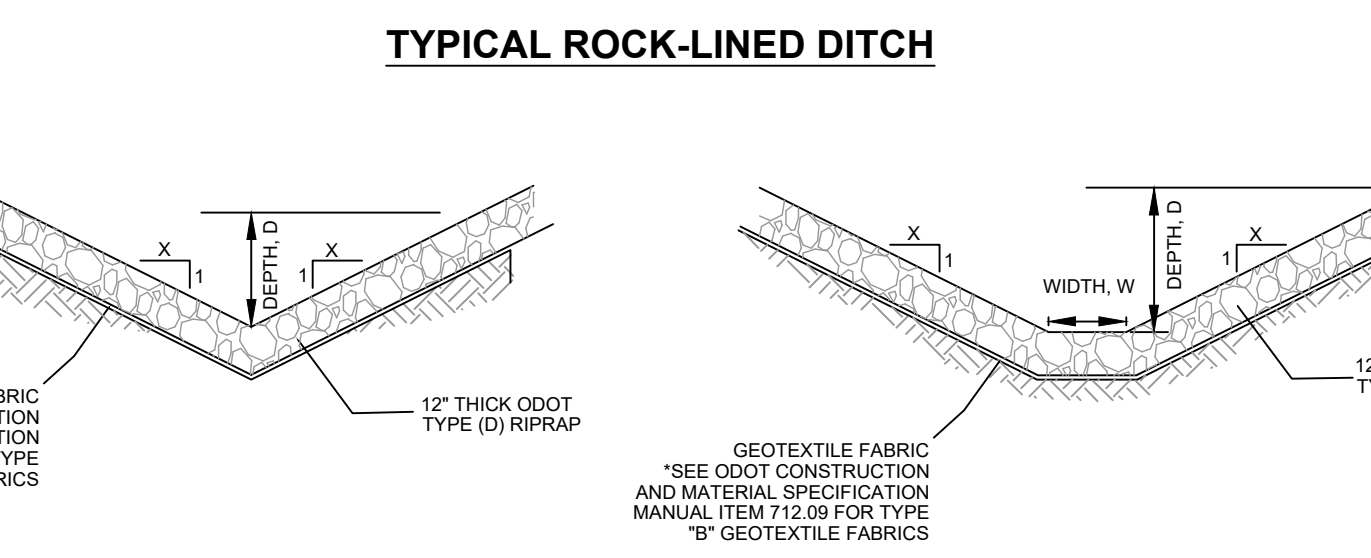
TYPICAL ENERGY DISSIPATING ROCK APRON (RA)



TYPICAL ENERGY DISSIPATING ROCK APRON
PLAN VIEW
NOT TO SCALE



TYPICAL ENERGY DISSIPATING ROCK APRON
SECTION A-A'
NOT TO SCALE

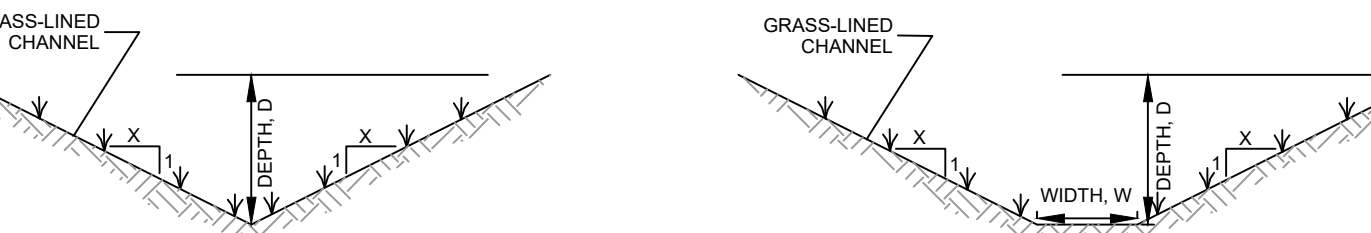


TYPICAL ROCK-LINED DITCH
SECTION A-A'
NOT TO SCALE

V-BOTTOM DITCH
NOT TO SCALE

FLAT BOTTOM DITCH
NOT TO SCALE

TYPICAL VEGETATED DITCH



TYPICAL VEGETATED FLAT BOTTOM DITCH
NOT TO SCALE

DITCH DIMENSION TABLE

DITCH	SLOPE	MIN. DEPTH, D (ft.)	MIN. WIDTH, W (ft.)	MIN. SIDE SLOPES (H : V)	CHANNEL LINING	100 YEAR, 24 HOUR STORM MAXIMUM WATER DEPTH (ft.)
A	0.4%	2.0	0.0	3 : 1	ODOT TYPE (D) RIPRAP	1.25
B	0.3%	2.0	0.0	3 : 1	ODOT TYPE (D) RIPRAP	1.26



BAIR GOODIE BAIR, GOODIE AND ASSOCIATES, INC. 433 NORTH BROADWAY STREET NEW PHILADELPHIA, OH 44663 TEL: 330.343.3499 FAX: 330.343.3505 WWW.BAIRGOODIE.COM		UNDERGROUND UTILITIES TWO WORKING DAYS CALL BEFORE YOU DIG Call: 800-362-2754 (Toll Free) OHIO UTILITIES PROTECTION SERVICE	
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AEP OHIO TRANSMISSION COMPANY, INC.			
NEW ALBANY		SOUDEUR STATION	
OHIO			
EROSION & SEDIMENT CONTROL DETAILS - SHEET 1			
SCALE: AS NOTED	DR: DB/BGA	ENG: JP/BGA	CH: JP/BGA
WOW: T1059317002	APPD: JP/BGA	DATE: 09/11/2024	
1 RIVERSIDE PLAZA COLUMBUS, OH 43215		DWG. NO.: CU-ED02-S01	R/V: 1

TEMPORARY SEEDING



DESCRIPTION

TEMPORARY SEEDINGS ESTABLISH TEMPORARY COVER ON DISTURBED AREAS BY PLANTING APPROPRIATE RAPIDLY GROWING ANNUAL GRASSES OR SMALL GRASSES. TEMPORARY SEEDING PROVIDES EROSION CONTROL ON AREAS BETWEEN CONSTRUCTION OPERATIONS. GRASSES, WHICH ARE QUICK GROWING, ARE SEEDING AND USUALLY MULCHED TO PROVIDE PROMPT, TEMPORARY SOIL STABILIZATION. IT EFFECTIVELY MINIMIZES THE AREA OF A CONSTRUCTION SITE PRONE TO EROSION AND SHOULD BE USED EVERYWHERE THE SEQUENCE OF CONSTRUCTION OPERATIONS ALLOWS VEGETATION TO BE ESTABLISHED.

SPECIFICATIONS FOR TEMPORARY SEEDING

LBS / AC	BOTANICAL NAME	COMMON NAME
32	AVENA SATIVA	SEED OATS
10	LULIUM MULTIFLORUM	ANNUAL RYEGRASS

- STRUCTURAL, EROSION AND SEDIMENT CONTROL PRACTICES SUCH AS DIVERSIONS AND SEDIMENT TRAPS SHALL BE INSTALLED AND STABILIZED WITH TEMPORARY SEEDING PRIOR TO GRADING THE REST OF THE CONSTRUCTION SITE.
- TEMPORARY SEED SHALL BE APPLIED BETWEEN CONSTRUCTION OPERATIONS ON SOIL THAT WILL NOT BE GRADED OR REWORKED FOR 14 DAYS OR GREATER. THESE IDLE AREAS SHALL BE SEEDING WITHIN 7 DAYS AFTER GRADING.
- THE SEEDING SHOULD BE PULVERIZED AND LOOSE TO ENSURE THE SUCCESS OF ESTABLISHING VEGETATION. TEMPORARY SEEDING SHOULD NOT BE POSTPONED IF IDEAL SEEDING PREPARATION IS NOT POSSIBLE.
- SOIL AMENDMENTS—TEMPORARY VEGETATION SEEDING RATES SHALL ESTABLISH ADEQUATE STANDS OF VEGETATION, WHICH MAY REQUIRE THE USE OF SOIL AMENDMENTS. BASE RATES FOR LIME AND FERTILIZER SHALL BE USED.
- SEEDING METHOD—SEED SHALL BE APPLIED UNIFORMLY WITH A CYCLONE SPREADER, DRILL, CULTIPACKER SEEDER, OR HYDROSEEDER. WHEN FEASIBLE, SEED THAT HAS BEEN BROADCAST SHALL BE COVERED BY RAKING OR BRACING AND THEN LIGHTLY TAMPED INTO PLACE USING A ROLLER OR CULTIPACKER. IF HYDROSEEDING IS USED, THE SEED AND FERTILIZER WILL BE MIXED ON-SITE AND THE SEEDING SHALL BE DONE IMMEDIATELY AND WITHOUT INTERRUPTION.

MULCHING TEMPORARY SEEDING

- APPLICATIONS OF TEMPORARY SEEDING SHALL INCLUDE MULCH, WHICH SHALL BE APPLIED DURING OR IMMEDIATELY AFTER SEEDING. SEEDING MADE DURING OPTIMUM SEEDING DATES ON FAVORABLE, VERY FLAT SOIL CONDITIONS MAY NOT NEED MULCH TO ACHIEVE ADEQUATE STABILIZATION.

- MATERIALS:
 - STRAW—IF STRAW IS USED, IT SHALL BE UNRITTED SMALL-GRAIN STRAW APPLIED AT A RATE OF 2 TONS PER ACRE OR 90 LBS/1,000 SQ. FT. (24 BALES).
 - HYDROSEEDERS—IF WOOD CELLULOSE FIBER IS USED, IT SHALL BE USED AT 2000 LBS./AC OR 496 LBS./1,000-SQ.-FT.
 - OTHER—OTHER ACCEPTABLE MULCHES INCLUDE MULCH MATTINGS APPLIED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS OR WOOD CHIPS APPLIED AT 1 TON/AC.
- STRAW MULCH SHALL BE ANCHORED IMMEDIATELY TO MINIMIZE LOSS BY WIND OR WATER. ANCHORING METHODS:
 - MECHANICAL—A DISK CRUMPER, OR SIMILAR TYPE TOOL, SHALL BE SET STRAIGHT TO PUNCH OR ANCHOR THE MULCH MATERIAL INTO THE SOIL. STRAW MECHANICALLY ANCHORED SHALL NOT BE FINELY CHOPPED BUT LEFT TO A LENGTH OF APPROXIMATELY 6 INCHES.
 - MULCH NETTING—NETTING SHALL BE USED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS. NETTING MAY BE NECESSARY TO HOLD MULCH IN PLACE IN AREAS OF CONCENTRATED RUNOFF AND ON CRITICAL SLOPES.
 - SYNTHETIC BINDERS—SYNTHETIC BINDERS SUCH AS ACRYLIC DLR (AGRI-TAC), DCA-7H, PETROSET, TERRA TRAC OR EQUIVALENT MAY BE USED AT RATES RECOMMENDED BY THE MANUFACTURER.
 - WOOD CELLULOSE FIBER—WOOD CELLULOSE FIBER BINDER SHALL BE APPLIED AT A NET DRY WT. 750 LB./AC. THE WOOD CELLULOSE FIBER SHALL BE MIXED WITH WATER AND THE MIXTURE SHALL CONTAIN A MAXIMUM OF 50 LB./100 GAL.

DUST CONTROL



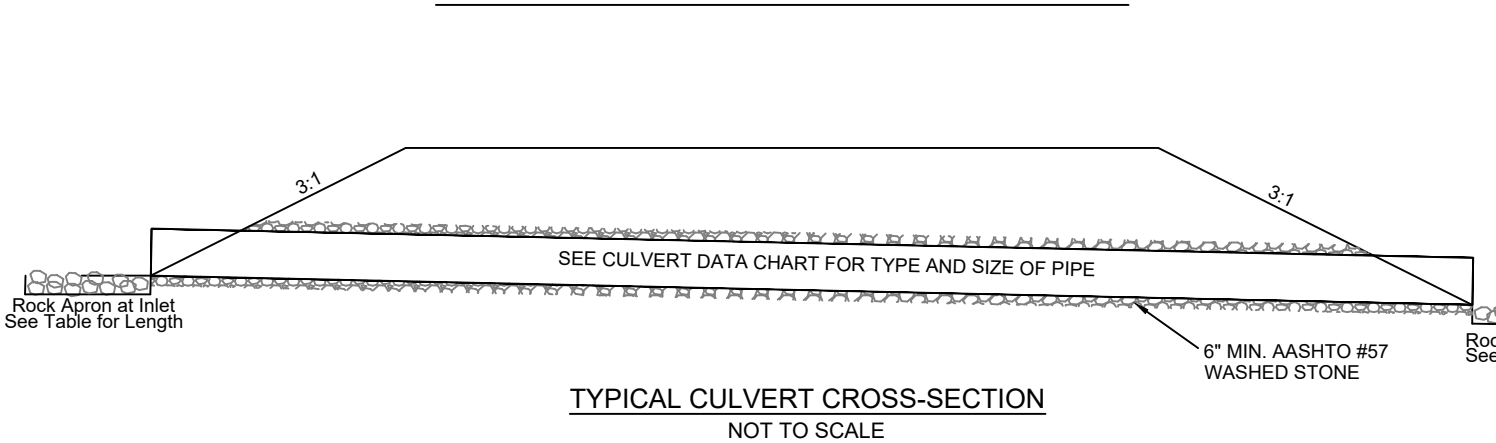
DESCRIPTION

DUST CONTROL INVOLVES PREVENTING OR REDUCING DUST FROM EXPOSED SOILS OR OTHER SOURCES DURING LAND DISTURBING, DEMOLITION AND CONSTRUCTION ACTIVITIES TO REDUCE THE PRESENCE OF AIRBORNE SUBSTANCES WHICH MAY PRESENT HEALTH HAZARDS, TRAFFIC SAFETY PROBLEMS OR HARM ANIMAL, OR PLANT LIFE.

SPECIFICATIONS FOR DUST CONTROL

- VEGETATIVE COVER AND MULCH—APPLY TEMPORARY OR PERMANENT SEEDING AND MULCH TO AREAS THAT WILL REMAIN IDLE FOR OVER 14 DAYS. SAVING EXISTING TREES AND LARGE SHRUBS WILL ALSO REDUCE SOIL AND AIR MOVEMENT ACROSS DISTURBED AREAS. SEE TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING PRACTICES, AND TREE AND NATURAL AREA PROTECTION PRACTICES.
- WATERING—SPRAY SITE WITH WATER UNTIL THE SURFACE IS WET BEFORE AND DURING GRADING AND REPEAT AS NEEDED, ESPECIALLY ON PAVED ROADS AND OTHER HEAVY TRAFFIC ROUTES. WATERING SHALL BE DONE AT A RATE THAT PREVENTS DUST BUT DOES NOT CAUSE SOIL EROSION. WETTING AGENTS SHALL BE UTILIZED ACCORDING TO MANUFACTURER'S INSTRUCTIONS.
- SPRAY-ON ADHESIVES—APPLY ADHESIVE ACCORDING TO MANUFACTURER'S INSTRUCTIONS.
- STONE—GRADED ROADWAYS AND OTHER SUITABLE AREAS WILL BE STABILIZED USING CRUSHED STONE OR COARSE GRAVEL AS SOON AS PRACTICABLE AFTER REACHING AN INTERIM OR FINAL GRADE. CRUSHED STONE OR COARSE GRAVEL CAN BE USED AS A PERMANENT COVER TO PROVIDE CONTROL OF SOIL EMISSIONS.
- BARRIERS—EXISTING WINDBREAK VEGETATION SHALL BE MARKED AND PRESERVED. SNOW FENCING OR OTHER SUITABLE BARRIER MAY BE PLACED PERPENDICULAR TO PREVAILING AIR CURRENTS AT INTERVALS OF ABOUT 15 TIMES THE BARRIER HEIGHT TO CONTROL AIR CURRENTS AND BLOWING SOIL.
- OPERATION AND MAINTENANCE—WHEN TEMPORARY DUST CONTROL MEASURES ARE USED, REPETITIVE TREATMENT SHOULD BE APPLIED AS NEEDED TO ACCOMPLISH CONTROL.
- STREET CLEANING—PAVED AREAS THAT HAVE ACCUMULATED SEDIMENT FROM CONSTRUCTION SHOULD BE CLEANED DAILY, OR AS NEEDED UTILIZING A STREET SWEEPER OR BUCKET-TYPE ROAD LOADER OR SCRAPER.

TYPICAL CULVERT CROSS-SECTION



PERMANENT SEEDING



DESCRIPTION

PERENNIAL VEGETATION IS ESTABLISHED ON AREAS THAT WILL NOT BE RE-DISTURBED FOR PERIODS LONGER THAN 12 MONTHS. PERMANENT SEEDING INCLUDES SITE PREPARATION, SEEDING PREPARATION, PLANTING SEED, MULCHING, IRRIGATION AND MAINTENANCE. PERMANENT VEGETATION IS USED TO STABILIZE SOIL, REDUCE EROSION, PREVENT SEDIMENT POLLUTION, REDUCE RUNOFF BY PROMOTING INFILTRATION, AND PROVIDE STORMWATER QUALITY BENEFITS OFFERED BY DENSE GRASS COVER.

SPECIFICATIONS FOR PERMANENT SEEDING

SITE PREPARATION

- SUBSOILER, PLOW OR OTHER IMPLEMENT SHALL BE USED TO REDUCE SOIL COMPACTION AND ALLOW MAXIMUM INFILTRATION, MAXIMIZING INFILTRATION WILL HELP CONTROL BOTH RUNOFF RATE AND WATER QUALITY. SUBSOILING SHOULD BE DONE WHEN THE SOIL MOISTURE IS LOW ENOUGH TO ALLOW THE SOIL TO CRACK OR FRACTURE. SUBSOILING SHALL NOT BE DONE ON SLOPES WHERE SOIL PREPARATION SHOULD BE LIMITED TO WHAT IS NECESSARY FOR ESTABLISHING VEGETATION.
- THE SITE SHALL BE GRADED AS NEEDED TO PERMIT THE USE OF CONVENTIONAL EQUIPMENT FOR SEEDING PREPARATION AND SEEDING.
- TOPSOIL SHALL BE APPLIED WHERE NEEDED TO ESTABLISH VEGETATION.

SEEDING PREPARATION

- LIME—AGRICULTURAL GRADE LIMESTONE SHALL BE APPLIED TO ACID SOIL AS RECOMMENDED BY A SOIL TEST. IN LBS/100 SQ. FT. LIME SHALL BE APPLIED AT THE RATE OF 100 POUNDS PER 1,000-SQ. FT. OR 2 TONS PER ACRE.
- FERTILIZER—FERTILIZER SHALL BE APPLIED AS RECOMMENDED BY A SOIL TEST. IN PLACE OF SOIL TEST, FERTILIZER SHALL BE APPLIED AT A RATE OF 25 POUNDS PER 1,000-SQ. FT. OR 1000 POUNDS PER ACRE OF A 10-10-10 OR 12-12-12 ANALYSIS.
- THE LIME AND FERTILIZER SHALL BE WORKED INTO THE SOIL WITH A DISK HARROW, SPRING-TOOTH HARROW, OR OTHER SUITABLE FIELD IMPLEMENT TO A DEPTH OF 3 INCHES ON SLOPING LAND. THE SOIL SHALL BE WORKED ON THE CONTOUR.

SEEDING DATES AND SOIL CONDITIONS

SEEDING SHOULD BE DONE MARCH 1 TO MAY 31 OR AUGUST 1 TO SEPTEMBER 30. IF SEEDING OCCURS OUTSIDE OF THE ABOVE-SPECIFIED DATES, ADDITIONAL MULCH AND IRRIGATION MAY BE REQUIRED TO INSURE A MINIMUM OF 80% GERMINATION. TILLAGE FOR SEEDING PREPARATION SHOULD BE DONE WHEN THE SOIL IS DRY ENOUGH TO CRUMBLE AND NOT FORM RIBBONS WHEN COMPRESSED BY HAND. FOR WINTER SEEDING, SEE THE FOLLOWING SECTION ON DORMANT SEEDING.

DORMANT SEEDING

- SEEDING SHOULD NOT BE MADE FROM OCTOBER 1 THROUGH NOVEMBER 20. DURING THIS PERIOD, THE SEEDS ARE LIKELY TO GERMINATE BUT PROBABLY WILL NOT BE ABLE TO SURVIVE THE WINTER.
- THE FOLLOWING METHODS MAY BE USED FOR "DORMANT SEEDING":
 - FROM OCTOBER 1 THROUGH NOVEMBER 20, PREPARE THE SEEDBED, ADD THE REQUIRED AMOUNTS OF LIME AND FERTILIZER, THEN MULCH AND ANCHOR. AFTER NOVEMBER 20, AND BEFORE MARCH 15, BROADCAST THE SELECTED SEED MIXTURE, INCREASE THE SEEDING RATES BY 50% FOR THE TYPE SEEDING.
 - FROM NOVEMBER 20 THROUGH MARCH 15, WHEN SOIL CONDITIONS PERMIT, PREPARE THE SEEDBED, LIME AND FERTILIZE, APPLY THE SELECTED SEED MIXTURE, MULCH AND ANCHOR. INCREASE THE SEEDING RATES BY 50% FOR THIS TYPE OF SEEDING.
 - APPLY SEED UNIFORMLY WITH A CYCLONE SEEDER, DRILL, CULTIPACKER SEEDER, OR HYDRO-SEEDER. SURRY MAY INCLUDE SEED AND FERTILIZER ON A FIRM MOIST SEEDBED.
 - WHERE FEASIBLE, EXCEPT WHEN A CULTIPACKER TYPE SEEDER IS USED, THE SEEDBED SHOULD BE FURROW FOLLOWING SEEDING OPERATIONS WITH A CULTIPACKER, ROLLER, OR LIGHT DRAG, ON SLOPING LAND. SEEDING OPERATIONS SHOULD BE ON THE CONTOUR WHERE FEASIBLE.

MULCHING

- MULCH MATERIAL SHALL BE APPLIED IMMEDIATELY AFTER SEEDING. DORMANT SEEDING SHALL BE MULCHED. 100% OF THE GROUND SURFACE SHALL BE COVERED WITH AN APPROVED MATERIAL.
- MATERIALS:
 - STRAW—IF STRAW IS USED IT SHALL BE UNRITTED SMALL-GRAIN STRAW APPLIED AT THE RATE OF 2 TONS PER ACRE OR 90 POUNDS (TWO TO THREE BALES) PER 1,000-SQ. FT. THE MULCH SHALL BE SPREAD UNIFORMLY BY HAND OR MECHANICALLY APPLIED SO THE SOIL SURFACE IS COVERED FOR UNIFORM DISTRIBUTION OF HAND-BROADCAST MULCH. DIVIDE AREA INTO APPROXIMATELY 1,000-SQ. FT. SECTIONS AND SPREAD TWO-TO-FIVE BALES OF STRAW IN EACH SECTION.
 - HYDROSEEDERS—IF WOOD CELLULOSE FIBER IS USED, IT SHALL BE APPLIED AT 2,000 LB./AC. OR 46 LB./1,000-SQ. FT.
 - OTHER—OTHER ACCEPTABLE MULCHES INCLUDE ROLLED EROSION CONTROL MATTINGS OR BURLAPETS APPLIED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS OR WOOD CHIPS APPLIED AT 1 TONS PER ACRE.
- STRAW AND MULCH ANCHORING METHODS:
 - STRAW MULCH SHALL BE ANCHORED IMMEDIATELY TO MINIMIZE LOSS BY WIND OR WATER.
 - MECHANICAL—A DISK CRUMPER, OR SIMILAR TYPE TOOL, SHALL BE SET STRAIGHT TO PUNCH OR ANCHOR THE MULCH MATERIAL INTO THE SOIL. STRAW MECHANICALLY ANCHORED SHALL NOT BE FINELY CHOPPED BUT, GENERALLY, BE LEFT LONGER THAN 6 INCHES.
 - MULCH NETTING—NETTING SHALL BE USED ACCORDING TO THE MANUFACTURER'S RECOMMENDATIONS. NETTING MAY BE NECESSARY TO HOLD MULCH IN PLACE IN AREAS OF CONCENTRATED RUNOFF AND ON CRITICAL SLOPES.
 - ASPHALT EMULSION-ASPHALT SHALL BE APPLIED AS RECOMMENDED BY THE MANUFACTURE OR AT THE RATE OF 160 GALLONS PER ACRE.
 - SYNTHETIC BINDERS—SYNTHETIC BINDERS SUCH AS ACRYLIC DLR (AGRI-TAC), DCA-7H, PETROSET, TERRA TRAC OR EQUIVALENT MAY BE USED AT RATES SPECIFIED BY THE MANUFACTURER.
 - WOOD CELLULOSE FIBER—WOOD CELLULOSE FIBER SHALL BE APPLIED AT A NET DRY WEIGHT OF 750 POUNDS PER ACRE. THE WOOD CELLULOSE FIBER SHALL BE MIXED WITH WATER WITH THE MIXTURE CONTAINING A MAXIMUM OF 50 POUNDS CELLULOSE PER 100 GALLONS OF WATER.

IRRIGATION

PERMANENT SEEDING SHALL INCLUDE IRRIGATION TO ESTABLISHED VEGETATION DURING DRY WEATHER OR ON ADVERSE SITE CONDITIONS, WHICH REQUIRE ADEQUATE MOISTURE FOR SEED GERMINATION AND PLANT GROWTH. IRRIGATION RATES SHALL BE MONITORED TO PREVENT EROSION AND DAMAGE TO SEEDBED AREAS FROM EXCESSIVE RUNOFF.

PERMANENT SEED MIXES

NATIVE SLOPE SEED MIX*						
GRASSES						
OZ / AC	LB / AC	SCIENTIFIC NAME	COMMON NAME			
4	1	ANDROPOGON GERARDI	BIG BLUESTEM			
16	1	BOUTELOUA CURTIPENDULA	SIDE OATS GRAMA			
16	1	ELYMUS VIRGICUS	VIRGINIA WILD RYE			
32	2	SCHIZACHYRIUM SCOPARIUM	LITTLE BLUESTEM			
32	2	SORGHASTRUM NUTANS	INDIAN GRASS			
TOTAL 8						
NURSE CROP						
OZ / AC	LB / AC	AVENA SATIVA		OATS		
32	2					
FORBES						
OZ / AC	LB / AC	SCIENTIFIC NAME	COMMON NAME			
4	0.25	BIDENS ARISTATA	SHOWY (SUNFLOWER) TICKSEED			
10	0.625	CASSIA FASCICULATA	PARROTREE PEA			
6	0.375	ECHRINACEA PURPUREA	PURPLE CONEFLOWER			
0.75	0.047	MONARDA FISTULOSA	WILD BERGAMOT			
2	0.125	RATIBIDA PINNATA	GREY-HEADED CONEFLOWER			
2	0.125	RUDECKIA HIRTATA	BLACK-EYED SUSAN			
0.5	0.031	SOLIDAGO NEMORALIS	GRAY GOLDENROD			
TOTAL 1.79						

*The AEP Native Slope Mix #2023 is based on pure live seed and per acre. It should not exceed \$390.00 per acre. If the price exceeds \$550.00 per acre, please contact Amy Torkey, ajtorkey@baigoodie.com or (614) 965-1486 for clarification or modification. Do not revise seed mix without approval of AEP permit lead (WEBS-TDT). Seed mix can be adjusted for Hydro Seeding application - please contact AEP permit lead (WEBS-TDT).

LAWN MIX - SUN TO PARTIAL SHADE			
LBS / AC	GRASSES	SEED OATS	ANNUAL RYEGRASS
20	LULIUM MULTIFLORUM	ANNUAL RYEGRASS	
100	POLYTRICHUM	KENTUCKY BLUEGRASS	
100	LULIUM PERENNE	PERENNIAL RYEGRASS	

LAWN MIX - SHADE			
LBS / AC	GRASSES	SEED OATS	ANNUAL RYEGRASS
20	LULIUM MULTIFLORUM	ANNUAL RYEGRASS	
100	POLYTRICHUM	KENTUCKY BLUEGRASS	
100	FESTUCA RUBRA	CREeping RED FESCUE	

SHADE AND RETENTION AREA SEED MIX			
OZ / AC	GRASSES	SEED OATS	ANNUAL RYEGRASS
32	AVENA SATIVA	SEED OATS	
160	LULIUM MULTIFLORUM	ANNUAL RYEGRASS	

TEMPORARY MATRIX			
OZ / AC	GRASSES	SEED OATS	ANNUAL RYEGRASS
8	CAREX FRANKII	FRANK'S SEDGE	
2	ELEOCHARIS ORTOSA	BULB SPINE RUSH	
8	CAREX VULPINODEA	FOX SEDGE	
32	PANICUM VIRGATUM	SWITCHGRASS	
2	SCIRPUS ACUTILIS	HARD STEM RUSH	
FORBES			
2	ASCLEPIAS MILWAUKEE	SWAMP MILKWEED	
2	ASTER NOVAE-ANGLIAE	NEW ENGLAND ASTER	
2	EUPATORIUM PERFORIATUM	ROBINSSET	
1	HELENIUM AUTUMNALE	HELENIUM	
2	MONARDA FISTULOSA	BERGAMOT	
2	RATIBIDA PINNATA	YELLOW CONEFLOWER	
2	RUDECKIA SUBTMENTOSA	SWEET BLACK-EYED SUSAN	

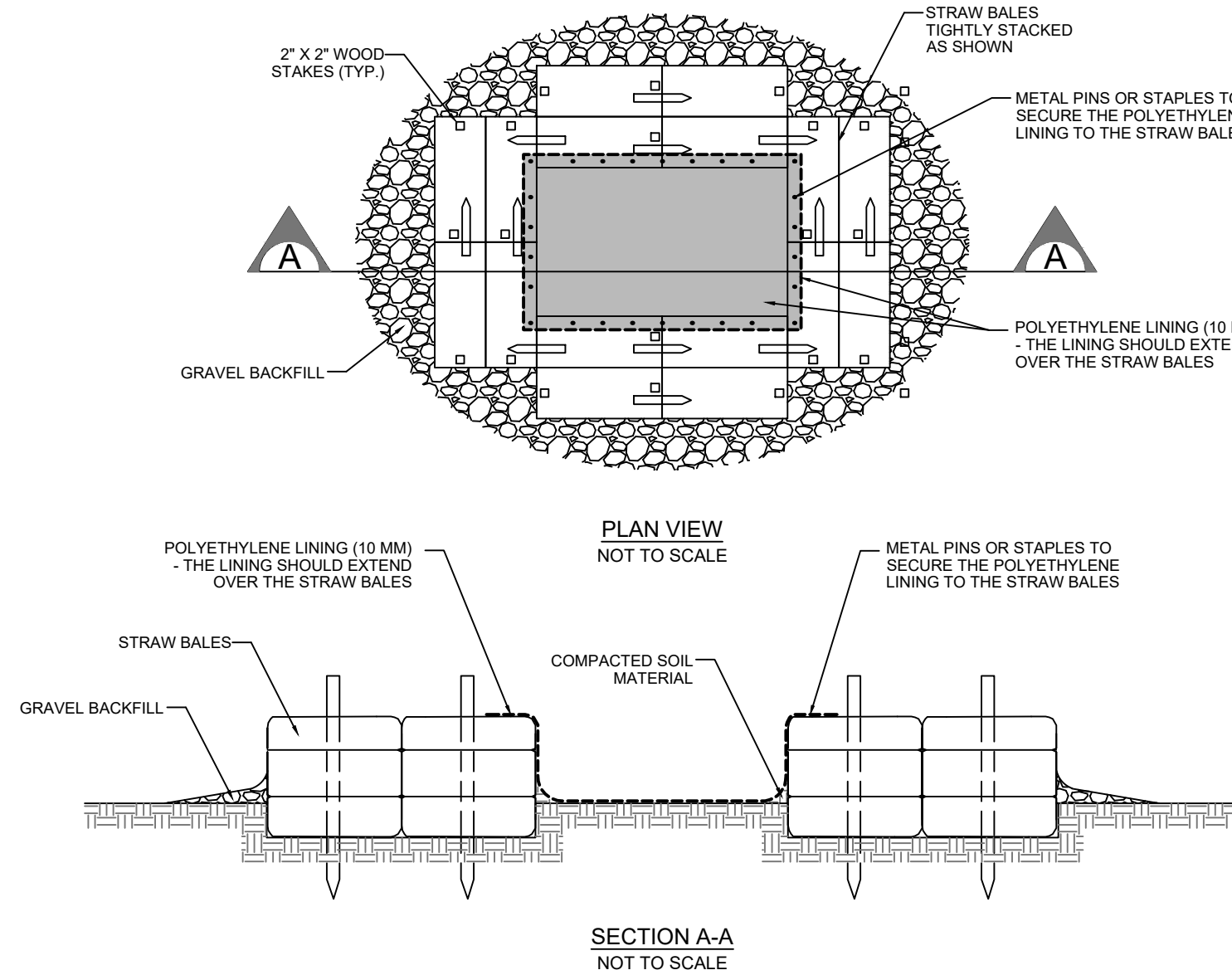
TEMPORARY MATRIX			
OZ / AC	GRASSES	SEED OATS	ANNUAL RYEGRASS
32	AVENA SATIVA	SEED OATS	
160	LULIUM MULTIFLORUM	ANNUAL RYEGRASS	

PERMANENT MATRIX			
OZ / AC	GRASSES	SEED OATS	ANNUAL RYEGRASS
64	TRIFOLIUM PRATENSE	RED CLOVER	
32	TRIFOLIUM REPENS	WHITE CLOVER	

CONCRETE WASHOUT



- THE RESIDUE OR CONTENTS OF ALL CONCRETE MIXERS, DUMP TRUCKS, OTHER CONVEYANCE EQUIPMENT AND FINISHING TOOLS SHALL BE WASHED INTO CONCRETE CLEAN-OUT STRUCTURES CONSISTING OF A POLYETHYLENE-LINED STRAW BALE BARRIER WITH GRAVEL BACKFILL. THE LENGTH AND WIDTH OF THESE STRUCTURES SHALL BE AS DETERMINED BY THE CONTRACTOR TO FACILITATE THE PARTICULAR EQUIPMENT USED. THESE STRUCTURES SHALL BE CONSTRUCTED ON LEVEL GROUND AT LEAST 100' FROM THE NEAREST WATERCOURSE, DRAINAGE SWALE OR INLET. AT NO TIME SHALL THE STRUCTURE BE ALLOWED TO BE MORE THAN 50% FULL. THE CONTRACTOR SHALL MAINTAIN THESE PONDS UNTIL ALL CONCRETE PLACEMENT IS COMPLETE FOR THE PROJECT.
- EMBED THE STRAW BALES 4" INTO THE SOIL. PROVIDE TWO ROWS OF BALES, AS SHOWN ON THE DETAIL, WITH ENDS AND CORNERS TIGHTLY BUTTING. ORIENT THE STRAW BALES LENGTHWISE WITH BINDINGS AROUND THE SIDES OF THE BALES SO THE WIRE DOES NOT CONTACT THE SOIL. DRIVE 2" X 2" WOOD STAKES THROUGH EACH BALE TO SECURELY ANCHOR THE BALE AND CONNECT ADJACENT BALES. GRAVEL BACKFILL SHALL BE PROVIDED AND TAMPED AROUND THE OUTSIDE PERIMETER OF THE BALES TO PREVENT EROSION AND FLOW AROUND THE BALES.
- THE INTENT OF THESE STRUCTURES IS TO COLLECT ALL CONCRETE WASH OUT WATER AND ALLOW IT TO DRY TO A SOLID MATERIAL. AFTER DRYING, THE SOLID MATERIAL CAN BE REMOVED WITH A LOADER OR EXCAVATOR FOR PROPER DISPOSAL. WASH OUT WILL NOT BE PERMITTED IN ANY OTHER AREAS.
- USE THE MINIMUM AMOUNT OF WATER TO WASH THE VEHICLES AND EQUIPMENT. NEVER DISPOSE OF WASH OUT INTO THE STREET, STORM INLET, DRAINAGE SWALE OR WATERCOURSE. DISPOSE OF SMALL AMOUNTS OF EXCESS DRY CONCRETE, GROUT AND MORTAR IN THE TRASH. ANY SOAPS THAT ARE UTILIZED SHALL BE PHOSPHATE-FREE AND BIODEGRADABLE.
- ADDITIONAL CONCRETE CLEAN-OUT STRUCTURES SHALL BE CONSTRUCTED WITHIN THE SPECIFIED AREA AS NEEDED BASED UPON THE VOLUME OF WASH OUT GENERATED DAILY.



CONSTRUCTION ENTRANCE



DESCRIPTION

A CONSTRUCTION ENTRANCE IS A STABILIZED PAD OF STONE UNDERLAIN WITH A GEOTEXTILE AND IS USED TO REDUCE THE AMOUNT OF MUD TRACKED OFF-SITE WITH CONSTRUCTION TRAFFIC. LOCATED AT POINTS OF INGRESS/EGRESS, THE PRACTICE IS USED TO REDUCE THE AMOUNT OF MUD TRACKED OFF-SITE WITH CONSTRUCTION TRAFFIC.

SPECIFICATIONS FOR CONSTRUCTION ENTRANCE

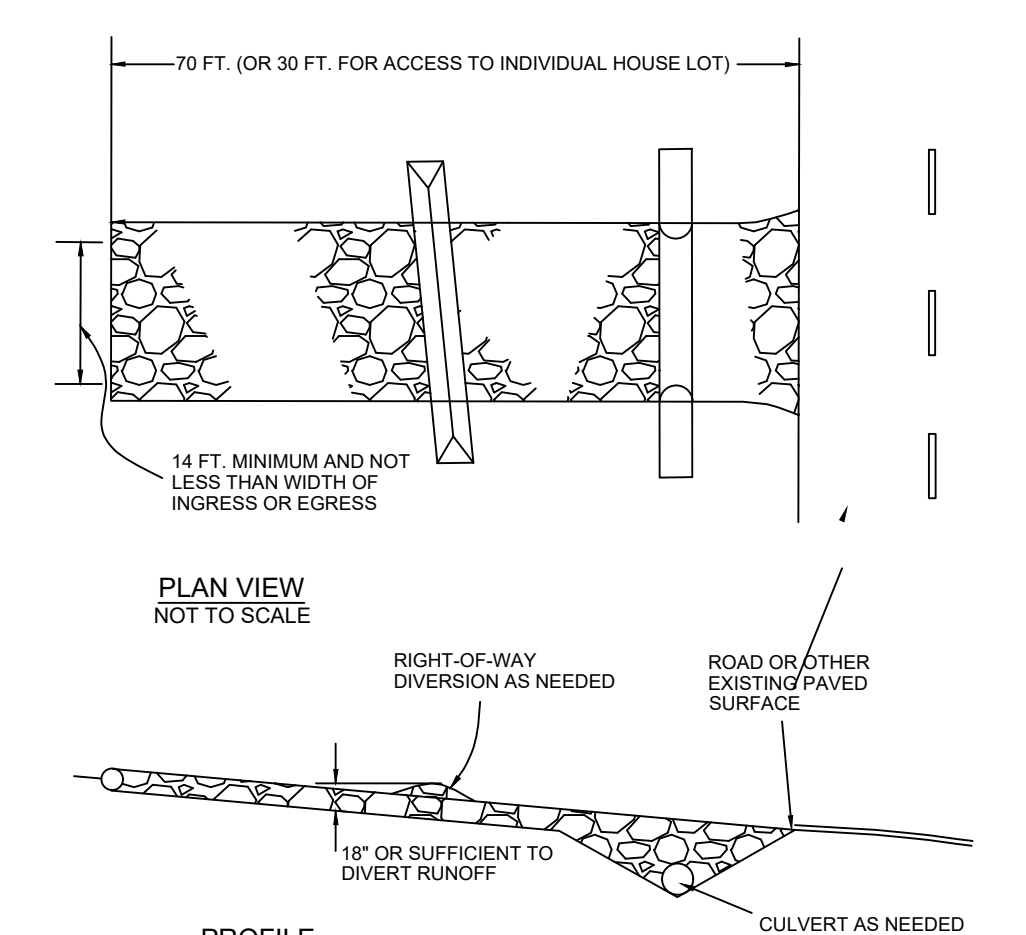
- STONE SIZE—GOOD 40/1 2-5 INCH STONE SHALL BE USED, OR RECYCLED CONCRETE EQUIVALENT.
- LENGTH—THE CONSTRUCTION ENTRANCE SHALL BE AS LONG AS REQUIRED TO STABILIZE HIGH TRAFFIC AREAS BUT NOT LESS THAN 70 FT. (EXCEPTION: APPLY 30 FT. MINIMUM TO SINGLE RESIDENCE LOTS).
- THICKNESS—THE STONE LAYER SHALL BE AT LEAST 6 INCHES THICK FOR LIGHT DUTY ENTRANCES OR AT LEAST 10 INCHES FOR HEAVY DUTY USE.
- WIDTH—THE ENTRANCE SHALL BE AT LEAST 14 FEET WIDE, BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS.
- GEOTEXTILE—A GEOTEXTILE SHALL BE LAID OVER THE ENTIRE AREA PRIOR TO PLACING STONE. IT SHALL BE COMPOSED OF STRONG ROT-PROOF POLYMERIC FIBERS AND MEET THE FOLLOWING SPECIFICATIONS:

GEOTEXTILE SPECIFICATIONS FOR CONSTRUCTION ENTRANCE	
MINIMUM TENSILE STRENGTH	200 LBS
MINIMUM PUNCTURE STRENGTH	80 PSL
MINIMUM TEAR STRENGTH	50 LBS
MINIMUM BURST STRENGTH	320 PSL
MINIMUM ELONGATION	20%
EQUIVALENT OPENING SIZE	600-6.5 MM
PERMITTIVITY	1X10-3 CMSEC.

- TIMING—THE CONSTRUCTION ENTRANCE SHALL BE INSTALLED AS SOON AS IS PRACTICABLE BEFORE MAJOR GRADING ACTIVITIES.
- CULVERT—A PIPE OR CULVERT SHALL BE CONSTRUCTED UNDER THE ENTRANCE IF NEEDED TO PREVENT SURFACE WATER FROM FLOWING ACROSS THE ENTRANCE OR TO PREVENT RUNOFF FROM BEING DIRECTED OUT ONTO PAVED SURFACES.
- WATER BAR—A WATER BAR SHALL BE CONSTRUCTED AS PART OF THE CONSTRUCTION ENTRANCE IF NEEDED TO PREVENT SURFACE RUNOFF FROM FLOWING THE LENGTH OF THE CONSTRUCTION ENTRANCE AND OUT ONTO PAVED SURFACES.
- MAINTENANCE—TOP DRESSING OF ADDITIONAL STONE SHALL BE APPLIED AS CONDITIONS DEMAND. MUD SPILLED, DROPPED, WASHED OR TRACKED ONTO PAVED ROADS, OR ANY SURFACE WHERE RUNOFF IS NOT CHECKED BY SEDIMENT CONTROLS, SHALL BE REMOVED IMMEDIATELY. REMOVAL SHALL BE ACCOMPLISHED BY SCRAPING OR SWEEPING.
- CONSTRUCTION ENTRANCES SHALL NOT BE USED TO REMOVE MUD FROM VEHICLES AND PREVENT OFF-SITE TRACKING. VEHICLES THAT ENTER AND LEAVE THE CONSTRUCTION SITE SHALL BE RESTRICTED FROM MUDDY AREAS.
- REMOVAL—THE ENTRANCE SHALL REMAIN IN PLACE UNTIL THE DISTURBED AREA IS STABILIZED OR REPLACED WITH A PERMANENT ROADWAY OR ENTRANCE.

NOTES:

- FOR THIS PROJECT THE CONSTRUCTION ENTRANCE WILL HAVE A MINIMUM LENGTH OF 100 FEET.
- CONTRACTOR IS REQUIRED TO HAVE ACCESS TO A VACUUM STREET SWEEPER ON A DAILY BASIS.



TEMPORARY SILT FENCE



- SILT FENCE SHALL BE CONSTRUCTED BEFORE UPSLOPE LAND DISTURBANCE BEGINS.
- ALL SILT FENCE SHALL BE PLACED AS CLOSE TO THE CONTOUR AS POSSIBLE SO THAT WATER WILL NOT CONCENTRATE AT LOW POINTS IN THE FENCE AND SO THAT SMALL SWALES OR DEPRESSIONS THAT MAY CARRY SMALL CONCENTRATED FLOWS TO THE SILT FENCE ARE DISAPPEARED ALONG ITS LENGTH.
- ENDS OF THE SILT FENCES SHALL BE BROUGHT UPSLOPE SLIGHTLY SO THAT WATER PONDING AT THE SILT FENCE WILL BE PREVENTED FROM FLOWING AROUND THE ENDS.
- SILT FENCE SHALL BE PLACED ON THE FLATTEST AREA AVAILABLE.
- WHERE POSSIBLE, VEGETATION SHALL BE PRESERVED FOR 5 FEET (OR AS MUCH AS POSSIBLE) UPSLOPE FROM THE SILT FENCE. IF VEGETATION IS REMOVED, IT SHALL BE REESTABLISHED WITHIN 7 DAYS FROM THE INSTALLATION OF THE SILT FENCE.
- THE HEIGHT OF THE SILT FENCE SHALL BE A MINIMUM OF 16 INCHES ABOVE THE ORIGINAL GROUND SURFACE.
- THE SILT FENCE SHALL BE PLACED IN AN EXCAVATED OR SLICED TRENCH CUT AT A MINIMUM OF 6 INCHES DEEP. THE TRENCH SHALL BE MADE WITH A TRENCHER, CABLE LAYING MACHINE, SLICING MACHINE, OR OTHER SUITABLE DEVICE THAT WILL ENSURE AN ADEQUATELY UNIFORM TRENCH DEPTH.
- THE SILT FENCE SHALL BE PLACED WITH THE STAKES ON THE DOWNSLOPE SIDE OF THE GEOTEXTILE. A MINIMUM OF 8 INCHES OF GEOTEXTILE MUST BE BELOW THE GROUND SURFACE. EXCESS MATERIAL SHALL LAY ON THE BOTTOM OF THE 6 INCH DEEP TRENCH. THE TRENCH SHALL BE BACKFILLED AND COMPACTED ON BOTH SIDES OF THE FABRIC.
- SEAL S BETWEEN SECTIONS OF SILT FENCE SHALL BE SPUN TOGETHER ONLY AT A SUPPORT POST WITH A MINIMUM 6 INCH OVERLAP PRIOR TO DRIVING INTO THE GROUND.
- MAINTENANCE—SILT FENCE SHALL ALLOW RUNOFF TO PASS ONLY AS DIFFUSE FLOW THROUGH THE GEOTEXTILE. IF RUNOFF OVERTOPS THE SILT FENCE, FLOWS UNDER THE FABRIC (A) AROUND THE FENCE ENDS, OR IN ANY OTHER WAY ALLOWS A CONCENTRATED FLOW DISCHARGE, ONE OF THE FOLLOWING SHALL BE PERFORMED (AS APPROPRIATE): 1) THE LAYOUT OF THE SILT FENCE SHALL BE CHANGED, 2) ACCUMULATED SEDIMENT SHALL BE REMOVED, OR 3) OTHER PRACTICES SHALL BE INSTALLED.

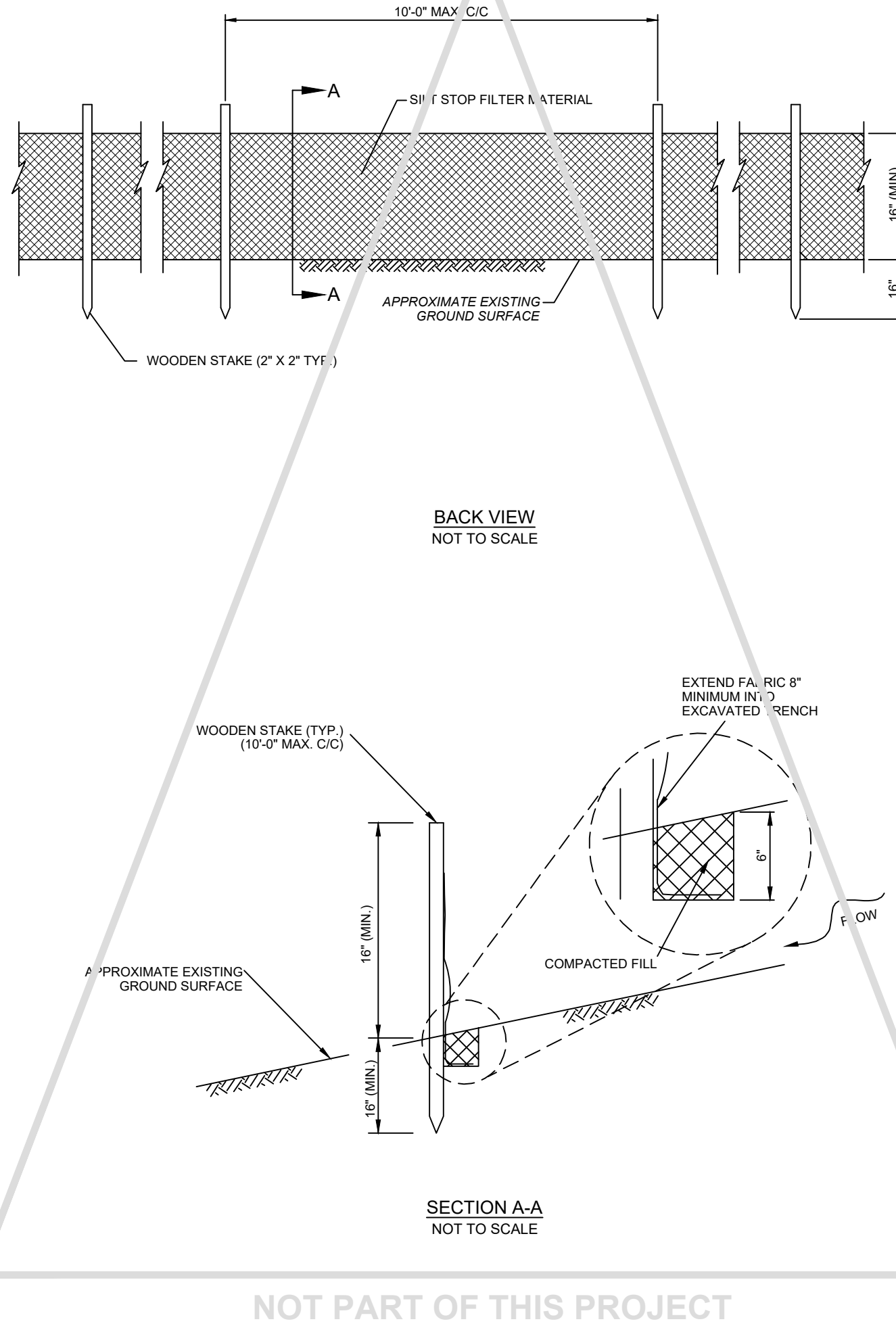
SEDIMENT DEPOSITS SHALL BE ROUTINELY REMOVED WHEN THE DEPOSIT REACHES APPROXIMATELY ONE-HALF THE HEIGHT OF THE SILT FENCE.

SILT FENCES SHALL BE INSPECTED AFTER EACH RAINFALL AND AT LEAST DAILY DURING A PROLONGED RAINFALL. THE EXISTING SILT FENCE SHALL BE REVIEWED DAILY TO ENSURE ITS PROPER LOCATION AND EFFECTIVENESS. IF DAMAGED, THE SILT FENCE SHALL BE REPAIRED IMMEDIATELY.

CRITERIA FOR SILT FENCE MATERIALS

- FENCE POST—THE LENGTH SHALL BE A MINIMUM OF 32 INCHES. WOOD POSTS WILL BE 2-BY-3 IN NOMINAL DIMENSIONED HARDWOOD, OF SOUND QUALITY. THEY SHALL BE FREE OF KNOTS, SPLITS AND OTHER VISIBLE IMPERFECTIONS THAT WILL WEAKEN THE POSTS. THE MAXIMUM SPACING BETWEEN POSTS SHALL BE 10 FEET. POSTS SHALL BE DRIVEN AT A MINIMUM 18 INCHES INTO THE GROUND, WHERE POSSIBLE. IF NOT POSSIBLE, THE POSTS SHALL BE ADEQUATELY SECURED TO PREVENT OVERTURNING OF THE FENCE DUE TO SEDIMENT/WATER LOADING.
- SILT FENCE FABRIC—SEE CHART BELOW:

FABRIC PROPERTIES	VALUES	TEST METHOD
MINIMUM TENSILE STRENGTH	120 LBS (220N)	ASTM D 4832
MINIMUM ELONGATION AT 60 LBS	20%	ASTM D 4832
MINIMUM PUNCTURE STRENGTH	20 LBS (220N)	ASTM D 4833
MINIMUM TEAR STRENGTH	40 LBS (180N)	ASTM D 4833
APPARENT OPENING SIZE	< 64MM	ASTM D 4751
MINIMUM PERMITTIVITY	< 110-2 SEC-1	ASTM D 4481
UV EXPOSURE STRENGTH RETENTION	70%	ASTM G 4355



														INLET						OUTLET													
CULVERT ID		WATERSHED		DIA. (in)		LENGTH (ft)		INLET ELEV (ft)		OUTLET ELEV (ft)		SLOPE (%)		VELOCITY (ft/sec)		PIPE MATERIAL		TYPE OF PROTECTION		LENGTH OF PROTECTION (ft)		NORTHING		EASTING		TYPE OF PROTECTION		LENGTH OF PROTECTION (ft)		NORTHING		EASTING	
A	3	0.3	Ac	12	40		1047.9		1047.7		0.4		5.2	RCP	ODOT TYPE D	6	768.142	1,882.925	ODOT TYPE D	6	768.102		1,882.922										
B	1.8	Ac	12	40		1046.7		1046.5		0.4		5.2	RCP	ODOT TYPE D	6	767.807	1,882.905	ODOT TYPE D	6	767.820		1,882.902											
C	5.6	Ac	18	60		1046.5		1046.4		0.2		6.3	RCP	ODOT TYPE D	6	767.851	1,882.973	ODOT TYPE D	6	767.801		1,882.975											

CITY OF NEW ALBANY
STANDARD NOTES

Revised
July 2024

1 GENERAL

1.1 Standards

1.1.1 The City of Columbus and Ohio Department of Transportation Construction and Material Specifications, current editions, together with the City of New Albany specifications including all supplements thereto (hereafter referred to as Standard Specifications), shall govern all construction items of these plans unless otherwise noted. If conflict between specifications is found, the more strict specification will apply as decided by the City Engineer. CMSC Item numbers listed refer to the City of Columbus Construction and Material Specifications.

1.2 Plan Modifications

1.2.1 Any modifications to the work as shown on these drawings must have prior written approval by the City Engineer, City of New Albany. Inspectors have no authority to approve revisions in the field.

1.3 Preconstruction Conference

1.3.1 A pre-construction conference involving a representative of the City of New Albany, the Owner, the Principal Contractor, and all available Sub-Contractors will be held prior to the start of construction.

1.3.2 All easements shall be recorded and submitted to the City Engineer prior to the pre-construction conference.

1.3.3 During the conference the Contractor shall submit his construction schedule, proposed schedule for controlling siltation and erosion, and for temporary and permanent seeding for the project.

1.4 Working Hours

1.4.1 City Ordinance 521.12 restricts the hours of work to 7:30 am to 7:00 pm.

1.4.2 Work will not be permitted on Sundays unless otherwise approved by the City Manager.

1.5 Inspection

1.5.1 Inspection on this project will be provided by the representatives of the City of New Albany.

1.5.2 The Owner shall deposit with the City of New Albany the total estimated costs for construction inspection prior to any construction operations.

1.14.5 If during construction of the sewer, the water wells belonging to nearby residences are dewatered, the contractor shall provide potable water to the residents. Bottled water will be provided in 4 hours and a 500 gallon water tank hooked up to the existing plumbing system will be provided within 48 hours should well service become dewatered. If the well is unable to be re-constructed after construction, a tap to a water line shall be provided if available or another well dug, at no extra cost to the residents.

1.15 Blasting

1.15.1 If the contractor intends to use blasting during excavation, the blasting shall be in accordance with the City of a New Albany Ordinance 1505.

1.16 Street Lighting

1.16.1 Contractor Requirements

(a) The contractor must register with the City of New Albany and show evidence of liability insurance and a copy of their State of Ohio license.
(b) Obtain required permits through the New Albany Service Department and Community Development Department.

1.16.2 Street Light Submittals

(a) A site development plan must be submitted by Ohio Registered Engineer to the City of New Albany Service Department for preliminary review. The plans need to show the following information:

- (1) Property lines.
 - (2) Utility and drainage easements.
 - (3) Storm drains and catch basins.
 - (4) Street light layout.
- (b) Submit three (3) copies of the standard construction drawings to Community Development for review to receive approval. Permit must be issued prior to beginning work.
- (c) Information on construction drawings are to include:
- (1) Location of light poles, disconnect switch, and power source.
 - (2) Voltage drop calculations, loads, wire size, and over-current protection.
 - (3) Photo cell location shown near or at disconnects.
 - (4) Foundation and rebar placement details for pole bases.

1.16.3 Inspection Requirements

- (a) The Contractor must schedule inspections through the Community Development.
- (b) The following inspections from the Community Development Department are required:
 - (1) Rough inspections
 - (2) Conduit Depth: (100% of conduit must be inspected before burial)
 - (3) Ground rod and rebar connections
 - (4) Rebar reinforcement of light pole foundation

1.19.2 Silt Fence or Snow Fence shall be used, if deemed necessary, to preserve the maximum amount of existing trees and vegetation.

1.20 Aggregate Base and Backfill Material

1.20.1 Aggregate base and backfill material shall be free of recycled concrete, reclaimed asphalt pavement, brick, wood or any other deleterious material that would prevent proper compaction from being achieved.

1.21 Prohibited Construction Activities

1.21.1 The contractor shall not use construction proceedings, activities or operations that may unnecessarily impact the natural environment or the public health and safety. Prohibited construction proceedings, activities or operations include, but are not limited to:

- (a) Disposing of excess or unsuitable excavated material in wetlands or floodplains, even with the permission of the property owner.
- (b) Indiscriminate, arbitrary, or capricious operation of equipment in any stream corridors, any wetlands, any surface waters, or outside the easement limits.
- (c) Pumping of sediment-laden water from trenches or other excavations into any surface waters, any stream corridors, any wetlands or storm drains.
- (d) Discharging pollutants such as chemicals, fuel, lubricants, bituminous materials, raw sewage, and other harmful waste into or alongside of rivers, streams, impoundments or into natural or man-made channels leading thereto.
- (e) Permanent or unspecified alteration of flow line of a stream.
- (f) Damaging vegetation outside of the construction area.
- (g) Disposal of trees, brush and other debris in any stream corridors, any wetlands, and surface water, or at unspecified locations.
- (h) Open burning of project debris without a permit.
- (i) Storing construction equipment and vehicles and/or stock piling construction materials on property, public or private, not previously specified by the City Engineer for said purpose.

2 SANITARY SEWER

2.1 Clean Water Connections Prohibited

2.1.1 Roof drains, foundation drains, and other clean water connections to the sanitary sewer system are prohibited on this project.

2.2 Risers

2.2.1 Service risers, Item 914, shall be installed where depth from the wey fitting to the existing or proposed surface elevation exceeds 10 feet. Top of riser shall be no more than 9 feet +/- below existing or proposed surface elevation, whichever is higher.

2.3 Sanitary Services

2.3.1 All sanitary services shall be a minimum of 2.08% grade (1/4" per foot).

1.5.3 The Contractor shall notify the City Engineer at least 48 hours prior to construction.

1.6 Work Within Public Right of Ways

1.6.1 All trenches within public right-of-way shall be backfilled according to the approved construction drawings or securely plated during non-working hours. Trenches outside these areas shall be backfilled or shall be protected by approved temporary fencing or barricades during non-working hours. Clean up shall follow closely behind the trenching operation. Trenches within City right of way shall be backfilled per item 911, City of Columbus Construction and Material Specification. Item 912 (Type 1 Only) compacted granular backfill shall be used within the 45 degree influence plane of paved surfaces.

1.6.2 The contractor shall be responsible for the condition of trenches within the right-of-way and public easements for a period of 2 (two) years from the final acceptance of the work, and shall make any necessary repairs at no cost to the City of New Albany. The Developer/Contractor shall provide a letter to the City indicating any settlement of the trenches will be repaired at their expense for a period of 5 (five) years from the date of acceptance of the subdivision or site (whichever applicable).

1.6.3 Non-rubber tired vehicles shall not be moved on public streets. The City Engineer may grant exceptions where short distances and special circumstances are involved. Granting exceptions must be in writing, and any damages must be repaired to the satisfaction of the City of New Albany.

1.6.4 No materials, including pipe, shall be stored within the public right-of-way or within one hundred (100) feet of any intersecting street or driveway. During non-working hours, storage of equipment shall comply with these same requirements. Compliance with these requirements along with additional provisions of the contract specifications shall not relieve the contractor of their legal responsibility to maintain job safety.

1.6.5 Any deteriorated pavement due to construction operations shall be saw cut and removed and replaced as per City of Columbus Standard Drawing 2130 Dr.A. The location of the saw cut shall be determined by the City Engineer in the field.

1.6.6 When a new roadway is to adjoin an existing roadway any existing underdrain is to be maintained, or replaced if not functional. A relief joint shall be constructed at the intersection of the existing and new road.

1.6.7 Ingress and egress shall be maintained at all times to public and private property. Access to all adjoining properties shall be maintained at all times.

1.6.8 Access to the site shall be provided through the construction access drive (only) as shown on the erosion control plan.

Final Inspection

- (1) Final connections at disconnect and light poles.
- (2) Demonstrate 25 OHMS or less to the ground or add a second ground rod.
- (3) Light pole finish (scratches, dents or paint defects) shall be repaired if damaged.
- (4) Final inspection demonstrating the operation of all lights

1.16.4 Installation Requirements

- (a) This work shall consist of furnishing and installing electrical materials and equipment complete and ready for service, in reasonably close conformity with locations, dimensions, and grades shown on the plans or as ordered by the City Engineer. This work shall also include necessary excavation and backfill, and disposal of discarded materials, and restoration of disturbed areas.
- (b) Foundations shall have a sleeve for the grounding electrode conductor. The connection to the ground rod shall be by exothermic welding or listed pressure connector. The ground rod shall be driven 8 feet into undisturbed earth next to the pole base.
- (c) Trenches adjacent to the pavement shall be excavated in a manner that will prevent the curb from moving or separating from the road base. Minimum distance from the curb to the ditch shall be 2 feet.
- (d) Where conduit crosses the street, a pull-box shall be installed on both sides of the street and at directional changes more than 45 degrees. No conduit runs to exceed 200' between junction points.
- (e) Conduit shall be schedule 40 PVC and shall be at a depth of at least 24".
- (f) Where, in the opinion of the Engineer, an excavation for a foundation has revealed an unstable condition at the bottom of the excavation, the foundation shall be deepened or enlarged in size as directed by the Engineer. Payment for additional quantities of excavation and foundation concrete required by the Engineer for this purpose shall be made by the Contractor. If a cave-in should occur during the excavation, the Contractor may continue excavation with use of a casing, sleeves, or other methods, with the approval of the Engineer.
- (g) Anchor bolts for light poles shall be installed in the foundations in accordance with approved shop drawings and anchor bolt setting templates. The tops of foundations shall be finished smooth and level. Anchor bolt settings for light poles shall provide that light poles predominantly illuminating a mainline roadway shall be positioned with the arm of the pole perpendicular to the longitudinal centerline of the roadway at that location. After forms have been removed, excavated spaces around the foundations shall be backfilled with suitable materials placed and tamped in thin layers as directed by the Engineer.
- (h) When pull boxes are installed in paved areas, an adequate area shall be removed by saw cutting on the sides, or by direct back to an expansion joint. The cover surface shall be adjusted to be slightly above the surrounding pavement.

2.3.2 All sanitary sewer lines installed on this project shall be in accordance with CMSC Item 901 & ASTM D-2321, or approved equal. Minimum requirements for sanitary sewer on the project shall be PVC sewer pipe ASTM D-3034 or High Density HDPE ASTM F-2726 & F-2784. All joints shall be gasketed integral bell & spigot in accordance with ASTM D-3212.

2.3.3 All sanitary sewer including service lines shall be subject to and pass infiltration or ex-filtration tests according to CMSC Item 901.20 and must be approved for use by the City Engineer before any service connections are tapped into sewers. Refer to Item 901.20 (3) for sanitary manhole testing requirements. All public sanitary sewer lids shall be stamped NEW_ALBANY OHIO SANITARY SEWER. Private sewers shall be stamped SANITARY SEWER.

2.3.4 Existing manhole shall be core cut to accept proposed sanitary sewer. All manholes shall be tested in accordance with CMSC Item 901.20.

2.3.5 All precast products shall be inspected at the location of manufacture (refer to Note Block 4).

2.3.6 Provide cut sheets in digital format to the City's inspection agency.

2.4 Deflection Testing

2.4.1 All sewer lines installed on this project using P.V.C., HDPE or HDPP pipe will be deflected tested by pulling an approved Mandrel equal in diameter to 95% of the pipe diameter through the pipe after pipe is backfilled and a sufficient amount of time is allowed for weight transfer of the backfill to the pipe and bedding, as required under CMSC Item 901.21. Testing shall be performed no sooner than 30 days after installation and backfilling.

2.5 Trench Dams

2.5.1 Cut off trench dams, in accordance with Item 901.11, shall be constructed between each pair of manholes.

2.6 Temporary Bulkheads

2.6.1 Temporary bulkheads shall be placed where indicated on the plans, and shall remain in place until removal is directed by the City Engineer.

2.7 Wye Poles

2.7.1 Wye poles shall be placed at the end of sanitary service laterals and at the end of stub manhole risers ending 5 feet or more from a manhole. The wye pole shall be placed from the pipe to at least three feet above the proposed grade. A 2 foot long minimum section of rebar shall be placed vertically alongside the wye pole 6 inches below the proposed grade. The rebar shall not be fastened in any way to the wye pole.

1.6.9 When mail boxes, road or street name signs and supports interfere with construction, the contractor shall remove and erect them in temporary locations during construction in a manner satisfactory to the City Engineer and U.S. Postal Service. After completion of the construction and before final acceptance of the project the contractor shall erect the mailboxes, road or street name signs and supports in a permanent location in accordance with the plans unless otherwise directed by the City Engineer. Removal, temporary erection and permanent erection of mailboxes shall be in accordance with U.S. Postal regulations. This work shall be performed at no cost to the City or the property owners.

1.6.10 Trenches along roadways shall be protected in accordance with the ODOT "Drop offs in Work Zones" policy copies of which are available from the Ohio Department of Transportation, Bureau of Traffic, 1980 E. Broad Street, Columbus, Ohio 43215.

1.7 Equipment on Public Roads

1.7.1 Non-rubber tired vehicles shall not be moved on public streets. The City Engineer may grant exceptions where short distances and special circumstances are involved. Granting exceptions must be in writing, and any damages must be repaired to the satisfaction of the City of New Albany.

1.8 Traffic Maintenance

1.8.1 All traffic control devices shall be furnished, erected, maintained and removed by the Contractor in accordance with the Ohio Manual of Uniform Traffic Control Devices for Construction and Maintenance Operations (current edition), copies of which are available from the Ohio Department of Transportation, Bureau of Traffic, 1980 West Broad Street, Columbus, Ohio 43215.

1.8.2 All traffic lanes shall be fully open to traffic on all public roadways. Any lane closings must be coordinated with the City Engineer at least 48 hours prior to the lane closure.

1.8.3 Steady-burning Type "C" lights shall be required on all barricades, drums, and similar devices in use at night.

1.8.4 Manual control of traffic by anyone other than a police officer is not permitted.

1.8.5 The maintenance of traffic should follow Typical Application (TA)-6 "Shoulder Work with Minor Encroachment" from the Ohio Manual of Uniform Traffic Control Devices (OMUTCD) current edition and ODOT SCD MT-101.90 for drop off requirements.

1.8.6 The minimum lane width of 10 feet must be maintained if the work zone encroaches in to the traveled lane. If this requirement cannot be met, the lane must be closed and flaggers employed following Typical Application (TA)-10 "Lane Closure on a Two-Lane Road Using Flaggers" from the Ohio Manual of Uniform Traffic Control Devices (OMUTCD) current edition.

1.16.5 General Requirements

(a) Street lighting illumination and installation shall meet the New Albany Standards.

Luminaire supports shall be a Holophane brand Hallbrook Series, with a 15' pole plus goose arm(s).

- Reference for single head pole is Model HLBK ALN 15 1A QSM CMC AGB.
- Double Head pole (twin Goosenecks) shall be Model HLBK ALN 15 2A QSM CMC AGB.
- Clam Shell Pole Base shall be Model GWBAS12RPP99P335.
- Pole and Base shall be factory painted New Albany Green (Paint Reference PMS 5535).

Luminaires shall be:

- Holophane Brand Glasswicks LED Hallbrook Model G3L3 P40 40K MVOLT ASY QSM CMC.
- Color Temperature 3000K.
- Photocontrol receptacle reference PR3.
- 120-277V.
- Luminaire housing shall be factory painted New Albany Green (Paint Reference PMS 5535).

- (1) This work shall consist of furnishing and installing electrical materials and equipment complete and ready for service, in conformity with the locations, dimensions, and grades shown on the plans or as ordered by the Engineer. This work shall also include necessary excavation and backfill, and disposal of discarded materials, and restoration of disturbed facilities and surfaces.
- (2) Each system shall conform as to voltage, amperage, frequency and type as specified by design. The Contractor shall furnish and install all incidentals necessary to provide a complete and practical working unit or system. All installations shall be in accordance with the National Electrical Code and shall also conform to local laws and codes governing such work. The Contractor shall obtain and pay for all permits required. In order to provide the necessary requirements for the proposed lighting system, the Contractor shall cooperate with the agency which will furnish electrical service also hereinafter referred to as the supplying agency.
- (3) Light poles conforming to approved shop drawings shall be set in the ground, erected up on the completed concrete foundations or other specified type of mounting. Light poles shall be plumbed. After erection, each light pole shall be adequately grounded and shall have hand hole covers or transformer base doors fastened in place. After erection, painted poles shall be inspected for defects in the painted surfaces. Minor scratches shall be given two coats of matching paint. The second coats

2.8 Manhole Coring

2.8.1 The contractor shall furnish all material, equipment, and labor to make connections to existing manholes. The sewer pipe to manhole connections for all sanitary sewers shall be flexible and watertight. All holes shall be neatly corod. The sewer pipe barrel at the springline shall not extend more than 1-inch beyond the inside face of the manhole. Any metal that is used shall be Type 300 Series Stainless Steel. The connection may be any of the following:

1. Rubber Sleeve with Stainless Steel Banding.
 - a. Kor-N-Seal as manufactured by National Pollution Control Systems, Inc.
 - b. Lock Joint Flexible Manhole Sleeve as manufactured by Interpace Corporation.
 - c. Or equal as approved by the City Engineer.
2. Rubber Gasket Compression.
 - a. Press Wedge II as manufactured by Press-Seal Gasket Corporation.
 - b. Dura Seal III as manufactured by Dura Tech, Inc.
 - c. Link-Seal as manufactured by Thunders Corporation.
 - d. Or equal as approved by the City Engineer.

2.9 Sewer Inspection

2.9.1 See note block 4.14 for inspection requirements.

3 STREETS

3.1 Concrete Base Construction

3.1.1 In addition to the requirements set forth in the City of Columbus Specifications, the following shall apply:

- a) No water shall be added to the concrete while in the mixers unless specifically authorized by the City Engineer or his representative.
- b) Subgrade shall be at proper moisture content prior to base construction. Water shall be added to the subbase if necessary.
- c) Concrete exceeding a 4" slump or being on the truck for 60 minutes or more will be rejected from the project.

3.2 Street Pre-Construction Conference

3.2.1 Prior to street construction a pre-construction conference shall be held at the City Hall with the owner and superintendent/foreman of the base, curb and asphalt sub-contractors. The pre-construction conference shall be scheduled by the contractor for 48 hours prior to the pouring of the curb. The purpose of the meeting is to ensure a 6" curb height is provided upon the completion of the street system.

3.3 Transverse & Longitudinal Joints

3.3.1 Transverse contraction and longitudinal joints shall be constructed as per 305.01 paragraph (C) & (D). (Including 26' pavement)

1.8.7 This operation may be performed at any time, except during peak hours (7am – 9am and 4pm-6pm).

1.8.8 If in the opinion of the City Engineer, the Contractor fails to comply with these requirements and the provisions of the approved maintenance of traffic plan, the City Engineer shall suspend work until all requirements are met. Any costs or delays incurred as a result of the failure shall be the full responsibility of the Contractor.

1.8.9 The following devices must meet NCHRP 350 or MASH-08 before the devices are installed on the project: drums, cones, vertical panels and the panel support, portable sign supports, temporary impact attenuators, temporary concrete barrier, and barricades.

1.8.10 Payment for all traffic maintenance items shall be included within the price bid for the project improvements.

1.8.11 All permanent traffic controls not in conflict with the temporary controls shall be maintained throughout this project by the Contractor. Permanent traffic controls may be temporarily relocated, as approved by the Engineer. The Contractor shall assume all liability for missing, damaged and improperly placed signs.

1.8.12 The Contractor shall be responsible for the reinstallation and/or replacement of all permanent traffic control devices damaged or removed during the construction. Permanent traffic control no longer in conflict with temporary traffic control shall be replaced immediately.

1.9 Existing Traffic Sign Maintenance

1.9.1 Special care shall be taken to maintain existing signs. If necessary, the contractor shall relocate these signs out of the way of construction, but in conformance with OMUTCD. Any damaged signs shall be replaced at the expense of the contractor.

1.10 Local Access

1.10.1 Ingress and egress shall be maintained to all residential and commercial properties. Driveway closures may be necessary to enable work on or in front of a drive. The contractor will be responsible for notifying owners, residents, or business operators in writing at least 48 hours but not more than 72 hours prior to closure. The engineer shall be given a list of the persons that were given notices with the date of notice included. Closure is permitted only during work hours and access must be returned at the end of each working day. Properties with multiple drives may have one drive closed at a time, while work is performed in the area of the closed drive. Individual drive closures shall be kept to the minimum time needed for construction activities. Every effort must be made to accommodate the owner's need for access.

shall not be applied until after the first coat has adequately dried. Poles having major scratches or defects in the painted surfaces will not be accepted.

- (4) The contractor shall furnish all of the materials in accordance with the listed specifications. The equipment list and receipts shall be delivered to the Service Department. A copy of the receipt shall be provided to the City Engineer.
- (5) The contractor shall provide the required number of poles complete with light fixture, bulb, wiring, and conduit in accordance with the City Engineer. The contractor shall deliver to the Service Department and a copy of the receipt shall be provided to the City Engineer.
- (6) Street fixtures shall be controlled to operate at the same time when in close proximity or on the same street in the areas they serve. Some areas may require a single photocell for each light, while others may be joined to one photocell. In no case shall there be more than 6 lights on a photocell. The photo controller shall be placed near the disconnect box.

1.16.6 Material Specifications

- (a) Disconnect box for a 120 rated current circuit shall be mounted to a 4x6 treated lumber pole containing a circuit breaker and have a lockable door. The box needs to be a minimum of 24 inches above final grade. Disconnect box for a 480 volt circuit shall be stainless steel in material and mounted to a concrete footer. The box shall be a minimum of 30 inches tall, 18 inches wide, and 15 inches deep. The concrete footer shall exceed 4 inches in all directions beyond base of disconnect box. The access door on disconnect shall be a minimum of 16 inches wide by 23 inches tall. The door shall have a latching handle that can be locked by padlock, and hinged on one side.
- (b) Wiring for a 120 volt circuit to the pole and/or disconnect shall be 6 gauge in size, copper conductor, and have a USE jacketing or equivalent thickness. Wiring going up all poles to the disconnect shall be 10 gauge stranded copper wire. The hot lead shall have a black jacket, neutral lead shall have a white jacket, and the ground lead shall have a green jacket.
- (c) Each electrical circuit shall have a fuse in the pole base. The fuse holder must be capable of accepting its avg on line side and 10 gauge on load side. 480 volt circuits must be capable of passing power to another pole on the line side of the holder.
- (d) Pull boxes in residential areas shall be 18 inches long, 12 inches wide and 18 inches deep in size or equivalent. All 480 volt circuit pull boxes shall be traffic rated. The 480 volt boxes shall be 25 inches long, 16 inches wide, and 18 inches deep in size or equivalent. All pull boxes must have the word "electrical" embossed on the cover of the box. Plates attached to the cover will not be accepted. All pull boxes must be a minimum of curb height or final grade.

3.3.2 No transverse joints shall be permitted adjacent to a new pavement surface which is more than 24 hours old, weather permitting, except for joints which have existed over weekends and holidays. The surface course shall be continuous to the existing pavement surface.

3.3.3 The contractor shall provide a written procedure on how he/she intends to construct the final two courses of asphalt prior to construction for approval by the City Engineer. The procedure should include specifics for construction of intersections.

3.4 Curb Height

3.4.1 When constructing the pavement (concrete base to asphalt courses) the contractor shall ensure that a 6" height curb is available upon completion of street construction. The City may require this curb to be removed and reconstructed if this height deviates more or less than 1/4" of the 6" required height. All costs associated with the above shall be borne by the contractor.

3.5 Crack Sealing

3.5.1 The contractor, thirty (30) days prior to project acceptance by City Council or as directed by the City Engineer and weather permitting shall crack seal all pavement cracks as directed by the City Engineer. The crack seal shall be in accordance with Item 423. If acceptance occurs in winter months, crack seal may be delayed until weather permits.

3.6 Pavement Relief Joints

3.6.1 Asphalt shall not be placed in the pavement relief joints until permanent or temporary street signs are erected.

3.7 Curb Stamps

3.7.1 During installation, curb shall be stamped with the following symbols at the noted utilities:

- "X" – Utility Crossing
- "T" – Sump Pump Junction Box
- "W" – Water Service
- "WV" – Water Valve
- "S" – Sanitary Sewer Crossing

3.8 Detectable Warnings

3.8.1 Type A detectable warning shall be installed as per CQC Std. Dwg. 2319. Material shall be pre-cast manufactured 4"x8"x25" red clay brick.

4 STORM SEWER

4.1 Storm Sewer Pipe and Structures

4.1.1 Pipe specification for the plan improvements may be in accordance with the following (Except as designated within the profiles.)

NO		DATE	14	REVISION DESCRIPTION					APPR	DR	ENG	CK	ISSUE#
1		03/26/2025		ADDED POST CONSTRUCTION WATER QUALITY NOTE. ADDED NOI NUMBER, AND UPDATED QUANTITIES ON SHEET CU-GS01-S01. ADDED NOTE TO SHEET CU-EC02-S01 AND SHEET CU-SL01-S01. ADDED IMPERVIOUS AREAS TABLE TO SHEET CU-SL01-S01. ADDED POST CONSTRUCTION BMP TABLE. ADDED MAJOR FLOOD ROUTING PATH. ADDED 100 FOOT MINIMUM CONSTRUCTION ENTRANCE NOTE, LABELED THE 100 YEAR WATER SURFACE ELEVATION OF THE BASIN, AND ADDED NORTH BERM ON SHEET CU-GP01-S01. ADDED EMERGENCY SPILLWAY CENTERLINE PROFILE AND ADDED NORTH BERM TO CROSS SECTION A-A ON SHEET CU-GS01-S01. ADDED CULVERTS AND HEADWATER ELEVATIONS TO ACCESS ROAD B PROFILE AND ADDED TYPICAL CULVERT C UNDER ACCESS ROAD B DETAIL ON SHEET CU-RP01-S01. UPDATED DITCH DIMENSION TABLE TO PROVIDE 100 YEAR STORM MAXIMUM WATER DEPTH ON SHEET CU-ED02-S01. ADDED CULVERT DATA CHART TO PROVIDE VELOCITIES AND ADDED CONSTRUCTION ENTRANCE AS PART OF THIS PROJECT ON SHEET CU-ED02-S02. UPDATED CITY OF NEW ALBANY STANDARD NOTES ON SHEET CU-GN02-S01 AND SHEET CU-GN02-S02.									

OLD DWG #:

STD DWG #:

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JOHN H. BAIR
3/26/25

NEW ALBANY

OHIO

CITY OF NEW ALBANY STANDARD NOTES - SHEET 1

SCALE AS NOTED

DR: DB/BGA

ENG: JP/BGA

CH: JP/BGA

WOF: T10593117002

APPD: JP/BGA

DATE: 09/11/2024

1 RIVERSIDE PLAZA
COLUMBUS, OH 43215

DWG. NO.

CU-GN02-S01

R
V
1

(E-1119)

A) Reinforced concrete pipe ASTM C-76 (CMSC 706.02). Concrete classification shall be in conformance with the following unless otherwise referenced by the profiles.
- 12" -15" diameter Class IV- 18" – 24" diameter Class III
- 27" and larger diameter Class II, or

B) High Density Polypropylene, HDPP 12" – 60" Polypropylene Double Wall ASTM F 2736 12" thru 30" and ASTM F- 2881 36" thru 60" with integral bell & spigot meeting the watertight requirements of ASTM D 3212 (CMSC 720.13 & ODOT 707.65), or

C) Smooth-lined corrugated polyethylene pipe (CMSC Item 720.12) (Hancor HI-Q, ADS N-12, or equal). Except any sewers within Public R/W or as directed by the City Engineer, or

D) P.V.C. sewer pipe ASTM D3034 with joints as per ASTM D3212. PVC sewer pipe placement shall be limited to sewers through 10" diameter.

4.2 The Contractor shall provide written certification to the Engineer reflecting the pipe material to be used along with the current City assignment list identifying the approved pipe material specification.

4.3 All bedding shall be in accordance with Standard Drawing AA-S151 for rigid pipe sewer and in accordance with Standard Drawing AA-S149 for flexible pipe sewer.

4.4 The cost of compacted backfill shall be included in unit price bid for Item 901. Concrete encasement will be required (CCMS 901.12) where 30" of cover is not maintained. Cost to be included in unit price bid for Item 901.

4.5 All public manhole castings shall be stamped NEW ALBANY OHIO STORM. Temporary casting tops may be used until such are made available. Private manhole lids shall be stamped Storm Sewer.

4.6 All pre-cast concrete products shall be inspected at the location of manufacture. Approved pre-cast concrete products must be stamped or have such identification noting that inspection has been performed by the City of Columbus. Pre-cast concrete products without proof of inspection shall not be approved for installation.

4.7 The contractor shall submit a copy of the plans and a list of proposed pre-cast concrete product manufacturers to the City of Columbus Construction Inspection Division before commencing construction. Send the information to the following address:
Construction Inspection Division
City of Columbus
1800 East 17th Avenue
Columbus, Ohio 43219

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coordinates are required on the water main every 200' where no fitting or other water main structure is being installed within that length of the improvement.

5.12.2 All survey coordinates shall be referenced to the applicable County Engineer's Monuments, and shall be based on the North American Datum of 1983 (NAD 83) with the NSRS2007 adjustment, with further reference made to the Ohio State Plane South Coordinate System, South Zone, with elevations based on NAVD 88 datum. All coordinates (Northing, Easting, Elevation) shall be referenced to the nearest hundredth (NXXXXXXX.XXXXXXXX, Elev. XXXXX). All survey coordinates shall be accurate to within 1.0 foot horizontal and a tenth of a foot (0.10) or less vertical.

5.12.3 The coordinates shall be documented to the Municipality Engineer or designated Representative in digital spreadsheet form and shall include the applicable Item, Station, Northing, Easting, and elevation. Coordinates shall be submitted to the Municipality Engineer or designated Representative on a bi-weekly basis. Coordinates shall also be submitted to the Division of Power and Water as part of the request for chlorination (See Note Block 5.6).

5.12.4 Lump sum payment is full compensation for all work involved in obtaining and documenting the survey coordinates as described in this specification.

5.13 The Contractor must receive pre-approval from the Division of Water and City Engineer 48 hours in advance if elimination of bends is proposed and joint deflection is utilized instead.

5.14 Special Notes (If Applicable)

5.14.1 All water line valve boxes, service boxes, test stations, pitometer tap structures, meter pit covers, and other surface utility structures within the disturbed area shall be adjusted to grade. Any of these structures located within pavement, driveways, or other traveled areas, whether existing or proposed, shall be equipped with a traffic rated, heavy duty valve box and/or cover in accordance with the Standard Drawings. Existing water service boxes to remain that are encountered within the project limits shall be cleaned out, centered over the curb stop, and adjusted to the proposed grade.

5.14.2 Where new conduit is proposed to cross an existing or proposed water main or water service, a minimum of 12-inch of vertical clearance shall be maintained between the conduit and the water main or service. A minimum of 3-feet of horizontal clearance (out to out) is required at locations where the conduit is parallel to the water main and at locations of water line thrust blocks.

5.14.3 A minimum of 3 feet horizontal clearance (out to out) shall be maintained between all existing water mains and foundations for poles, pull boxes, push button pedestals, and any other miscellaneous electrical structure.

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UTILITY	OWNER	TELEPHONE
Electric/FOC	AEP 8500 Smiths Mill Road New Albany, OH 43054	(614) 714-1000
Sanitary Sewer, & Storm Sewer & Water & Fiber Optic Cable (FOC)	City of New Albany Service Department 7800 Bevelhymmer Road New Albany, OH 43054	(614) 855-0076
Gas	Columbia Gas of Ohio, Inc. 3550 Johnny Appleseed Court Columbus, OH 43231	(614) 280-7300

11 TREES

11.1 All branches or growth from trees that are to be saved and which are interfering with the grading operation may be removed by the use of pruning tools. All pruning tools used and methods employed shall meet with the approval of the City Arborist. The branches shall be removed with a good clean cut made flush with the parent trunk or if having a good healthy lateral branch, the cut shall be a good clean slanting cut close to and beyond the healthy branch. All pruning cuts shall be painted with an accepted pruning preservative. All branches removed shall be at the direction of the City Arborist (614) 855-0076. The cost of all work and expenses connected with the removal of trees and/or branches shall be included in the price bid for cleaning and grubbing. No extra payment shall be made therefore.

12 Benchmarks and Survey Monuments

12.1 Do not disturb any Franklin County or Licking County certified benchmarks (vertical and/or horizontal) located within the working limits of the project. The Contractor shall contact either the Franklin County Survey Department (614) 462-3026 or Licking County Survey Department (740) 670- 5280, prior to construction, to coordinate the proper procedures for resetting, relocation, or replacement of any Franklin County Certified Benchmark or Survey Monument.

12.2 The Contractor shall reference all iron pins and monuments before excavating at or near said iron pins or monuments. The contractor shall not disturb existing right-of-way or property corner markers that are required to remain after construction. If any pins or monuments are disturbed, destroyed, or damaged by the Contractor that have not been designated to be removed in these plans, they shall be accurately replaced by a Registered Surveyor at the completion of the project or at the direction of the City

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4.8 Openings shall be provided in the drainage structures to accommodate underdrain outlets.

4.9 All storm structures with a depth greater than four feet shall have steps (AA-S119) installed at 16" intervals maximum.

4.10 All standard catch basins and curb inlets within paved areas are to have bicycle safe grates.

4.11 When a new roadway is to adjoin an existing roadway any existing underdrain is to be maintained, or replaced if not functional. A relief joint shall be constructed at the intersection of the existing and new road.

4.12 All existing inverts along with the proposed top of casting elevations shall be verified by the Contractor prior to construction of the sewer.

4.13 Within proposed roadway sections that include straight 18" concrete curb, all frames and grates for curb and gutter inlets shall be per East Jordan 7505 Series or approved equal.

4.14 Sewer Inspection

4.14.1 The Contractor shall ensure there is a surveyor's level and rod on the project for use in performing grade checks whenever sewer line structures or pipe are being installed. The Contractor shall make this equipment available for the use of and assist the City Inspector in performing grade checks when requested by the inspector. The Inspector will make all reasonable attempts to confine requests for assistance in performing grade checks to times convenient to the Contractor.

4.14.2 These checks will be performed to ensure the following:

1. Proper placement of each structure.
2. Proper installation of initial runs of pipe from a structure.
3. Grade, after an overnight or longer shutdown.
4. Grade, at any other time the Inspector has reason to question grade of installation.

4.14.3 Grade checks performed by the City Inspector in no way relieve the Contractor for the ultimate responsibility to ensure construction to the plan grade.

4.14.4 At the request of the City Engineer, the contractor shall remove 36" storm sewer castings for inspection during construction and for final inspection.

4.14.5 Deflection Testing

All sewer lines installed on this project using P.V.C., HDPP, or H.D.P.E., pipe will be deflection tested by pulling an approved Mandrel equal in diameter to 95% of the pipe

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5.14.4 A minimum of 4 feet of cover is required prior to pressure testing any water main. A sufficient amount of backfill shall be installed to provide the adequate restraint in areas where required.

5.14.5 Proposed water mains shall be located a minimum distance of twenty (20) feet away from any structure, overhang or footer.

5.14.6 No two (2) adjacent fire hydrants shall be taken out of service concurrently.

5.14.7 Relocated fire hydrants shall be put back in service as soon as possible.

5.14.8 The Contractor shall coordinate his work such that no water customer will have their service disrupted more than two (2) times throughout the duration of this project.

5.14.9 Fire hydrant relocations shall conform to applicable sections of Item 809 of the Columbus Construction and Material Specifications. Work shall consist of removing the existing hydrant, installing new 6" pipe and fitting as required to locate the fire hydrant 2 feet from back of proposed curb or 8 feet off edge of pavement, resetting hydrant and blocking as required. All 6" pipe shall be installed at 4'0" minimum cover. Hydrant extensions shall be provided per Item 810, as required. Relocated fire hydrants shall be adjusted to proper grade and faced in proper direction. When a hydrant is relocated fifteen (15) feet or more from the "Typical Hydrant Setting" valve location (see L-8409 & L-6637), an additional valve shall be installed, and restrained, within two (2) feet of the relocated hydrant. Payment is to be included under Item 809, Fire Hydrant Relocated.

6 OWNERS NOTES IF APPLICABLE

7 EROSION CONTROL

7.1 Control of erosion and sedimentation shall be in accordance with the City of New Albany Codified Ordinance chapter 1183.

7.2 Temporary Soil Erosion and Sediment Control

7.2.1 Erosion and sediment control measures are required as a part of this project. The erosion and sediment control plan reflects a schematic diagram of the intended measures for compliance with the required standards. General practice and/or site field conditions may warrant variation in the placement or use of the specific controls. Any variations shall be approved by the City Engineer.

7.2.2 The contractor in compliance with the NPDES General Permit for Storm Water Discharge associated with construction activity and in accordance with the City of New

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Engineer and at the contractor's expense as per the City of Columbus Construction and Materials Specifications, Section 107.12. If replacement of pins or monuments is required, the Engineer, Developer, or Contractor shall provide an exhibit during the final punch list inspection verifying that monuments have been placed at all property corners.

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diameter through the pipe after pipe is backfilled and a sufficient amount of time is allowed for weight transfer of the backfill to the pipe and bedding, as required under CMSC Item 901.21. Testing shall be performed no sooner than 30 days after installation and backfilling.

4.14.6 Adjustments of manholes that would result in a chimney section greater than 24" high shall require adding another barrel section. Adjustments of manholes shall include the use of HDPE or concrete grade rings in addition to the requirements of CMS Section 604. Grade rings are not acceptable if the top of casting change in elevation exceeds nine inches. Use of brick to adjust the heights of castings is unacceptable. Payment shall be included within Item 604- Manhole Reconstructed to Grade, As Per Plan where depths of adjustments are greater than 9" and Item 604-Manhole Adjusted to Grade where depths of adjustments are less than 9".

5 WATER LINE

5.1 All water line and fire hydrant construction, material and specification shall be in accordance with "City of Columbus Construction and Material Specifications", 2018 edition and all revisions, including supplements and City of New Albany requirements including Chapter 539 of the City Code. Water main materials and installations shall be in accordance with the current rules, regulations and standard drawings of the City of Columbus, Division of Water with the exception of utilization of C900 PVC pipe. Use of C900 PVC pipe will not be permitted in New Albany unless otherwise approved by the City Engineer.

5.2 For any emergencies involving the water distribution system, please contact the Division of Water Distribution Maintenance Office at 614-645-7788.

5.3 Each fire hydrant shall be acceptable to the City of New Albany with two (2) 2-1/2" size nozzles and one (1) 5" integrated Storz fitting in place of pumper nozzle (no add-on fittings) in accordance with New Albany Fire specifications. Hydrants shall be in accordance with the CMCS. All public hydrants and nozzles shall receive 2 coats of New Albany Red (Federal Color Book 595, Color 11105). Private fire hydrants shall be painted red with white caps and bonnets. An additional fire hydrant for future maintenance purposes shall be delivered to the Public Service Department Building located at 7800 Bevelhymmer Road, New Albany, OH 43054 (Residential Subdivision Projects Only). Prior to final acceptance, fire hydrants shall be inspected and accepted by the Plain Township Fire Chief and the Public Service Department Building located at 7800 Bevelhymmer Road, New Albany, OH 43054. These inspections will be scheduled by contacting the New Albany Building Department at (614) 939-2254. All brass fittings associated with water work, including repairs to the existing system, shall conform to the revised allowable lead extraction limit per the updated NSF/ANSI 61 Standard. The Division of Water's Approved Materials List has been updated to reflect this requirement.

5.4 No water service construction before or after the water meter shall begin until permits are issued by the City of Columbus Division of Water. It shall be unlawful for

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Albany's Ordinance 1183, will be responsible for providing adequate erosion and sediment control measures along with proper maintenance and inspection. An erosion control maintenance log shall be kept on site in compliance with OEPA regulations. The log shall be available for public inspection.

7.3 Seeding

7.3.1 Temporary seeding: No area for which grading has been completed shall be left unseeded or un-mulched for longer than 14 days. If permanent seed is not applied at this time, temporary seeding shall be done at the following rates:

March 1 to August 15		
Seed:	Oats	14 lbs./1,000 sq. ft.
Fertilizer:	(12:12:12)	12-½ lbs./1,000 sq. ft.
Mulch:	(Straw or Hay)	2 tons/acre

August 15 to November 1		
Seed:	Annual Rye	14 lbs./1,000 sq. ft.
Fertilizer:	(12:12:12)	12-½ lbs./1,000 sq. ft.
Mulch:	(Straw or Hay)	2 tons/acre

November 1 to March 1		
Mulch (ONLY):	(Straw or Hay)	2 tons/acre

7.3.2 "Permanent seeding" shall be done between March 15 and September 15. If seeding is done between September 15 and March 15, it shall be classified as "Temporary Seeding". Permanent seed shall be 40% Kentucky Bluegrass, 40% Creeping Red Fescue, 20% Annual Ryegrass.

7.3.3 Permanent seeding shall consist of fertilizing, watering and seeding rates indicated under Item 659. Seeding shall be applied within two (2) days after final grading or following seed bed preparation.

Rates of application of Item 659:		
Seed:	Oats	14 lbs./1,000 sq. ft.
Fertilizer:	(12:12:12)	25 lbs./1,000 sq. ft.
Mulch:	(Straw or Hay)	2 tons/acre

7.4 Stabilization of Denuded Areas

7.4.1 Denuded areas shall have soil stabilization applied within seven days if they are to remain dormant for more than fourteen - days.

7.4.2 Sheet flow runoff from denuded areas shall be filtered or diverted to a setting facility.

any person to perform any work on City of Columbus water line systems without first securing license to engage in such work, as indicated in Columbus City Code Section 1103.02 and 1103.06. This work includes any attachments, additions to or alterations in any city service pipe or appurtenances (including water service lines and taps). This requirement may be met by utilization of a subcontractor who holds a City of Columbus Water Contractor License or a Combined Water/Sewer Contractor License to perform this work. Utilization of a subcontractor must meet the licensing requirements of City of Columbus Building Code, in particular Section 414.119 and 414.529.

5.5 Water service taps 2" and smaller shall be Type K, soft temper copper tubing conforming with the requirements of 805.03 of the CMSC. The Contractor shall obtain the proper hydrant permits), and pay any applicable fees, for any approved hydrant usage deemed necessary for work under this improvement. Permits must be obtained from the New Albany Building Department prior to contacting the Division of Water Permit Office (645-7330). The Contractor shall adhere to all rules & regulations governing said permit and must have the original permit on site anytime in which the hydrant is in use. Cost to be included in the various bid items.

5.6 All water mains shall be disinfected in accordance with Section 801.15 of the City of Columbus Construction and Material Specifications. Special attention is directed to applicable sections of AWWA C-651. When water mains are ready for disinfection, the Contractor shall submit the survey coordinates to the Design Engineer for preparation of digital as-built drawings. The Design Engineer shall then submit three (3) SETS OF THE RED LINED "As-Built" plans (with survey coordinates) to the City Engineer. The City of New Albany shall submit a letter stating that the waterlines have been pressure tested and need to be disinfected to the City of Columbus, Division of Water. The Contractor shall be responsible for all costs associated with the disinfection of all water mains constructed under this plan. All water mains shall be cleaned and flushed, and any water main 12-inch and larger must be properly pigged, in accordance with section 801.13 of the City of Columbus, Construction, and Material Specifications. Only one connection to an existing water line is permitted before disinfection of a new water line has been completed. All other connections must be made after the line has been disinfected.

The Contractor and representatives from the City of New Albany shall meet with CCCCOW staff prior to installing blow-offs and taps to obtain pre-approval.

5.6.1.1 Any section of water main that is longer than 20 feet in length shall be chlorinated. Hand swabbing methods will only be permitted for sections less than or equal to 20 feet in length. Use unscented household bleach for hand swabbing of pipe and fittings. Please note that cut-in tees, sleeves, and any other required fittings or piping shall be taken into account and are included in the total length of the section (cut to cut).

5.6.1.2 Contractor shall adhere to the requirements of the Ohio Administrative Code Chapter 3745-63.02 Water Disruption of Service Rule. Excavate pits sufficiently below

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7.4.3 Sediment Barriers such as sediment fence or diversions to settling facilities shall protect adjacent properties and water resources from sediment transportation by sheet flow.

7.4.4 Prior to Construction Operations in a particular area, all sedimentation and erosion control features shall be in place. Field adjustments with respect to locations and dimensions may be made by the Engineer.

7.4.5 The Contractor shall place inlet protection for the erosion control immediately after construction of the catch basins or inlets, which are not tributary to a sediment basin or dam.

7.4.6 It may become necessary to remove portions of the barrier during construction to facilitate the grading operations in certain areas. However, the barrier shall be in place in the evening or during any inclement weather.

7.5 Maintenance

7.5.1 It is the Contractor's responsibility to maintain the sediment control features used on this project. The site shall be inspected periodically and within 24 hours of a significant rainfall. Records of these inspections shall be kept and made available to jurisdictional agencies if requested. Any sediment or debris which has reduced the efficiency of a structure shall be removed immediately. Should a structure or feature become damaged, the Contractor shall repair or replace at no additional cost to the Owner.

7.5.2 All Erosion & Sediment Control practices are subject to Field Modification at the direction of the City Engineer and/or Ohio EPA.

8 RIGHT OF WAY PERMITS

8.1 The contractor shall have all necessary permits before beginning construction. A permit is required to bury in public right-of-way. Permits may be required from more than one governing agency. The contractor shall notify the appropriate governing agency at least forty-eight hours in advance of commencement of work. On site right-of-way, call Ohio Department of Transportation, division of Highways Permit Expediter forty-eight hours in advance.

9 PAVEMENT REPLACEMENT

9.1 If any street or road within the City is damaged as a result of construction traffic related to Construction as determined by the City Engineer, all requested repairs shall be made by the Contractor. Existing pavement surfaces shall be video taped prior to

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NO	DATE	REVISION DESCRIPTION	APPR	DR	ENG	CK	ISSUE#
1	03/26/2025	ADDED POST CONSTRUCTION WATER QUALITY NOTE. ADDED NOI NUMBER, AND UPDATED QUANTITIES ON SHEET CU-GS01-S01. ADDED NOTE TO SHEET CU-EC02-S01 AND SHEET CU-EC02-S02. ADDED IMPERVIOUS AREAS TABLE TO SHEET CU-SL01-S01. ADDED POST CONSTRUCTION BMP TABLE. ADDED MAJOR FLOOD ROUTING PATH. ADDED 100 FOOT MINIMUM CONSTRUCTION ENTRANCE NOTE. LABELED THE 100 YEAR WATER SURFACE ELEVATION OF THE BASIN, AND ADDED NORTH BERM ON SHEET CU-SP01-S01. ADDED EMERGENCY SPILLWAY CENTERLINE PROFILE AND ADDED NORTH BERM TO CROSS SECTION A-A ON SHEET CU-GS01-S01. ADDED CULVERTS AND HEADWATER ELEVATIONS TO ACCESS ROAD B PROFILE AND ADDED TYPICAL CULVERT C UNDER ACCESS ROAD B DETAIL ON SHEET CU-RP01-S01. UPDATED DITCH DIMENSION TABLE TO PROVIDE 100 YEAR STORM MAXIMUM WATER DEPTH ON SHEET CU-ED02-S01. UPDATED CULVERT DATA CHART TO PROVIDE VELOCITIES AND ADDED CONSTRUCTION ENTRANCE AS PART OF THIS PROJECT ON SHEET CU-ED02-S02. UPDATED CITY OF NEW ALBANY STANDARD NOTES ON SHEET CU-GN02-S01 AND SHEET CU-GN02-S02.		DB	JP		

the area to be connected in order to maintain water levels below the water main. If water from the pit enters the existing main, contact the Division of Water immediately. Ensure that sufficiently sized pumps are utilized to remove water from the trench and back-up pumps are kept on site for redundancy.

5.7 All water mains shall be pressure tested in accordance with section 801.14 of the City of Columbus Construction and Material Specifications, with the following exception: 150 psi of pressure shall be maintained for at least two hours in any tested section. The City may not approve any less testing less than two hours regardless of the amount of leakage.

5.8 Where indicated on the plans, the existing water main shall be abandoned; and any existing water services off this main shall be transferred to the new water main. Prior to abandonment of the existing water main, the proposed water main shall be pigged (if required), tested, chlorinated and put in service and then the existing water services shall be transferred. The Contractor shall maintain water services to all properties during construction of the new water main and shall notify all customers affected by the transfer of services. To ensure that all existing services are transferred to the new main, no water main shall be abandoned until the new water main has been put in service; all affected water services have been transferred; and the existing water main to be abandoned has been shut down for 24 hours. All visible valve boxes, fire hydrants, and service boxes on the water main to be abandoned, which will no longer be in service, shall be removed. All water mains to be abandoned shall be made water tight. The required surface restoration shall be paid for under the appropriate bid item(s).

5.9 Water service boxes shall be placed 1' from the edge of the proposed or existing sidewalk between the sidewalk and the curb, or 2' feet inside the right-of-way or easement line when no sidewalk is present or proposed. Refer to Standard Drawing L-9901 for additional information.

5.9 Maintain eighteen (18) inches vertical and ten (10) feet horizontal separation between any sanitary or storm sewer piping and all proposed water mains.

5.10 When Controlled Density Fill (Item 613, Type 3 Only within Public R/W) is to be used as backfill, the Contractor shall provide Size No. 57 Crushed Carbonate Stone (CCS) 1 foot below to 1 foot above the existing water line.

5.11 All water lines installed within a 45 degree influence plain of pavement shall be backfilled with Item 912 (Type 1 Only) compacted granular backfill.

5.12 Survey Coordinates

5.12.1 Survey Coordinates shall include all material, equipment, and labor necessary to obtain horizontal and vertical (Northing, Easting, and Elevation) survey coordinates for the water main improvements. The survey coordinates shall be obtained for the completed water main construction and shall include all valves, tees, crosses, bends, deflections, plugs, reducers, tapping sleeves, blow offs, chlorination taps, fire hydrants, air releases, curb stops, casing pipe termini, and other fittings. Additional survey

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the pre-construction meeting by the Contractor and a copy of the tape is to be furnished to the City Engineer.

10 EXISTING UTILITIES

10.1 The identity and location of the existing underground utility facilities know to be located in the construction area have been shown on the plans as accurately as provided by the Owner of the underground utility. The City of New Albany and/or Engineer assumes no responsibility to the accuracy or the depths of the underground facilities shown on the plans.

10.2 Investigation, location, support, protection and restoration of all existing utilities and appurtenances shall be the responsibility of the Contractor. This work includes maintenance of adequate depth on all existing utility facilities. The Contractor is responsible to identify and coordinate field stakeout of all locations of possible grade conflicts with existing utilities prior to construction.

10.3 The Contractor is responsible for coordinating the relocation and/or protection of any utilities as required by the plan with the owner of the affected utility. Private utility manholes within the limits of the work shall be adjusted to grade by the respective utility. The cost of this work shall be included in the price bid for the project improvements.

10.4 Utility poles within the influence of the earthwork operations shall be reinforced by the utility company prior to these construction activities. Notification of the utility company prior to construction shall be the responsibility of the Contractor.

10.5 Abandonment (Capping, Etc.) of existing utility facilities (Amentech, Columbia Gas, American Electric Power) shall be performed by the respective utility company. Upon completion of same, the Contractor shall be responsible to remove any or all the necessary utility as required to complete the plan improvements. The cost of all removal along with the proper disposal thereof should be included in the price bid for the project improvement.

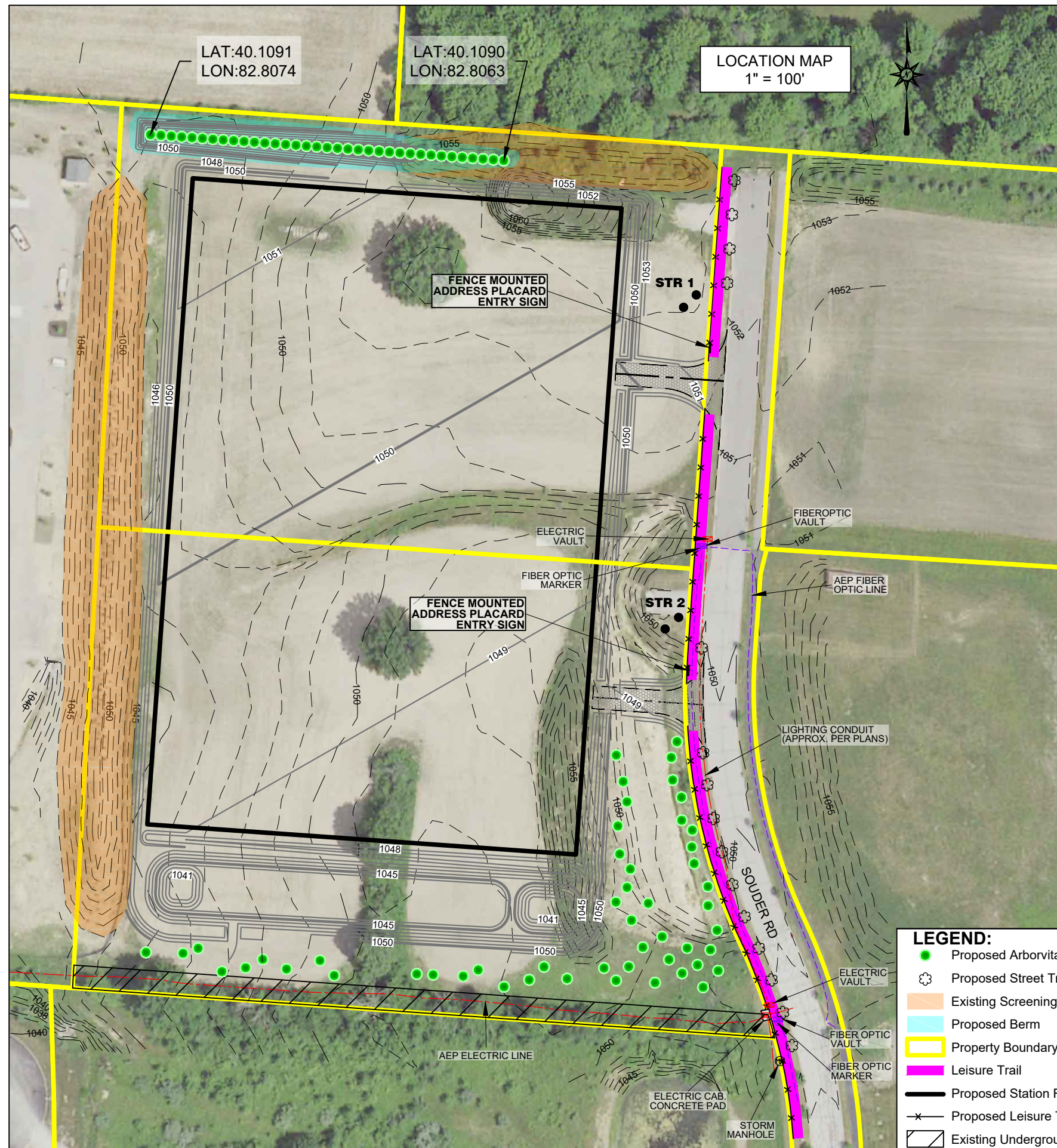
10.6 The Contractor shall cause notice to be given to the Ohio Utilities Protection Service (Telephone 800-382-2754, toll-free) and to the owners of the underground utilities who are not members of a registered underground protection service in accordance with Section 153.64 of the Revised Code. The above mentioned notice shall be given at least 48 hours prior to start of construction. The following utilities and Owners are located within the work limits of this project:

33



BAIR 6000IE		BAIR, GOODIE AND ASSOCIATES, INC. 153 NORTH BROADWAY STREET NEW PHILADELPHIA, OH 44663 TEL: 330-343-3499 FAX: 330-343-3505 WWW.BAIRGOODIE.COM	UNDERGROUND UTILITIES TWO WORKING DAYS CALL BEFORE YOU DIG Call 800-382-2754 (Toll Free) OHIO UTILITIES PROTECTION SERVICE
OLD DWG #:		STD DWG #:	
AEP OHIO TRANSMISSION COMPANY, INC.			
SOUDER STATION			
NEW ALBANY OHIO			
CITY OF NEW ALBANY STANDARD NOTES - SHEET 2			
SCALE AS NOTED	DR: DB/BGA	ENG: JP/BGA	CH: JP/BGA
WOF: T10593117002	APPD: JP/BGA	DATE: 09/11/2024	
1 RIVERSIDE PLAZA COLUMBUS, OH 43215		DWG. NO. CU-GN02-S02	R V 1

(E-1120)



Green Giant Western Arborvitae
Quantity: 86
Height at planting: 6'±

4' Diameter at time of planting
7' Diameter after 5 years
10' Diameter after 10 years

Notes:
1. Proposed North Berm: 6' high with trees planted such that the average spacing is 12 trees per 100 linear feet.
2. Plants expected to grow 1' per year.
3. Mature size: 25' height, 10' diameter
4. Trees to be planted 6' off of property line and easement.
5. Growth projections are approximate and require typical growing conditions.
6. Arborvitae or equivalent, depending on local nursery availability.



Quercus Rubra (Northern Red Oak)
Quantity: 15
Height at planting: 6'±

30' spacing center to center

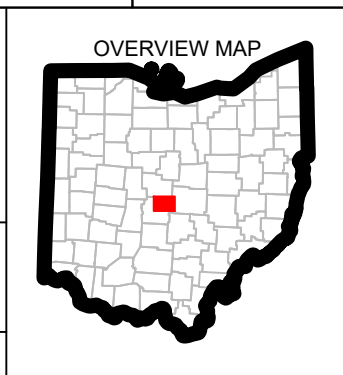
Notes:
1. Shall be planted no less than thirty (30) feet apart
2. Shall be planted in the tree lawn (between the leisure trail and the road pavement)
3. Shall be planted a minimum of three (3) inches caliper dbh (trunk diameter at breast height)
4. Tree species on the list of undesirable species in the New Albany Code of Ordinances shall not be planted
5. Shall be planted in locations to maintain a twenty-five (25) foot sight triangle at street intersections
6. Shall be maintained by developer for one (1) year after planting. Replacing any tree which dies during this period

- LEGEND:**
- Proposed Arborvitae Planting (86)
 - Proposed Street Tree Planting (15)
 - Existing Screening Mound
 - Proposed Berm
 - Property Boundary
 - Leisure Trail
 - Proposed Station Fence
 - Proposed Leisure Trail Fence
 - Existing Underground Electric Easement

BAIR GOODIE

NAD 83 Ohio State Plane South

March 26, 2025



CONCEPTUAL LANDSCAPE PLAN
SHEET 1 OF 3

AEP OHIO TRANSMISSION COMPANY
An AEP Company
BOUNDLESS ENERGY™

Souder Station

General Notes

1. Plant Materials. Plant materials include all trees, shrubs, perennial and vines, and plants required as part of the Work. Provide plant materials that are sourced from nurseries licensed by the Ohio Department of Agriculture or state equivalent, healthy specimens, typical of their species or variety, and that exhibit a normal habit of growth as set forth in the most current edition of the American Standard for Nursery Stock (ANSI Z60).

1.1. Location and Source of Supply. Supply the Engineer with complete and detailed information concerning the source of supply for each item of required plant material within 15 days after receiving the notice of award of the Contract. Ensure that all plant materials have been grown in the same hardiness zone or 1 zone colder than the project.

1.2. Transportation, Storage, and Handling. Transport all plants from nursery sources to the project site with the entire load completely covered for protection from drying winds. Thoroughly water all plants that cannot be immediately planted so as to keep the roots in a continually moist and protected condition. The TCR may reject plants that are not adequately protected during transportation and storage. Immediately remove all rejected plant materials from the project site. Handle all plant materials by the root ball or container.

1.3. Labeling. Attach legible labels to all specimens, or boxes, bundles, and other containers, indicating detailed information including, but not limited to, the botanical genus and the species name, the common name, the size or age of each species or variety and the quantity contained in the individual bundles or boxes. Remove all labels no more than two weeks prior to the completion of the establishment period. Notify the TCR prior to removing the plant material labels. Provide AEP copies.

1.4. Acceptance. Prior to acceptance by the Engineer, notify the Engineer when plant materials are delivered to the project site. Ensure that the plant species delivered are as described in the plans and are healthy, vigorous, and free from harmful plant diseases, and insect pests. Stockpiled materials can be inspected by ODA with advanced notice. Species substitutions must have written approval from the project engineer prior to delivery to the project. Do not install any plant materials until the TCR provides the Contractor with notification that the plant materials have been accepted.

1.5. Scheduling. Install all plant materials after March 15 and before June 1 or after September 15 and before November 30. Do not install plant materials in frozen or saturated soil conditions. Ensure a sufficient water supply is available to satisfy the requirements of 1. Plant Materials and 2. Watering.

1.6. Layout of Plant Materials. Before installation, use suitable staking to lay out the locations of all planting holes and beds. Provide the TCR with a scaled drawing that indicates the location, species, and size of plant materials required in the plan. Obtain the Engineer's approval of these locations before installation.

1.7. Backfill Mix. For all plantings, use backfill mix consisting of the following:

- A. One part soil.
- B. One part sphagnum peat moss, shredded pine bark, or EPA rated Class IV compost.
- C. One part sand.

Do not use backfill mix that is frozen or muddy.

1.8. Planting

A. Planting Holes: Dig planting holes that have sloping side walls and are 'bowl shaped'. Slope the side walls to approximately 45 degrees. Dig the planting hole so that the diameter at the top is at least two times the diameter of the root ball. Dig the planting hole to the same depth as the root ball structure. Dig planting holes for vines and perennials to a minimum depth and diameter of 6 inches (150 mm). Make planting holes for rooted cuttings and tree seedlings large enough to accommodate the root system.

B. Planting Trees & Shrubs: Set each plant in the center of the planting hole, plumb, and straight at a level such that the top of the root structure (i.e., trunk flare or root collar) is 1 inch (25 mm) above the surrounding soil. Set the root ball on compacted or unexcavated soil to prevent settlement. Prior to backfilling the hole, remove all twine, bags, and roping. For trees shipped with wire baskets supporting the root structure, remove the top two-third of the wire basket from root balls. Remove all rot-proof burlap. Remove or fold down the top one-third of standard (biodegradable) burlap. Take care not to separate the soil of the root ball from the plant's root system. Cut or remove circling roots before planting. Backfill the planting hole with the backfill mix. Fill the hole gradually and settle the backfill with water to the top of the root structure. Do not place backfill mix in direct contact with the trunks or stems. Add backfill mix around the root structure up to the plant's root collar is at the soil surface.

1.9. Landscape Mulch. Provide Landscape Mulch that consists of shredded bark and shredded wood. The length of any individual component cannot exceed 2 inches (50 mm). Ensure that at least 75 percent of the mulch can pass a 1 inch (25 mm) screen. Landscape Mulch may contain up to 50 percent shredded wood. Wood chips are not acceptable. Provide mulch that is free of soil, rocks, and weeds, and that has been aged at least one year before installation.

Smooth and shape the backfill mix to form a shallow basin slightly larger than the planting hole. Mulch these areas with a 4 inch (100 mm) layer of Landscape Mulch uniform in texture and size. Do not place mulch in direct contact with the trunks of any trees. Rake and smooth all planting beds upon completion of the work.

1.10. Bracing. Use only flexible, biodegradable ties when bracing trees. Use bracing only in areas where mower damage, vandalism, or windy conditions are a concern or as directed by the Engineer. Install loose fitting ties that will not girdle the trunk. Ensure that the tie will allow trunk movement and growth. Install all bracing as shown on the standard construction drawing SCD 1.10-1

1.11. Period of Establishment. Before final inspection, install all plants and care for them for a period of establishment. The period of establishment begins immediately upon completion of the planting operations and continues until October 1.

The minimum period of establishment is one complete growing season, beginning June 1 and ending the following October 1. During the period of establishment, follow standard horticultural practices to ensure the vigor and growth of the transplanted material including watering (according to 2. Watering), re-mulching, re-staking, and cultivating as necessary. Prune branches of deciduous plants to preserve the natural characteristics of the species according to ANSI Pruning Standards (ANSI A300). Remove broken, damaged, and dead branches. Do not trim the central leader of trees. Completely remove weeds and grasses from the planted and mulched areas by weeding and mowing (around trees, shrubs, and bed edges) at least two times during the growing season. Weed and mow the first time on or about June 15 and again approximately 8 weeks later.

1.12. Final Acceptance. On or about October 1 at the end of the establishment period, the TCR, in coordination with the AEP's Environmental Services, will inspect the plantings and supply the Contractor with a list of plant materials that do not comply with the contract requirements. Install all replacement plantings in accordance with the contract requirements. Replacement plants are subject to a new establishment period. Care for, water and maintain the replacement plant materials throughout the new establishment period at no additional cost to AEP.

1.13. Removal of Stakes and Wrapping. After receiving notification from the Department of the date of the final inspection, remove all stakes and wrapping material from all plants not more than 14 days before the final inspection, with the exception of the replacement plantings that have not been in place for a full growing season.

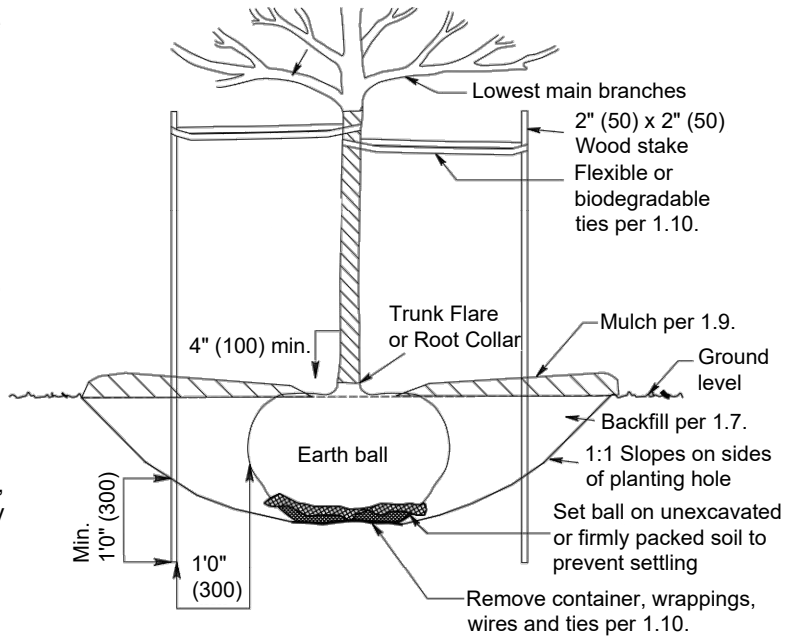
2. Watering. This work consists of furnishing, delivering, applying, measuring, and scheduling a sufficient amount of water necessary to keep each plant included in 1. Plant Materials in a healthy growing condition throughout the period of establishment and the Contract.

Furnish the water used in watering landscape plants. Thoroughly water all plant material at the time of planting regardless of soil moisture content. Continue to water throughout the period of establishment. Saturate the root zone and mulched area of each plant without causing run-off according to Table 2.1-1. During fall planting, continue to water until the ground is frozen and recommence watering after the spring thaw. Furnish a rain gauge approved by the Engineer.

2.1. Method of Measurement. The AEP TCR will measure Landscape Watering by the number of gallons (liters) delivered to plants from approved metered tanks or individually measured containers as follows:

Table 2.1-1	
Plant Description	Gallons (L)
Shrubs:	
12 to 36 inches (300 to 900 mm), height	4 (15)
36 inches to 5 feet (900 mm to 1.5 m), height	7 (25)
Trees:	
5 to 8 feet (1.5 to 2.5 m), height	15 (55)
2 to 3 inches (50 to 75 mm), caliper	25 (95)
3 to 4 inches (75 to 100 mm), caliper	30 (115)
Greater than 4 inches (100 mm), caliper	35 (130)

SCD 1.10-1



Top of hole shall be minimum of 2 x width of root ball

TREE PLANTING AND BRACING
PLANTING ON LEVEL GROUND

**BAIR
GOODIE**

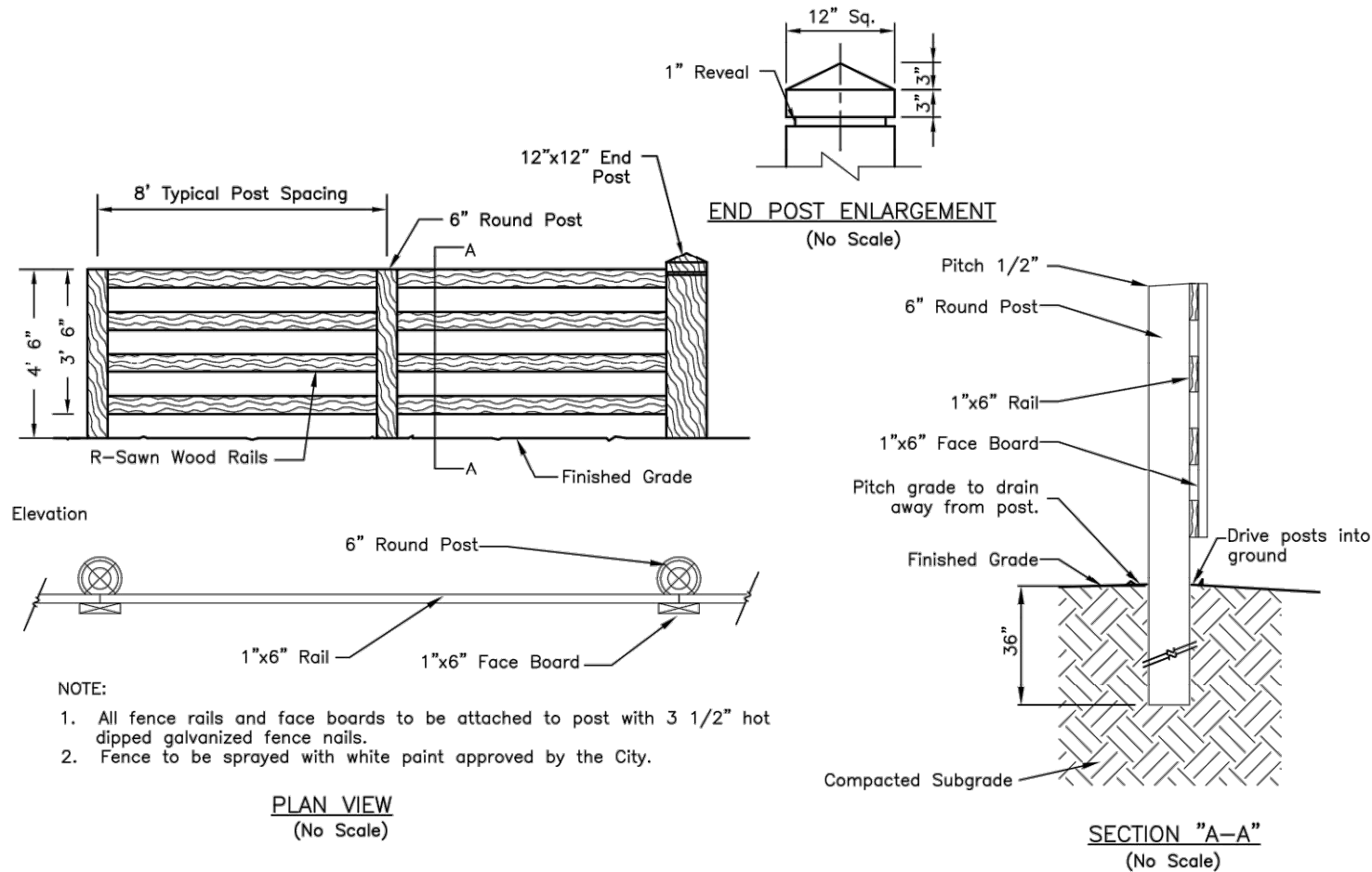
NAD 83 Ohio State
Plane South

March 26, 2025

OVERVIEW MAP

**CONCEPTUAL LANDSCAPE
PLAN
SHEET 2 OF 3**

Souder
Station



NOTES:

1. Signs shall be a **Standard Street Address Marker** in accordance to the Code of Ordinances City of New Albany, Ohio 1169.09 (c)
2. Signs shall be fence mounted
3. Site address to be displayed on signs: **7375 Souder Road**
4. Signs to be displayed at both entrances, attached to the four(4) rail wood fence



A 3

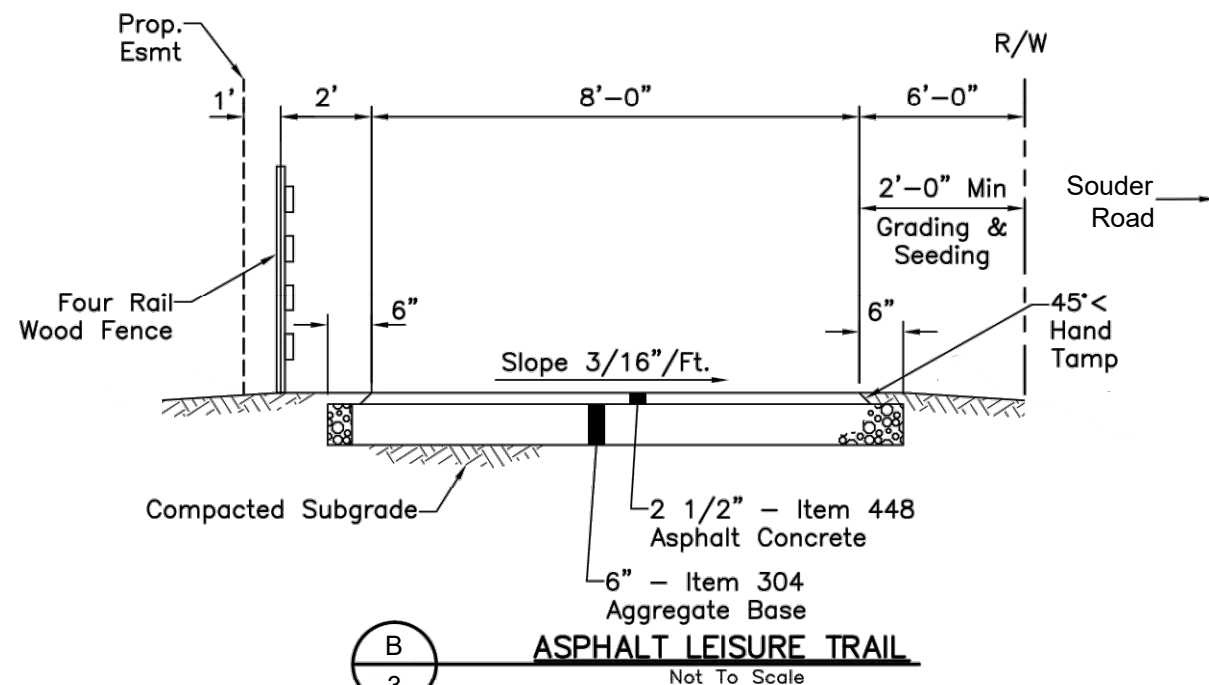
FOUR RAIL WOOD FENCE

(No Scale)

C 3

FENCE MOUNTED ADDRESS PLACARD

(No Scale)



BAIR GOODIE NAD 83 Ohio State Plane South March 26, 2025	OVERVIEW MAP 	CONCEPTUAL LANDSCAPE PLAN SHEET 3 OF 3
	 Souder Station	 Souder Station

ENGAGE NEW ALBANY

You're part of the plan

US-62 INTERCHANGE

Focus Area Plan

Spring 2025

DRAFT



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INTRODUCTION

INTRODUCTION

PLAN PURPOSE

The US-62 Interchange Focus Area Plan is a focused land use plan for a 573-acre study area north of OH-161. This plan is an addendum to the city strategic plan, Engage New Albany. The city strategic plan was adopted in 2021 and is updated when necessary to respond to changing conditions and priorities. The city has done addendums to the strategic plan every two years since its adoption in 2021. Each planning effort is built around community input and sound planning principles designed to improve the community's quality of life and protect long-term investments. Since its adoption, the city has taken proactive steps to pursue projects and additional planning initiatives that integrate recommendations set forth by Engage New Albany. This is one of several Focus Area plans stemming from the strategic plan that zoom into key areas in the city to set clear development goals and principles.

The US-62 Interchange Focus Area Plan is a key policy guide for city council, boards and commissions, and staff as they evaluate future land use and development along this important corridor in the city. Similar to the strategic plan, this plan will also provide focused recommendations for character, scale, location, and extent of public investments and private development throughout the study area.

The US-62 Interchange Focus Area Plan ultimately ensures that the city's community pillars – life-long learning, health and wellness, arts and culture, and sustainability – are prioritized in every corner of the city.

Why is this plan important?

Residents have expressed concern over the development trends in the city's transitional areas and along major roadway

corridors, stressing the importance of quality commercial offerings, safety improvements, and consistent development character that aligns with the rest of New Albany. US-62 has always been a major roadway that connects New Albany to other communities, and continues to be a corridor of focus to the City, ODOT, and neighboring jurisdictions. As regional economic development continues to unfold within and surrounding the community, the US-62 corridor and adjacent residential areas have seen significant growth and development pressure. The US-62 Interchange Focus Area Plan captures one of these dynamic areas and allows for the city to reassess development energy, consider new strategies since the adoption of the strategic plan, and prepare for future changes.

This Focus Area is confronted with several key challenges and opportunities due to its location and the greater context of the surrounding area:

- **Steady growth:** Primary highway access and the availability of previously underdeveloped land along US-62 led to the introduction of an auto-oriented commercial market in response to rising traffic volumes and employment uses in the surrounding area.
- **Pedestrian safety:** Multiple intersections in the Focus Area have experienced increases in daily traffic volumes over the last several years, leading to pedestrian safety concerns and strains on limited roadway infrastructure.
- **Impact on residential areas nearby:** Recent development interests have extended into transition areas, most notably along Bevelhymer Road, where commercial uses transition into

residential areas. As a result, residents and city officials have urged the re-evaluation of the Focus Area's long-term planning outcomes.

- **Development potential:** Several large-acre parcels remain undeveloped along Walton Parkway, while interest continues to rise for additional commercial and accommodation uses that complement the existing development pattern along US-62. These parcels were previously owned by corporate headquarters for potential expansion, but is now back on the market for development since the Covid-19 pandemic, which had significant impacts on the office market within and surrounding the Focus Area.
- **Community priorities:** The community expressed an overall dissatisfaction with the current mix of suburban, auto-oriented uses in the Focus Area. Engagement throughout the planning process identified that residents want to depart from the existing development pattern and create a more meaningful, intentional mix of retail and commercial offerings that better align with the character and charm found throughout New Albany. However, current market realities and demand pressures within and surrounding New Albany indicate that the current mix of auto-oriented commercial uses are well supported and in high-demand. Strategic placemaking and land use interventions are necessary to successfully generate a shift in the real estate market to a preferred retail and commercial environment.
- **Recreation opportunities:** The Kitzmiller Wetland Park is a planned, city-owned park within the Focus Area that presents opportunities to provide additional open space, recreation, and trail connectivity to residents. It can also serve as an anchor for future public and private investment into streets, leisure trails, and development along US-62 that aligns with the community's aspirations for this area.

PROCESS

Under the direction of the New Albany Community Development Department, an internal Working Group led by the planning, urban design, and landscape architecture firm, MKSK, oversaw the planning process with regular input and plan guidance from a Steering Committee composed of representatives of the city, neighborhoods, businesses, government agencies, community groups, and local institutions.

The planning process was broken into three phases, each of which included at least one meeting with the Steering Committee. The internal Working Group, in collaboration with the Steering Committee, hosted three Community Workshops at key points in the planning process. These engagement efforts were supported by online surveys and interactive tools to ensure a thorough and inclusive process.

Market Study Integration

A market study for the Focus Area and the city's Primary Market Area (PMA) guides this plan's future land use and development recommendations. The city's PMA consists of land areas generally within a 10-mile commute to the city. A market study is necessary because this Focus Area contains one of several commercial/retail areas within and surrounding the city. Recommendations concerning future commercial and retail land uses are guided by an analysis of the surrounding retail market landscape to ensure that decisions made within the Focus Area complement the existing commercial/retail offerings within the city and its PMA.

In addition to guiding recommendations for commercial and retail land uses, the study provides a comprehensive understanding of market realities, including demand and supply dynamics, emerging trends, and competitive pressures in the surrounding retail landscape. Furthermore, it incorporates insights gained through public engagement to identify community preferences and priorities. This dual focus ensures that





Study Area



strategies not only align with economic feasibility but also reflect the community's desires for a more curated mix of uses, such as boutique retail, sit-down dining, and entertainment.

The Market Study recommendations emphasize the need to shift the market toward achieving more community-driven aspirations while complementing and enhancing the existing commercial and retail offerings within the city and its PMA.

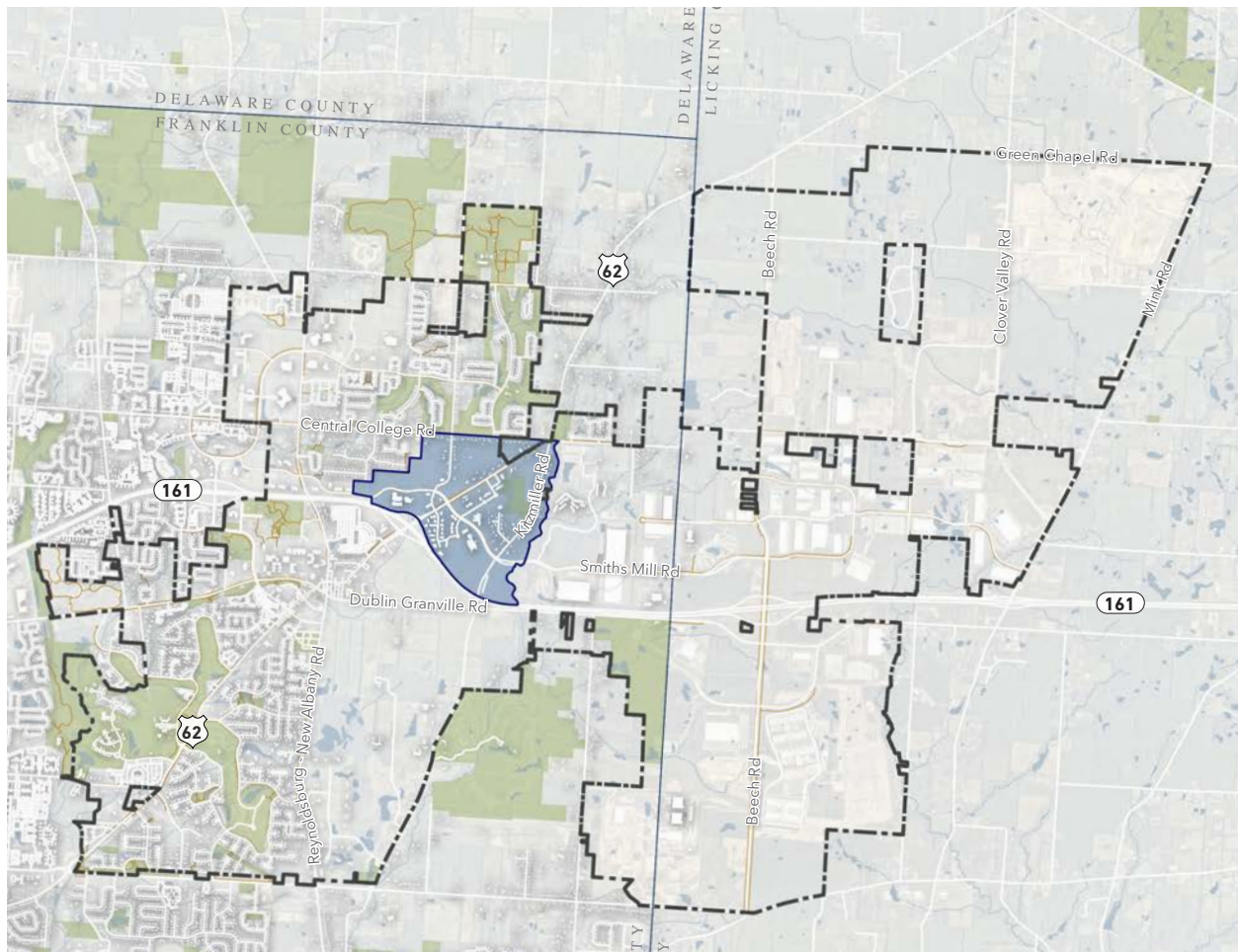
Legend

Plan Boundary	
Existing Leisure Trails	
Parks/Open Space	
Parcel Boundaries	

BACKGROUND

The Planning Team compiled existing conditions and research findings to help inform the planning process and guide conversations with residents. Many of the findings on the following pages were presented and discussed during the first Steering Committee meeting and the first Community Workshop. The general topics explored include the Focus Area's regional context, existing zoning and land use, local employment, mobility and transportation, parks, open space, and leisure trails.







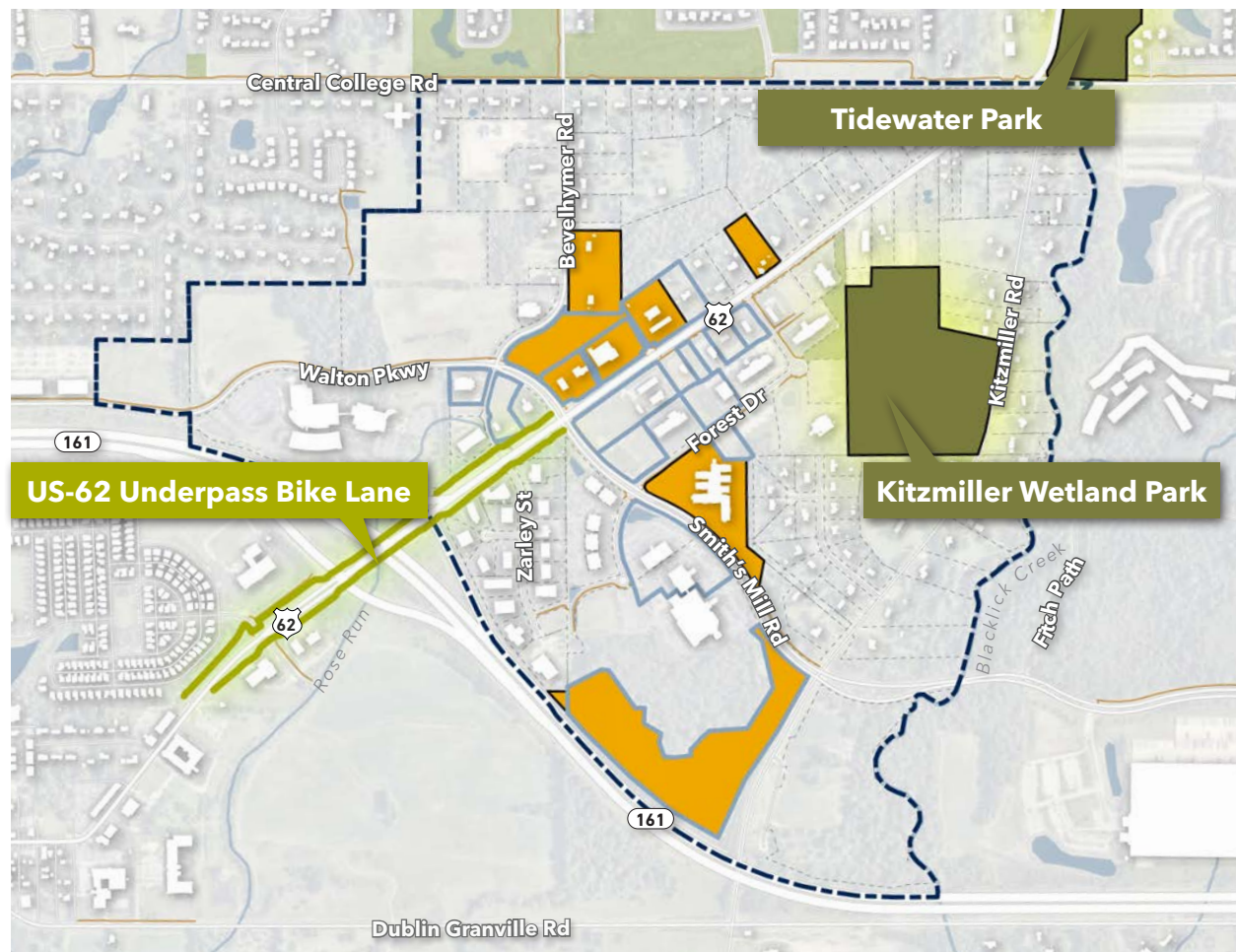


PLAN AREA CONTEXT

The Focus Area marks a significant transition into and out of the city's core, surrounding neighborhoods, and business districts. Several arterial and major collector roads converge onto the Focus Area, including Walton Parkway, Central College Road, Smith's Mill Road, Bevelhymer Road, and US-62. Residents frequently travel through the Focus Area to reach amenities like Bevelhymer Park, Rose Run Park, and the Village Center. Employees from the Walton Parkway and Smith's Mill Road Corridor commute through the Focus Area daily, in addition to regional traffic accessing OH-161 from the city of Johnstown heading southbound on US-62.

Legend

- Plan Boundary 
- Existing Leisure Trails 
- Parks/Open Space 
- City of New Albany Boundary 






RECENT & FUTURE PROJECTS

Given the level of daily traffic converging within the Focus Area, significant development activity has occurred over the last decade. The majority of approved developments and rezonings have occurred along US-62 and Forest Drive. Recently, development interest has shifted to Bevelhymer Road, marked by two non-residential rezoning applications.

The city recently completed construction of the US-62 Underpass bike path to provide a safer bike connection to and from the Village Center.

In addition to recent development activity, the 2022 Parks Framework Plan identified Kitzmiller Wetland Park and Tidewater Park as critical future investments within and adjacent to the Focus Area. These park investments would help fill citywide gaps in park access by foot or bike.

Legend

- Plan Boundary 
- Approved Developments 
- Rezonings 



US-62 Underpass Bike Lane



Sheetz, US-62



Planned Site for Tidewater Park



Aldi, US-62



Planned Site for Kitzmiller Wetland Park



Dunkin Donuts, US-62

EXISTING LAND USE

Land use describes the existing activities occurring on a parcel of land. Existing land uses do not reflect how the land is zoned, nor do they represent future land use recommendations or any preferred development patterns identified in this plan or related planning documents. Existing land uses are generally grouped into four categories displayed on the map on the following page: Residential/ Agriculture, Commercial /Industrial, Parkland, and Undeveloped/ Open Space. For simplification, medical and office land uses are categorized under Commercial/ Industrial. Undeveloped/Open Space indicate parcels where no buildings currently exist.

The Focus Area marks a significant transition in land use character and scale compared to nearby areas. Given its primary road connections, commercial land uses along US-62 are focused on automobile-oriented, quick services, and fast food-related businesses, serving not only local residents but also a regional commuter base traveling along OH-161. The Focus Area also marks a significant commercial/residential transition, where Bevelhymer and Kitzmiller Roads serve as a residential buffer between the non-residential uses along US-62, Walton Parkway, and Smith's Mill Road, and the surrounding residential neighborhoods north and west of the Focus Area.

Commercial/Industrial is the most common land use by land area in the Focus Area. This land use category includes medical uses like Mt. Carmel Health Campus, commercial and retail establishments along US-62, Zarley Industrial Park, and office uses along Walton Parkway.

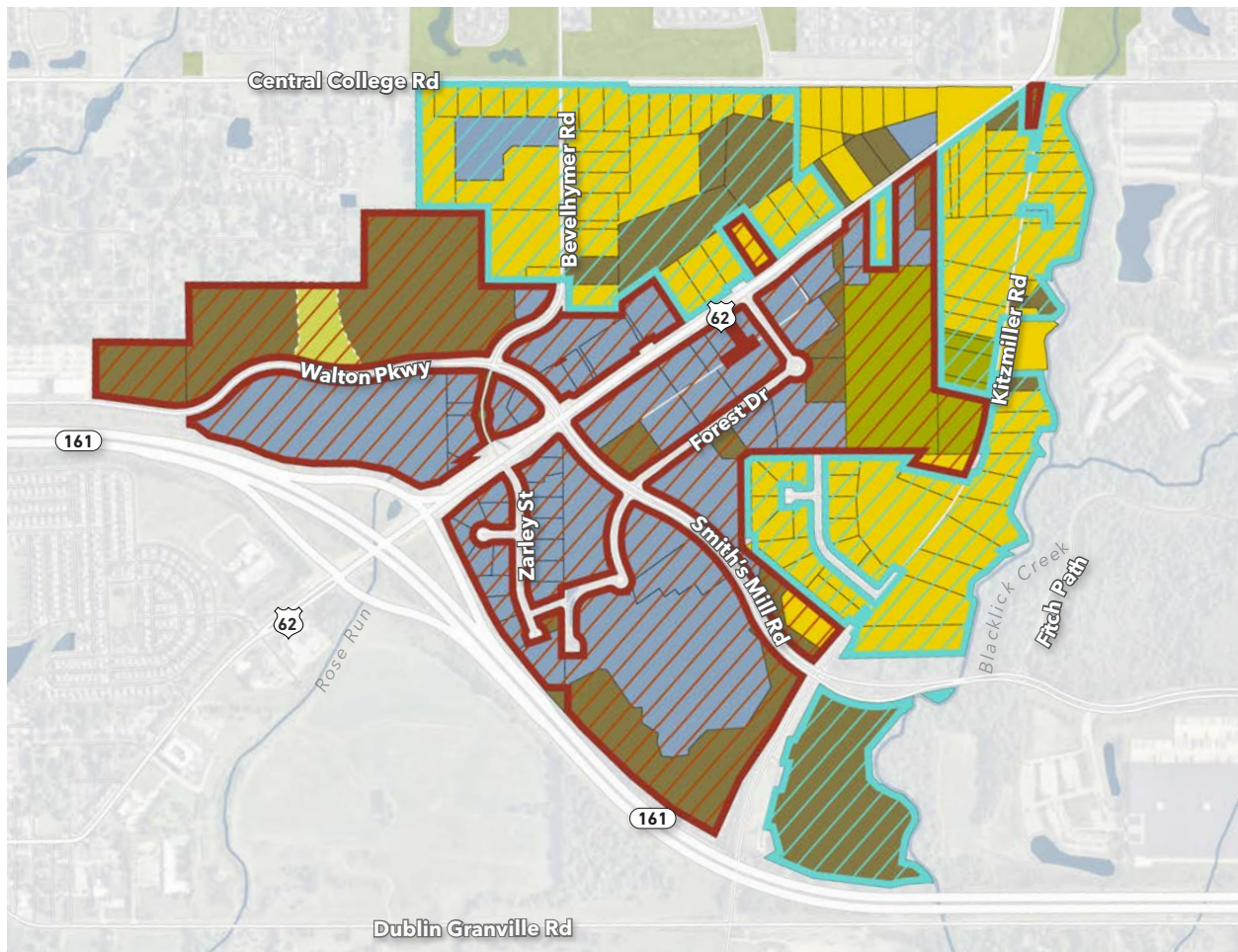
There are 208 acres of Residential/ Agriculture land uses in the Focus Area, situated along Kitzmiller Road, Bevelhymer Road, and Central College Road. Residential parcels along these roads are typically over 1 acre, un-subdivided, and were originally part of Plain Township. Plainview Country Estates is the only subdivision within the Focus Area.

Undeveloped/Open Space parcels make up 137 acres in the Focus Area. A significant portion of this acreage is made up of woodlots, naturally occurring open space, and wetlands. Parkland includes 28 acres of planned Kitzmiller Wetland Park.

Several major employment uses are directly east of the Focus Area along Walton Parkway, including the headquarters of Abercrombie & Fitch, the Personal Care & Beauty Campus, and AEP Ohio. The US-62 interchange is also a primary interchange for accessing the Village Center, the city's primary community and retail destination. West and north of the Focus Area are primarily made up of residential land uses, including the largest subdivision north of 161, the New Albany Links. Other notable subdivisions near the Focus Area include Tidewater, Saunton, Wentworth Crossing, and Upper Clarenton.


General zoning districts are included on the following page to illustrate how existing land uses are currently being regulated. Within these broad categories, specific zoning districts exist, such as Limited Industrial or General Employment. It is also important to note that existing parkland and open space are still permitted even if they are within a commercial or industrial zoning district.


Existing Land Use




Legend

Existing Land Uses:

Residential/Agriculture (207.8 acres) 

Commercial/Industrial (295.9 acres) 


Undeveloped / Open Space (164.9 acres) 

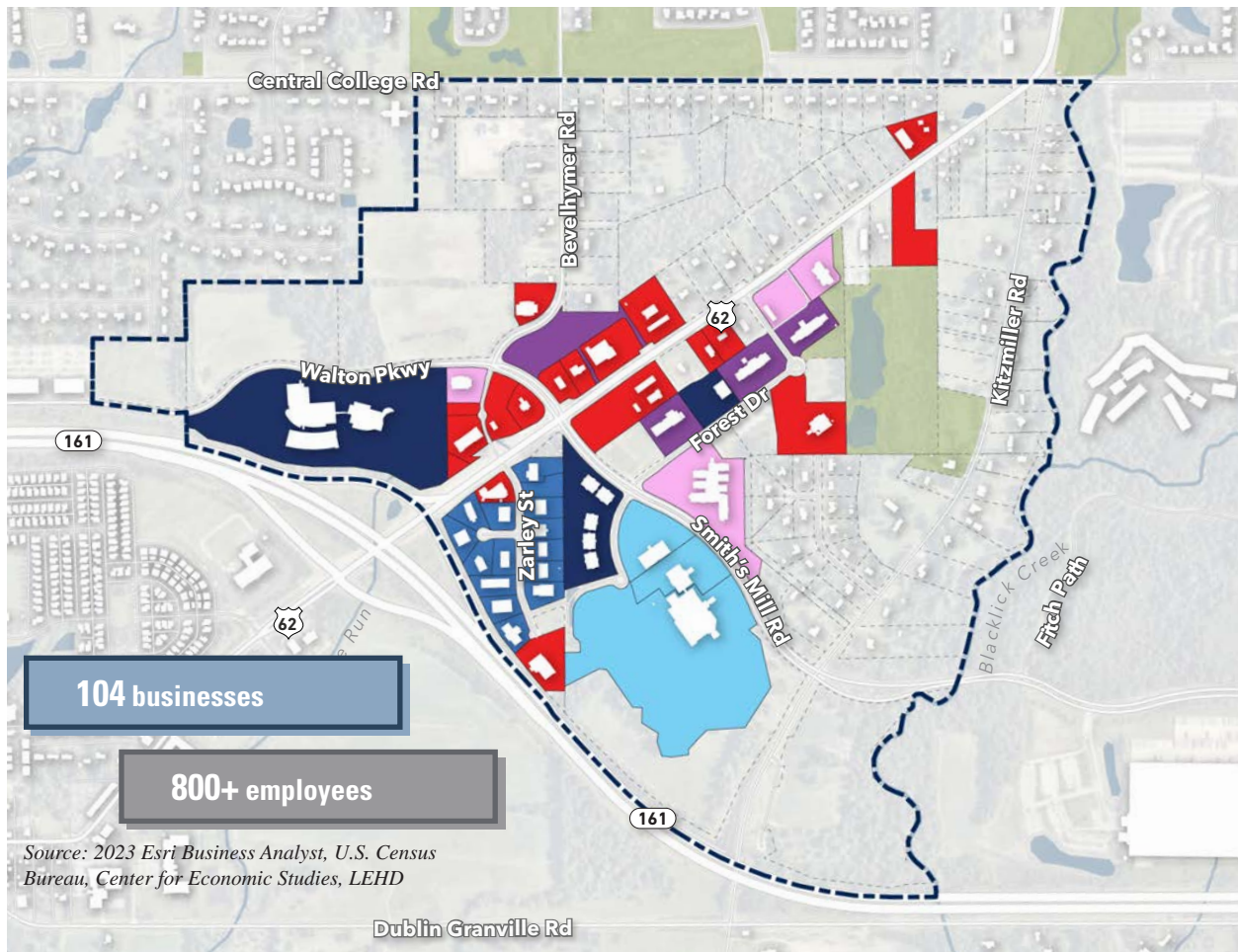
Parkland 

City Easement 

General Zoning Districts:

Residential or Agricultural Districts 

Commercial or Industrial Districts 



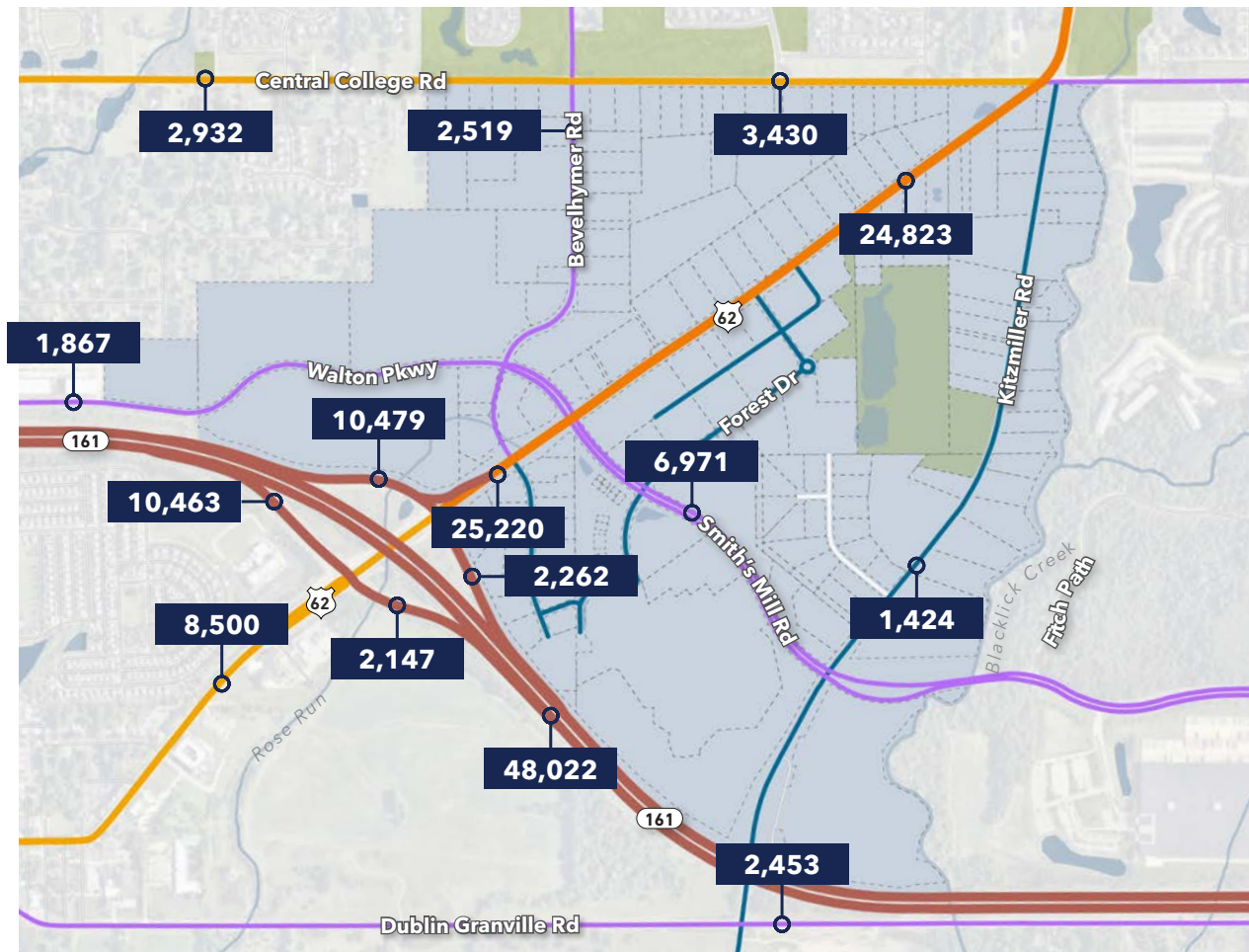
LOCAL ECONOMY & BUSINESSES

With a total of 104 businesses and over 800 employees, the Focus Area hosts a wide range of commercial, medical, office, retail, and industrial land uses. Commercial and retail uses are primarily fronting US-62, while hotel and accommodation uses have developed along Forest Drive and at the corner of Bevelhymmer Road and Walton Parkway. Several office headquarters are located along Walton Parkway, while Smith's Mill hosts a variety of medical offices, surgical and imaging centers, and other care-related uses. The Zarley Industrial Park is a unique business district in New Albany that accommodates small to medium-sized, light industrial businesses that require flex office, storage, and workshop facilities.

Legend

Plan Boundary	
Office (29.4 acres)	
Commercial/Retail (36.4 acres)	
Flex Office/Industrial (15.2 acres)	
Medical (36.8 acres)	
Hotel (12.8 acres)	
Care-Related (17.0 acres)	





EXISTING VEHICULAR NETWORK & AADT

The Focus Area's vehicular network is well connected to the rest of the city and regional destinations. However, average annual daily traffic numbers indicate that the segment of US-62 within the Focus Area experiences significantly higher daily traffic than the segment south of OH-161. This presents barriers for providing pedestrian oriented land uses and amenity offerings. In addition, Walton Parkway and Smith's Mill Road is a major east-west roadway in New Albany, providing commuter access to most of the business park. Traffic at the "five-points" intersection at US-62/Central College Road is likely to increase in the coming years as growth occurs within the business park. In 2024, ODOT began a separate US-62 planning process that studies the entire roadway segment from the Focus Area to Johnstown. The recommendations in this plan will be utilized by city officials to help inform ODOT's planning study and proposals for infrastructure improvements.

Legend

Plan Boundary	
Interstate	
Principal Arterial	
Minor Arterial	
Major Collector	
Minor Collector	
Local Road	

Source: ODOT Transportation Data Management System, Esri Business Analyst, 2023



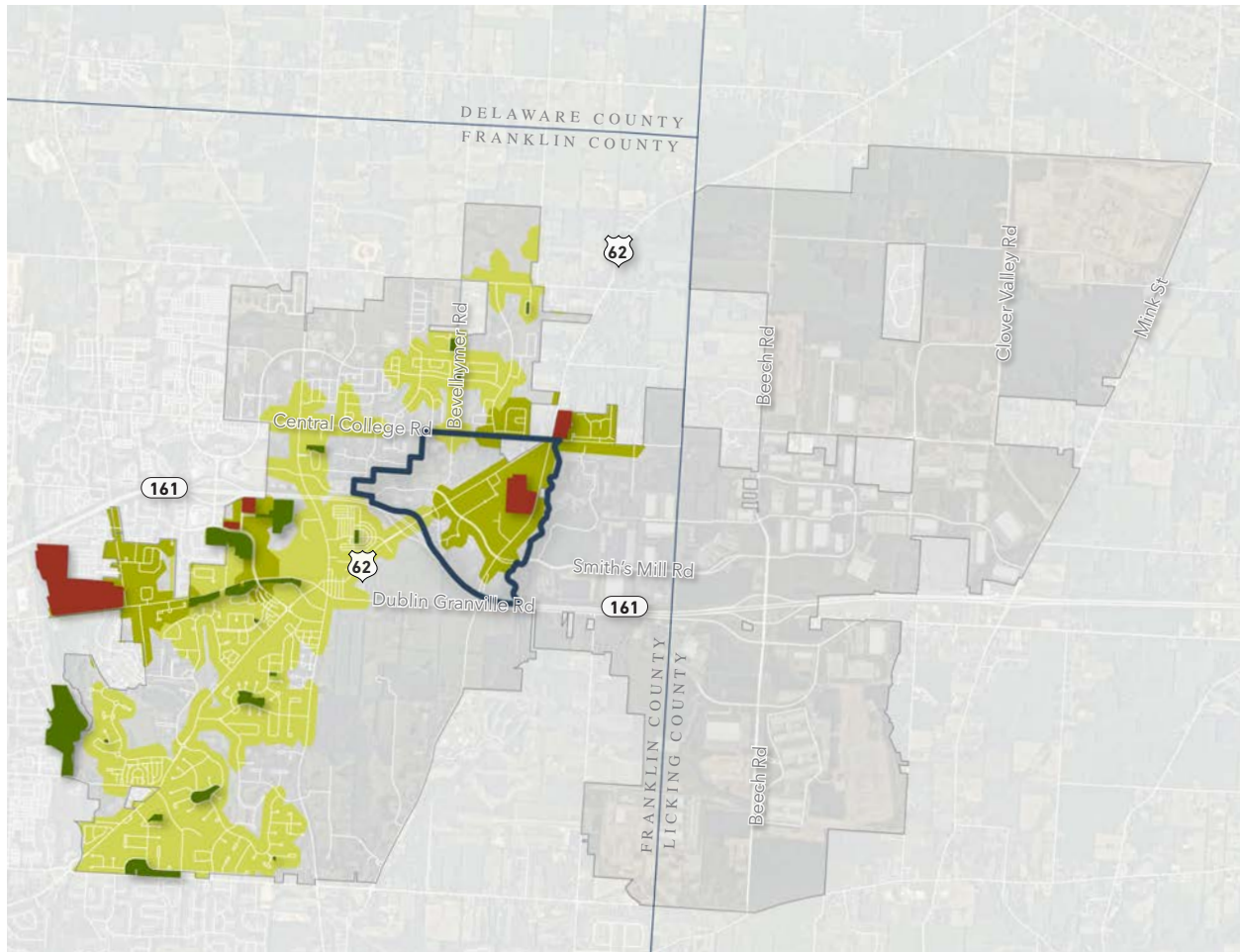
TRANSPORTATION

Through SmartRide New Albany, a partnership with Central Ohio Transit Authority (COTA) and the city, the New Albany Park and Ride is served by COTA Lines 35 and 45, which connect north Columbus, Easton Transit Center, and Downtown Columbus to New Albany. From there, the SmartRide Red and Blue Routes run seven days a week with stops that shuttle arrivals from Lines 35 and 45 to many corporate offices and other employment locations throughout New Albany. Previous planning initiatives have explored opportunities for transforming the New Albany Park and Ride into a mobility hub, providing residents with a more comprehensive range of mobility options. These enhancements typically include bike-share, EV charging stations, bike repair stations, and e-scooters to provide “first-mile, last-mile” mobility options.

Legend

Plan Boundary 

COTA Bus Lines 35 and 45 



EXISTING PARKS, OPEN SPACE, & LEISURE TRAILS

New Albany has invested over \$13 million in park improvements over the past five years, covering seven parks: Windsor, Sumption, Byington, James River, Lambton, Planters Grove, and Taylor Farm. However, there are currently no parks within the Focus Area, creating a significant gap in park access compared to other areas in the city. A walkshed analysis conducted in the 2022 Parks Framework Plan identified several gaps throughout the city. Taylor Farm Park has been the most significant addition to the city's parks and recreation offerings. Additionally, the study identified that the addition of Kitzmiller Wetland Park and Tidewater Park would fill existing gaps in park accessibility within the Focus Area.

Legend

Focus Area Boundary	
City Boundary	
Existing Parks	
Future Parks	
Existing Parks 10-min Walkshed	
Future Parks 10-min Walkshed	



PLANNED PARKS & LEISURE TRAILS

Multiple improvements have been proposed to enhance leisure trails and park access in the Focus Area. These improvements intend to leverage the recent bike path enhancements along US-62 that extend under the OH-161 underpass to link riders to the Village Center. The 2022 Parks Framework Plan proposed adding a leisure trail connection along Bevelhymer Road and Smith's Mill Road to bridge gaps in existing trail connections. Additionally, previous planning documents identified improvements to the Velo Loop along Walton Parkway, Smith's Mill Road, Kitzmilller Road, and Dublin Granville Road. The Velo Loop is designed as a dedicated bike path for intermediate bike riders.

Legend

Plan Boundary	
Parcel Boundaries	
Existing Parks/Open Space	
Planned Park	
City Easement	
Existing Leisure Trails	
Proposed Leisure Trails	
Proposed Velo Loop Cycle Track	
Proposed Sharrow	
Proposed Velo Loop Alternatives	

COMMUNITY ENGAGEMENT

A critical component of the US-62 Interchange Focus Area Plan and its long-term success is public engagement of the greater New Albany community. This process included the following groups and activities:

ENGAGEMENT APPROACH



Working Group

A team consisting of key city staff/administration that served as a direct day-to-day contact point for the Planning Team. The Working Group provided the first level of review for all work products.



Steering Committee

A group formed to guide and advise the Planning Team throughout the process. The Committee provided feedback and ideas to ensure that the plan's content reflected the values of stakeholders and the New Albany community.



Community Workshops

The Planning Team conducted three public workshops across the arc of the process to share the work process to-date, have discussions with attendees, and gather community feedback.



Project Webpage & Social Media

The Planning Team created a US-62 Interchange Focus Area Plan project webpage and facilitated a social media strategy to maximize the project's community outreach.



Online Surveys & Social Pinpoint

Steering Committee members and the community had opportunities to engage with interactive online surveys and mapping exhibits to provide detailed, place-oriented feedback on the planning process.

STEERING COMMITTEE

A project Steering Committee of 22 informed stakeholders was formed to help guide this planning process. This Steering Committee included representatives from city council, Planning Commission, neighborhoods, businesses, community groups, institutions, and government agencies. The planning team met four times throughout the project.

Steering Committee Meetings:

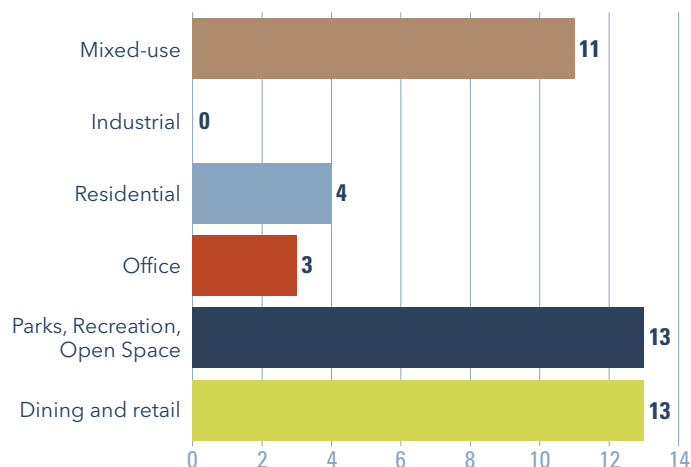
- April 23, 2024: Steering Committee #1
- July 23, 2024: Steering Committee #2
- September 9, 2024: Steering Committee #3
- November 19, 2024: Steering Committee #4
- January 28, 2025: Steering Committee #5



Steering Committee Meeting #1

Steering Committee Meeting #1: Initial Land Use Preferences

What type of land uses would you like to see more of in this Study Area? (n=16)



"Better access to parks with walking or bike trails."

"More locally owned retail and food, more unique green spaces."

"More community feel, less fast food."

Steering Committee Meeting #2

Steering Committee members reviewed the results of the first public survey, discussed initial findings from the market analysis, and shared their takeaways from their visits to several mixed-use destinations throughout Central Ohio.

In coordination with the Planning Team, Steering Committee members refined key themes from the first round of public engagement results. These themes served as a valuable tool in synthesizing and concisely communicating over 500 survey responses and 1,400 unique comments received.

Initial market findings were shared with the Steering Committee to better understand the broader market forces that affect the Focus Area's development pattern. These initial findings provided important metrics that informed the necessary preparation and facilitation of the third Steering Committee meeting.

Steering Committee members each visited several mixed-use destinations across the region before convening for the second Steering Committee meeting. Members were asked to document their observations, answering questions about what they liked and disliked, what surprised them, and to list design or land use elements that could potentially work well in the Focus Area. Steering Committee members visited the following locations:

- Bridge Park | Dublin
- Old Upper Arlington | Upper Arlington
- Tremont Road Corridor | Upper Arlington
- Grandview Yard | Granview Heights
- Evans Farm | Lewis Center

General Takeaways

Steering Committee members shared several takeaways after visiting these destinations:

- The Focus Area could benefit from a distinctive "brand" or name to attract visitors from other regions.
- The Focus Area should be planned holistically, with an emphasis on consistent design elements.
- Committee members liked walkable destinations for surrounding residents, connected by safe paths and leisure trails.
- Preferable types of retail and commercial uses require higher residential density to support them.



Bridge Park / Dublin

- ⊕ Healthy mix of housing, dining, shopping
- ⊕ Green space, pedestrian connections
- ⊖ Building scale/height



Old Upper Arlington / Upper Arlington

- ⊕ Building scale and aesthetic
- ⊕ Blends well with surroundings
- ⊖ Nothing else within walking distance



Tremont Road Corridor / Upper Arlington

- ⊕ Housing surrounding shopping areas
- ⊖ Congestion/traffic
- ⊖ Not pedestrian-friendly



Grandview Yard / Grandview Heights

- ⊕ Walkable
- ⊕ Building scale and aesthetic
- ⊖ Limited green space



Evans Farm / Lewis Center

- ⊕ Variety of housing options
- ⊕ Integrated commercial/retail area
- ⊖ Lack of amenitized green space

Steering Committee Meeting #3

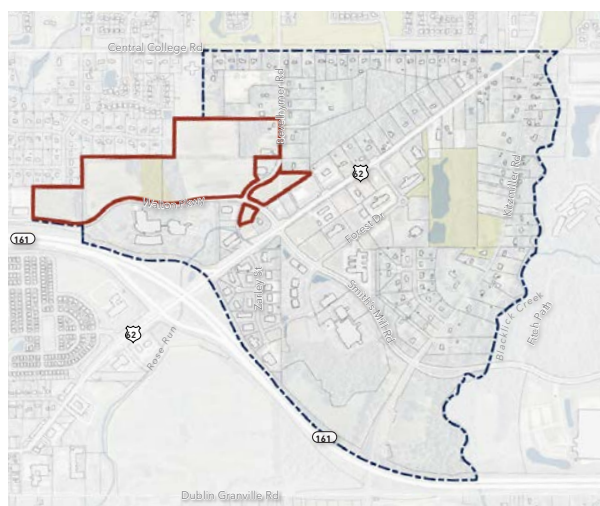
By the third meeting, Steering Committee members were well-informed on previous planning efforts, the community's overall goals and aspirations, the market findings, and the land areas most susceptible to future change in the Focus Area. Members were equipped to begin discussions around potential land use alternatives for areas in the Focus Area that remain undeveloped or have recently been acquired by landowners interested in future development.

To guide this discussion, Steering Committee members engaged in a development activity that involved placing land use “game pieces” on top of parcels susceptible to change in the Focus Area. These game pieces resembled to-scale building footprints, allowing members to visualize what future development could potentially look like and how it might fit into the surrounding context. The market findings informed a high-level understanding of how much retail, office, and residential uses the Focus Area can accommodate based on existing supply and demand. The market study also informed how each game piece affects the market potential for other pieces.

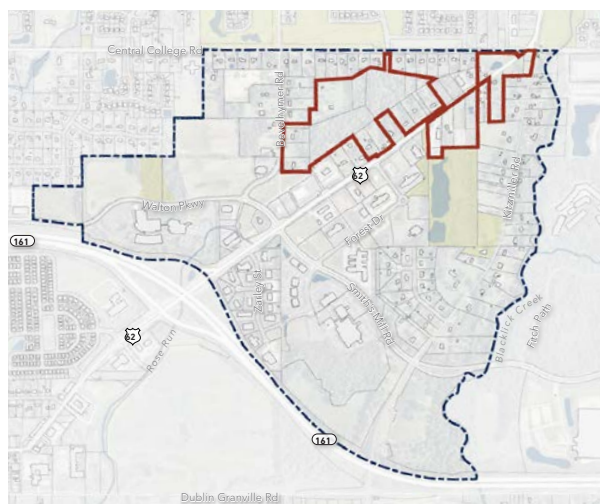
The goals of the development activity were to build consensus around the overall land use

and development approach for undeveloped parcels in the Focus Area, highlight the complexity of planning for private property, and align specific site programming with community engagement themes and analysis findings. The results of the development activity scenarios helped the Planning Team understand the Steering Committee's preferred land use patterns for land areas susceptible to change. This activity, in conjunction with input gathered through the plan's Social Pinpoint online engagement platform, informed the plan's future land use, thoroughfare plan, and development standards. The following sites were explored:

Site A: Walton Parkway Corridor



Site B: Village Transition Area

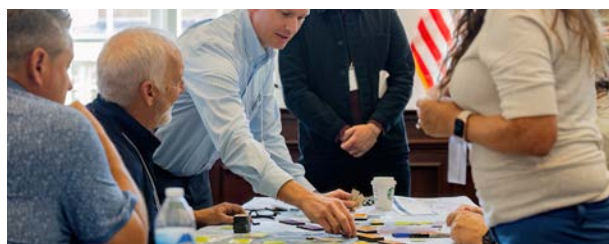


Development Scenario Activity / Site A

Site A features seven parcels primarily fronting Walton Parkway and Bevelhymer Road. The site is situated between office uses to the west and south, with residential neighborhoods directly north of the site. Existing leisure trails run along Walton Parkway, north of the site connecting into Cedar Brook, and fronting Broadway Bound. The city also has a conservation easement in the middle of the site that protects a substantial amount of tree canopy.

Through this activity, the Steering Committee built consensus around the following considerations for future development on the site:

- Land uses should gradually decrease in intensity as they get closer to adjacent neighborhoods to the north.
- The existing trail network should be expanded and connected throughout the site.
- Locations for mixed-use residential, retail, and hotel uses are most appropriate near the intersection of Walton Parkway and Bevelhymer Road.
- An internal roadway network should facilitate site access.
- Opportunities for civic/recreational uses should be explored.



Development Scenario Activity / Site B

Site B contains parcels primarily fronting US-62 and Bevelhymer Road toward the northeast of the Focus Area. Many of the larger parcels west of US-62 are heavily wooded and surrounded by residential lots along Central College Road and Bevelhymer Road. Several properties east of US-62 are situated adjacent to the planned Kitzmiller Park to the south. The northern boundary of the site is marked by the 5-points intersection of Central College Road, US-62, and Kitzmiller Road.

Through this activity, the Steering Committee built consensus around the following considerations for future development on the site:

- A roundabout constructed at the 5-points intersection should be accompanied by streetscape and public realm improvements to signify this node as a gateway into New Albany.
- Parcels fronting US-62 are suitable for capturing future retail and mixed-use residential demand in the Focus Area.
- A road connecting Central College Road to US-62 and Bevelhymer Road would provide a safer alternative for pedestrians walking or biking to commercial destinations.
- Single-family residential can be accommodated west of US-62 and adjacent to existing residential lots along Bevelhymer and Central College Road.
- Additional civic and mixed-use destinations should be explored north of Kitzmiller Park.



COMMUNITY WORKSHOP #1

To ensure that the broader community had an opportunity to share their feedback and preferences for the Focus Area, the Planning Team hosted three Community Workshops. Invitations to these workshops were posted on the city's website and social media pages.

The first Community Workshop was held on May 14, 2024, and focused on providing an overview of the Focus Area and the planning process, while also gathering general feedback from community members regarding the Focus Area. Residents provided their preferred priorities for mobility improvements, residential, employment, and retail land uses, in addition to filling out the paper version of Community Survey #1. Over 180 residents attended the first Community Workshop, with 43 paper surveys completed.



Community Workshop #1



Community Workshop #1

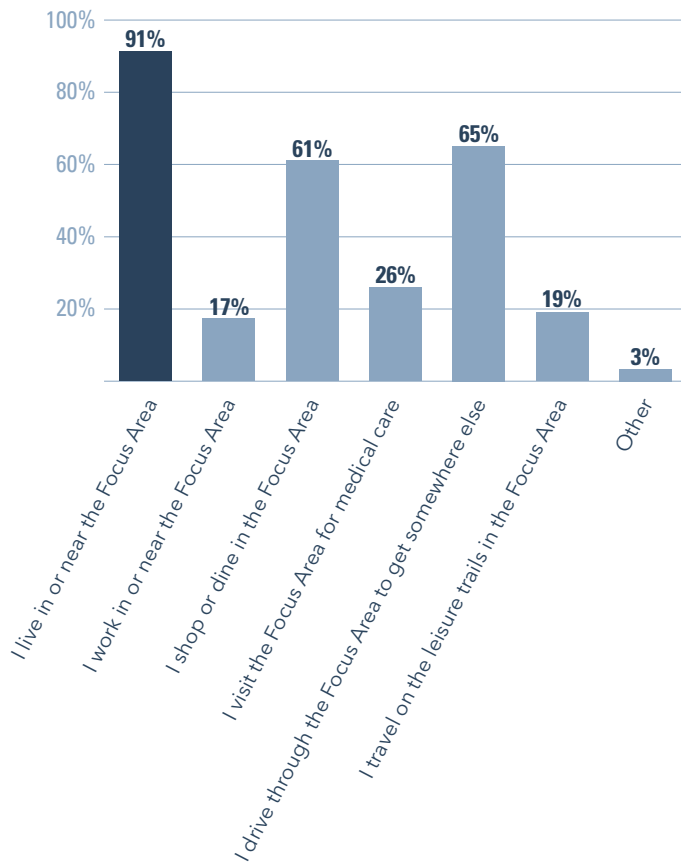


Community Workshop #1

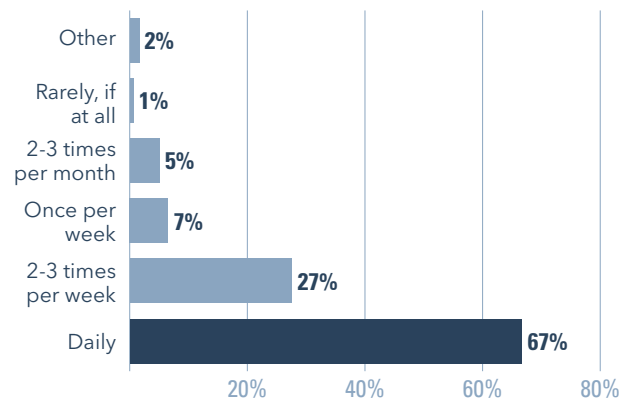
COMMUNITY SURVEY #1 SUMMARY

The first community survey was launched on May 14, 2024. The survey was available for residents to complete at Community Workshop #1 and online for those who were unable to attend in-person. A combined total of 518 paper and online surveys were completed by June 7, 2024.

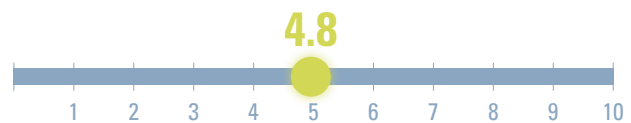
1. Please select all that describe you: (n=473)



2. How often do you visit the US-62 Interchange Focus Area? (n=473)



3. On a scale of 1-10, with 1 being the lowest and 10 being the highest, how satisfied are you with the Focus Area today? (n=488)

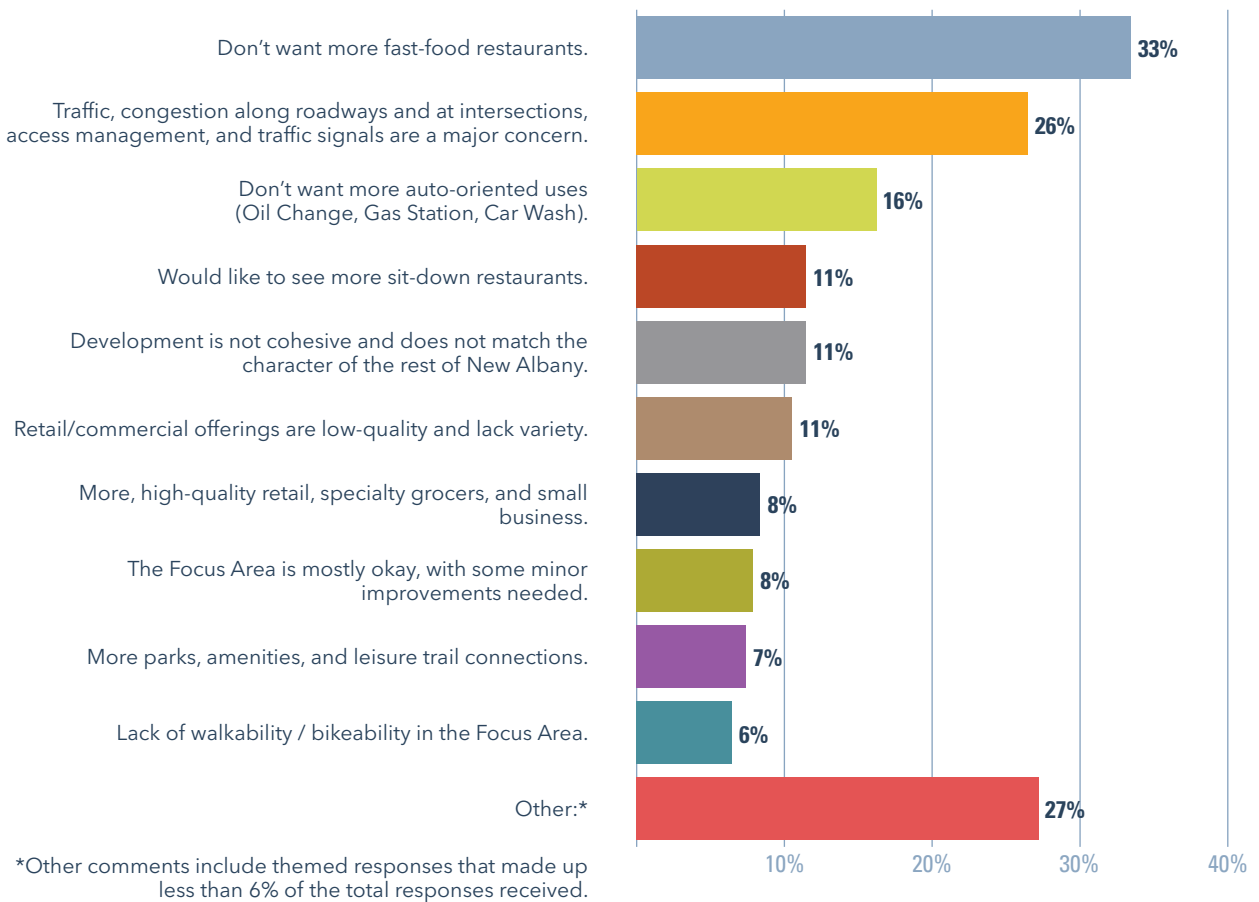


"My child's **daycare** is in the Focus Area."

"I visit to catch **transit** to Columbus"

"My daughter attends the **New Albany Ballet Company**."

4. Tell us more about your answer to question 3: (n=419)



Notable comments:

"I like the residential areas on Central College and Kitzmiller, would like to see **better planning along 62.**"

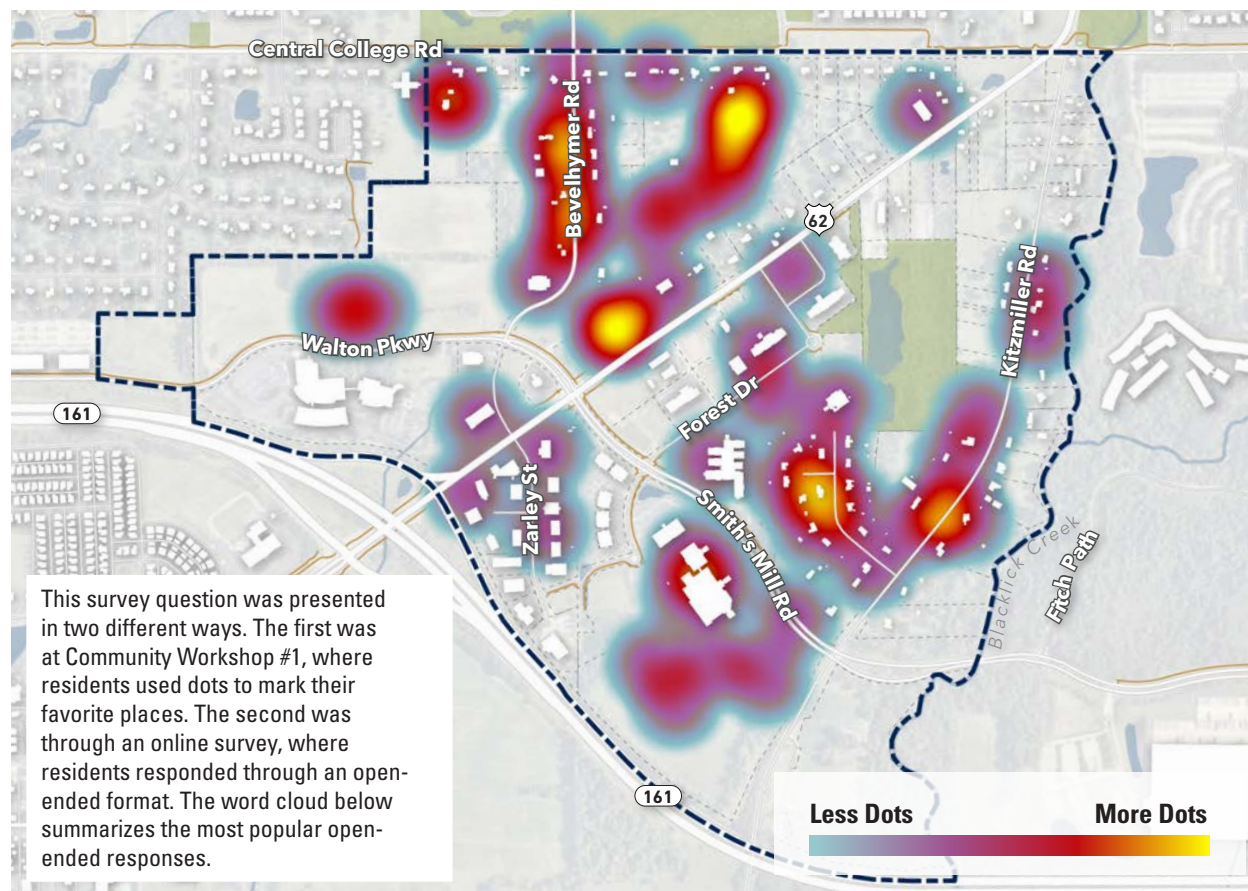
"It is an area that needs attention, especially the **industrial complex just north of 161.**"

"...there are **not many reasons to visit** the area other than just passing through."

"There are too many auto-oriented uses popping up. This area **does not match the character of New Albany.**"

5. Tell us about some of your favorite places in the Focus Area. This can be an institution, a business, a place, or a destination. (n=346)

Community Workshop #1: What are some of your favorite places in the Focus Area?



Notable comments:

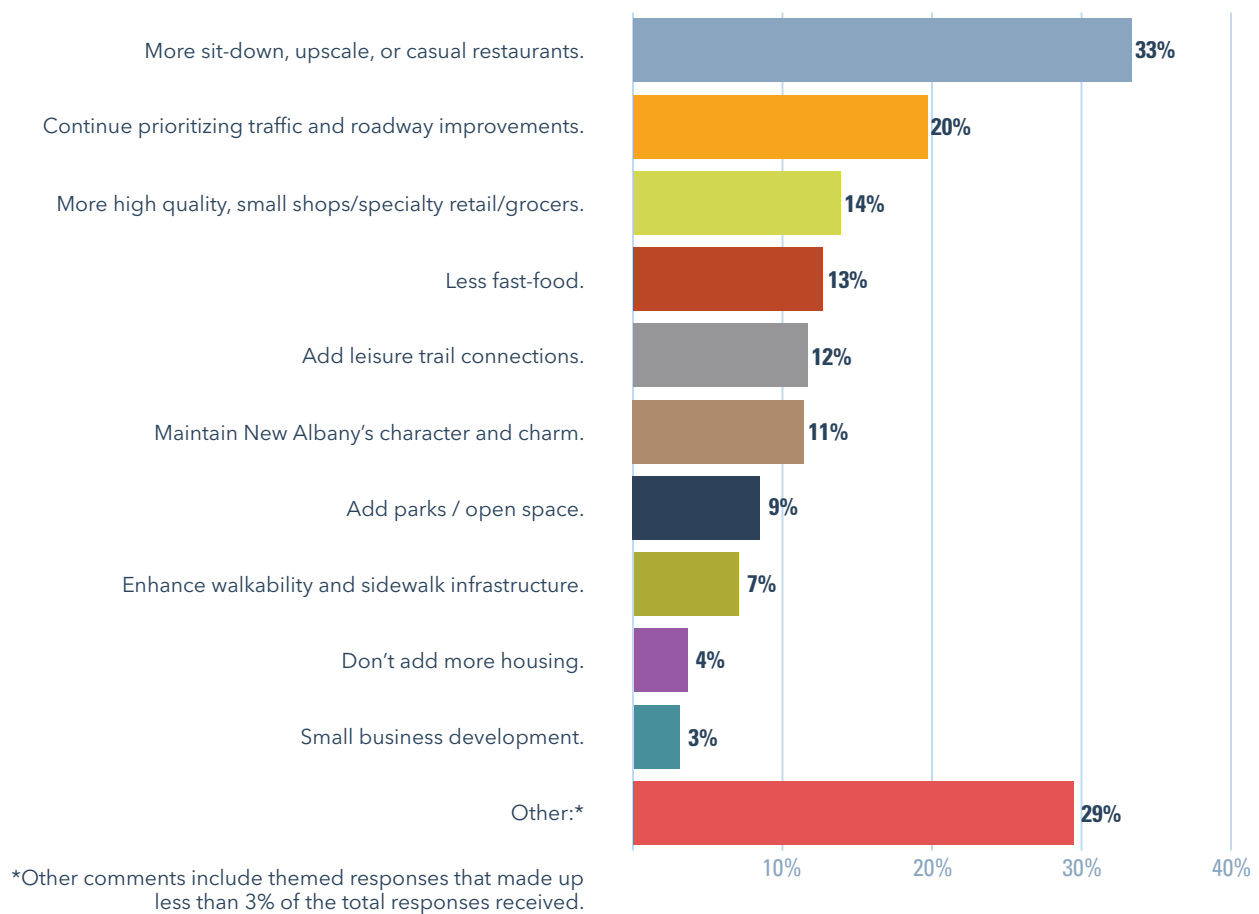
Online Survey #1 Results

"I walk a lot in this area since I live in it and walk everyday. The Smith's Mill **walking path is wonderful**, I just wish we had one along Kitzmiller."

"I appreciate the medical facilities, car wash, and local restaurants, but all the **chain food, car services, and gas stations are low-end.**"



6. What would you change or improve in the Focus Area? (n=411)



Notable comments:

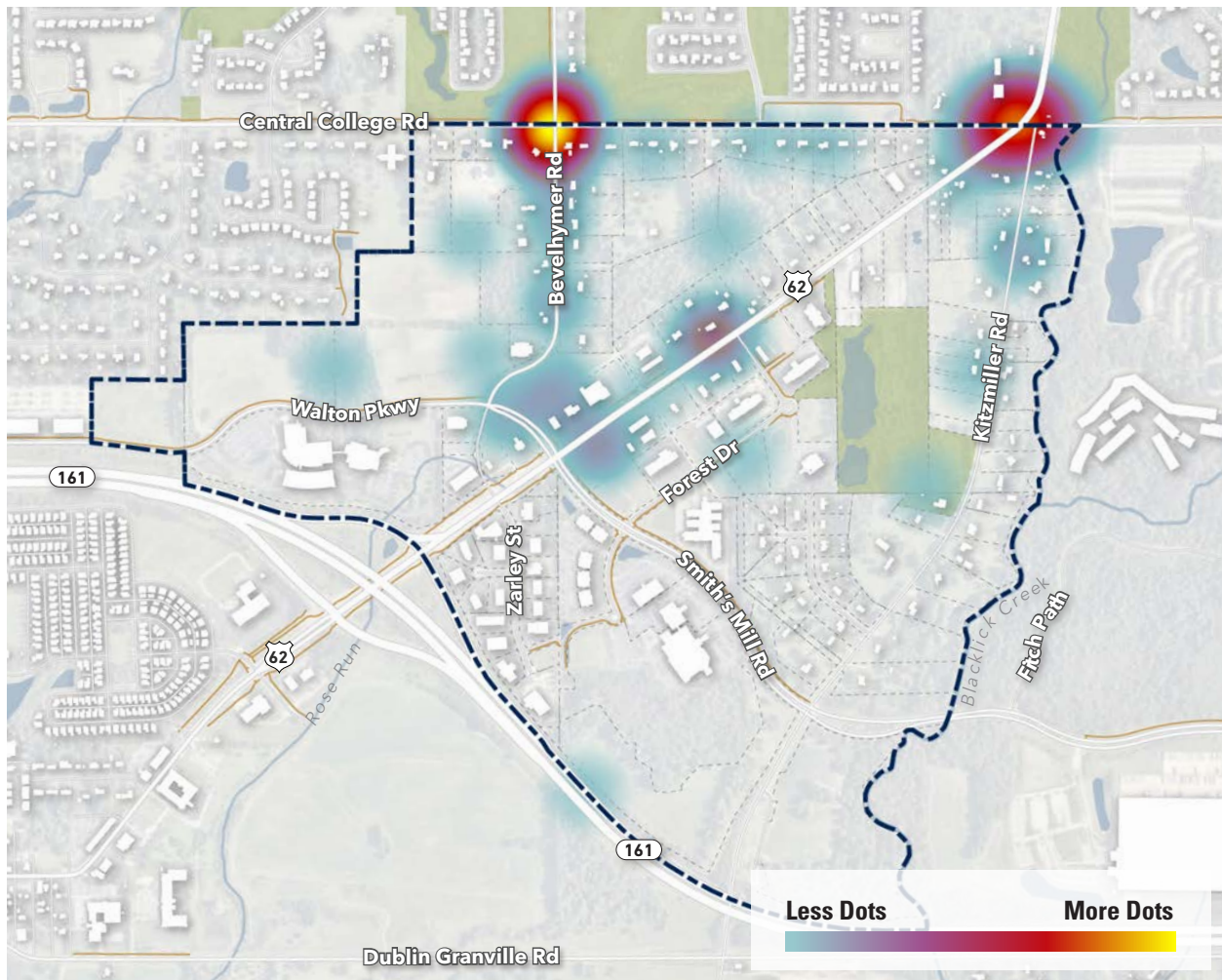
"The intersection of Central College/62 and Kitzmiller is a **bad place for kids to cross on the bike path.**"

"...adding some **dedicated bike paths along Bevelhymer Road** that allow riders better access to Bevelhymer and Rocky Fork Parks..."

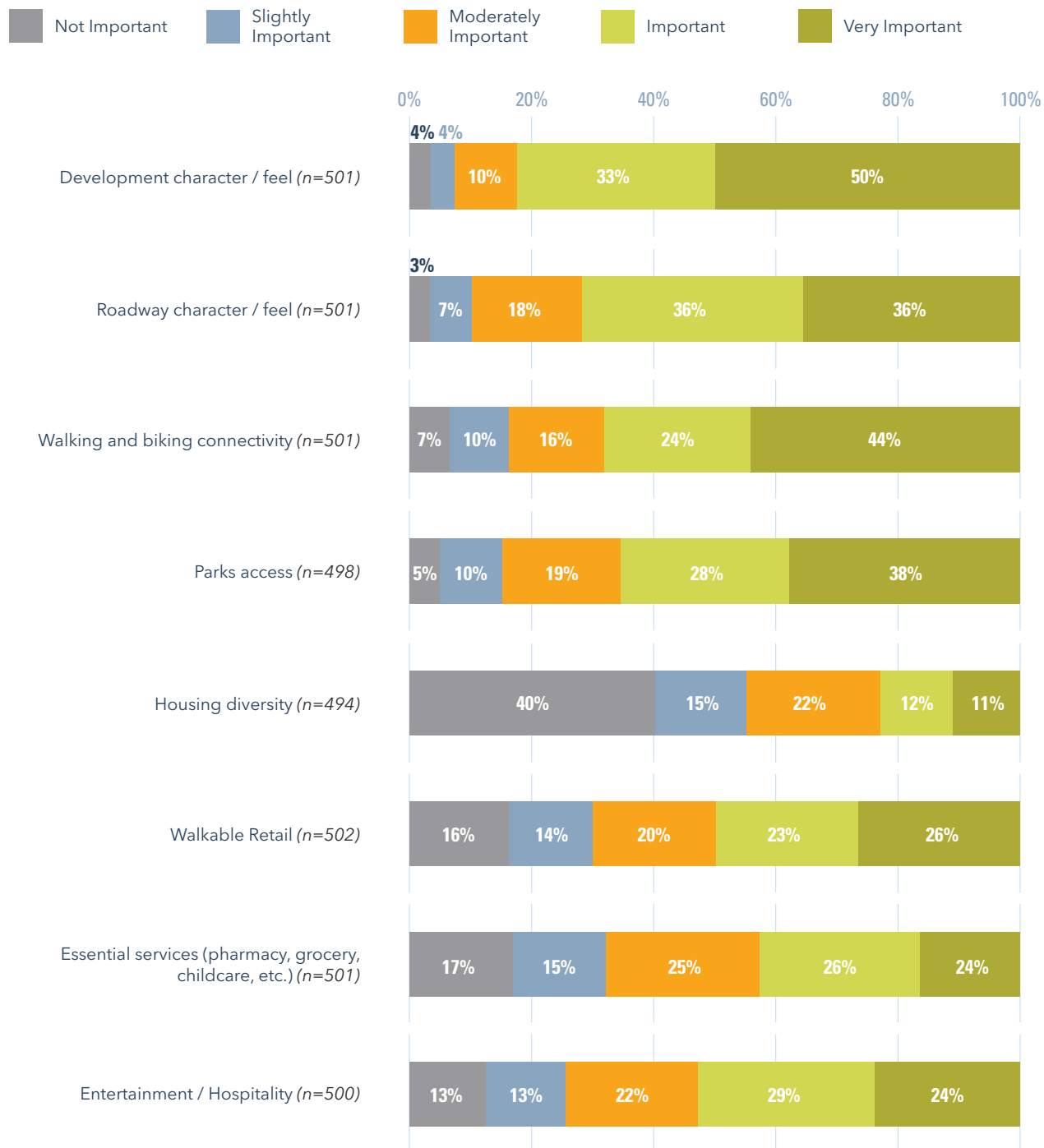
"Pedestrian friendly; more **interesting, local** restaurants or businesses..."

"...Develop the **Kitzmiller Wetland Park** in the way it was originally envisioned with limited impact on the natural features, but adding trails and other amenities..."

Community Workshop #1: What would you change or improve in the Focus Area?



7. How important are the following topics for this plan to address?



8. What types of mobility improvements would you like to see happen here? (Choose as many options as you'd like) (n=493)



69%

Intersection Improvements



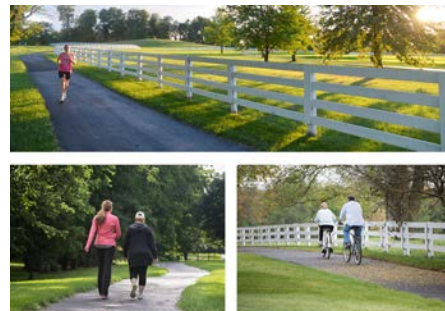
56%

Safer Crosswalks



51%

Traffic Calming Measures



69%

Additional Leisure Trail Connections



26%

Velo Loop / Cycle Track



66%

Streetscape Improvements



18%

On-Street Bike Infrastructure

- » Add speed bumps
- » Add traffic circles
- » Improve "5-points" intersection
- » Improve existing on-street bike lane

11%

Other Ideas

9. What type of residential development would be best suited for this Focus Area? (Choose as many options as you'd like) (n=512)



28%

Courtyard Building or Cottage Court



69%

Detached Single-Family



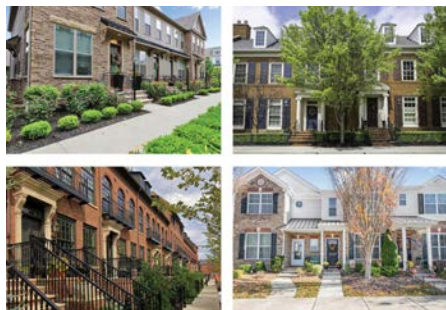
8%

Duplex



5%

Triplex or Fourplex



40%

Townhomes



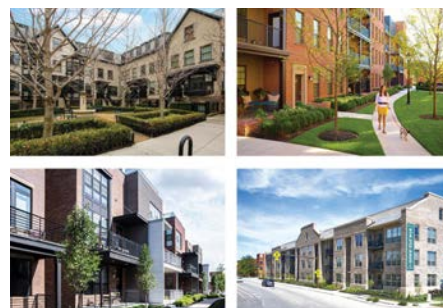
7%

Multiplex



41%

Live-Work Units or Commercial/Residential Mixed Use



8%

Mid-rise Apartment Building

10. What type of employment center development would be best suited for this Focus Area? (Choose as many options as you'd like) (n=497)



Corporate Office

28%



Professional Services

68%



Healthcare Office / Medical Center

41%



Civic / Institutional

42%



Light Industrial / Flex Office

21%



Logistics / Warehousing

2%



Research & Development

18%

- » Co-working spaces
- » Small business opportunities
- » Limit additional employment uses in the Focus Area

Other Ideas

13%

11. What type of retail development would be best suited for this Focus Area (Choose as many options as you'd like) (n=519)



30%

Fast Casual Dining / Restaurant



85%

Full Service Dining / Restaurant



5%

Fast Food Restaurant



48%

Bar / Brewery



22%

Grocery / Supermarket



61%

Small Specialty Grocer



22%

Personal Service Business



59%

Specialty Store



9%

Care Facility



16%

Fitness Facility / Gym



49%

Entertainment Business



10%

Hotel / Accommodations



2%

Automotive Business



36%

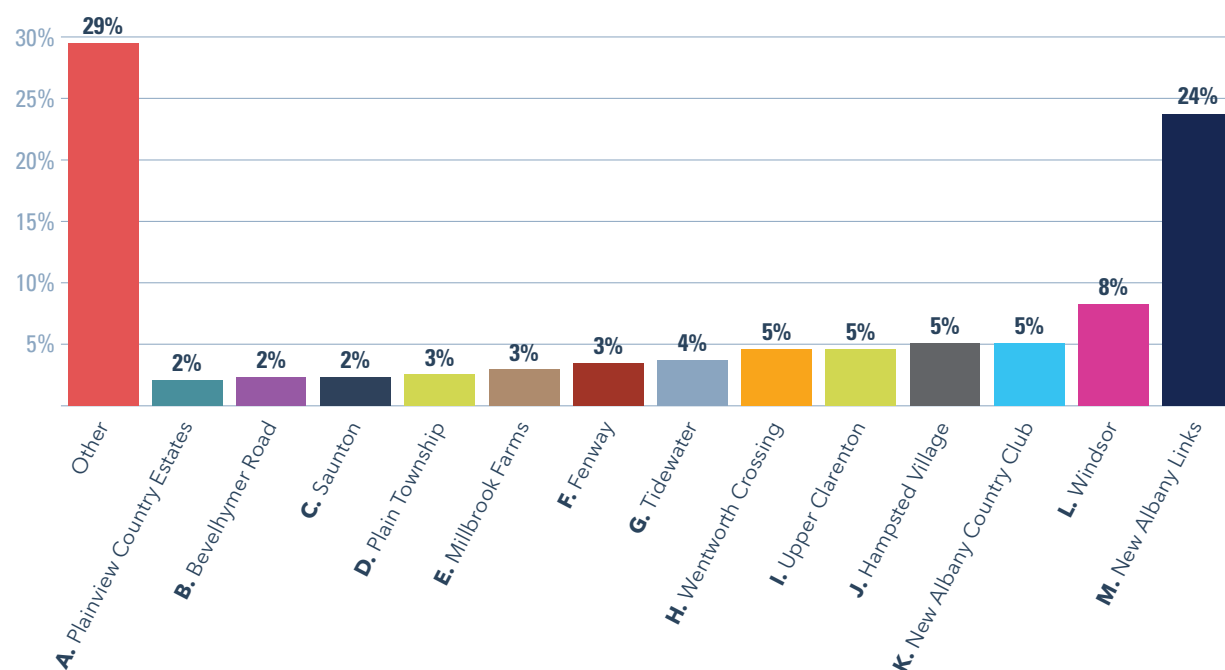
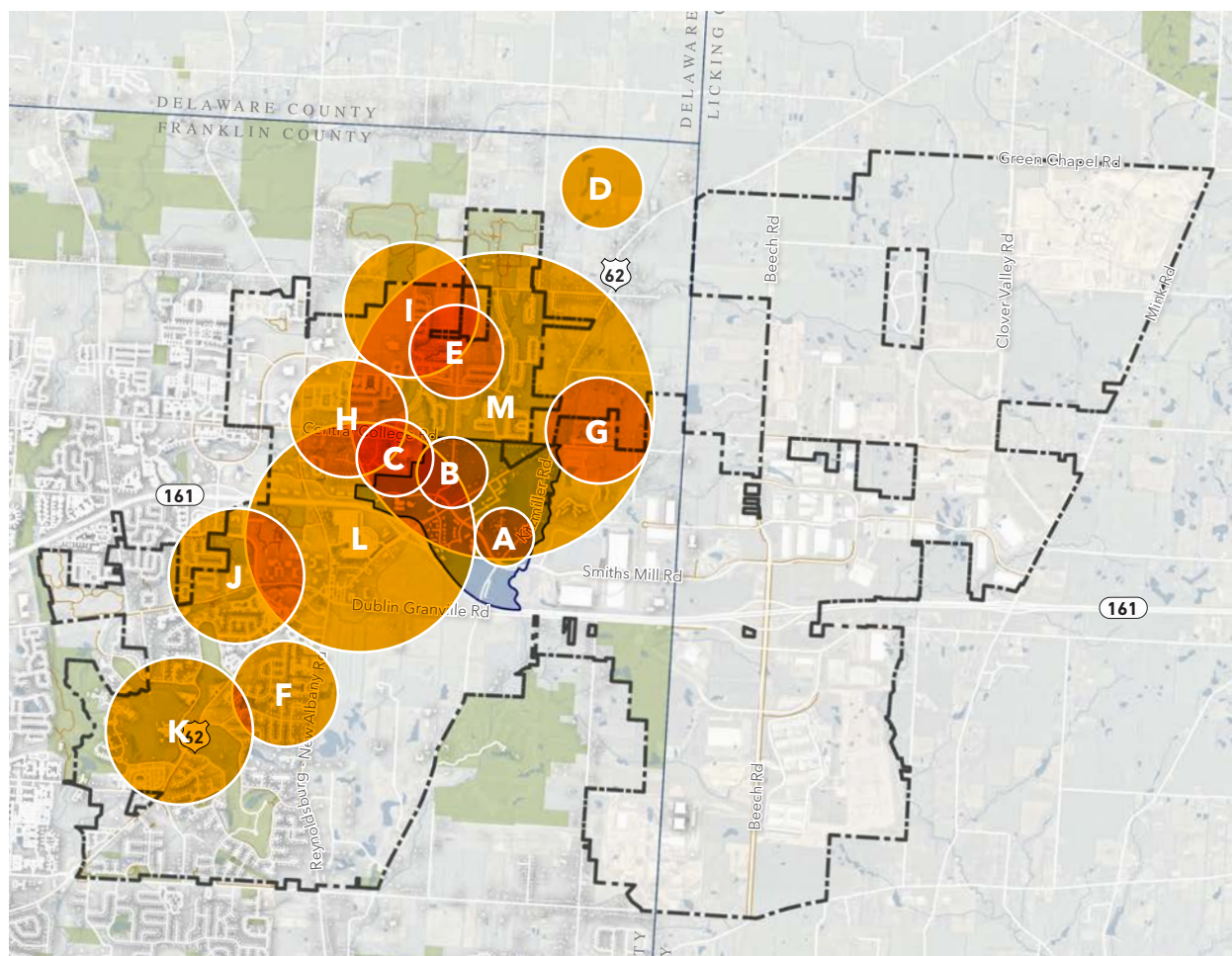
First Floor Retail Uses in a Mixed-Use Development

- » Trader Joe's
- » Limit additional fast-food restaurants
- » Market or food hall

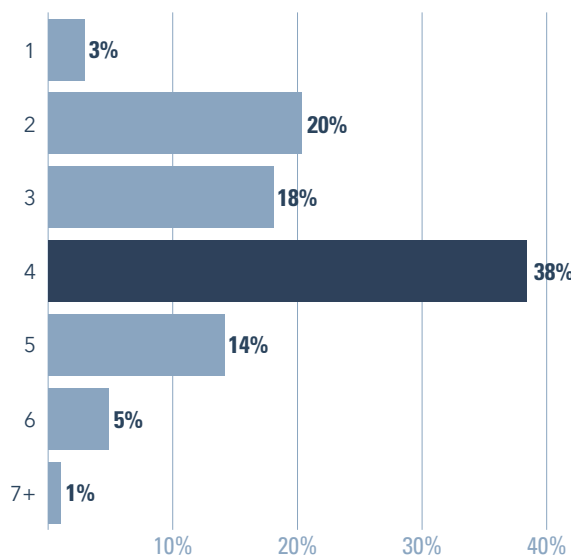
12%

Other Ideas

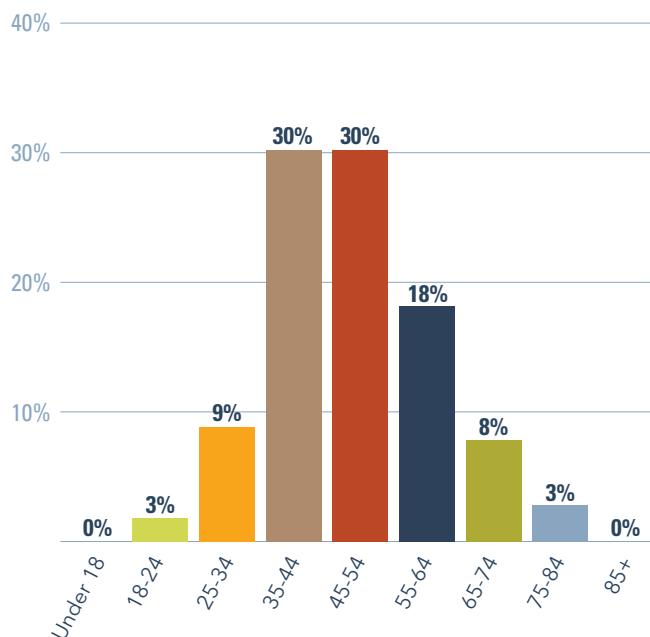
12.If you live in New Albany, in what neighborhood do you live? (n=435)



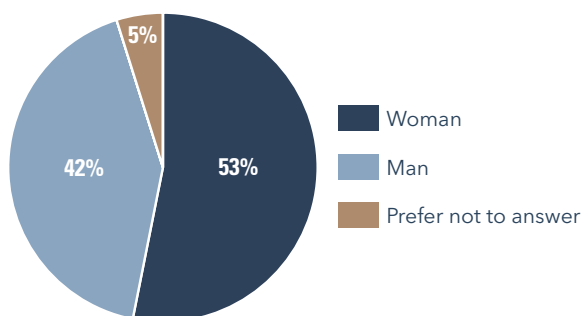
13. Including yourself, how many people live in your household? (n=409)



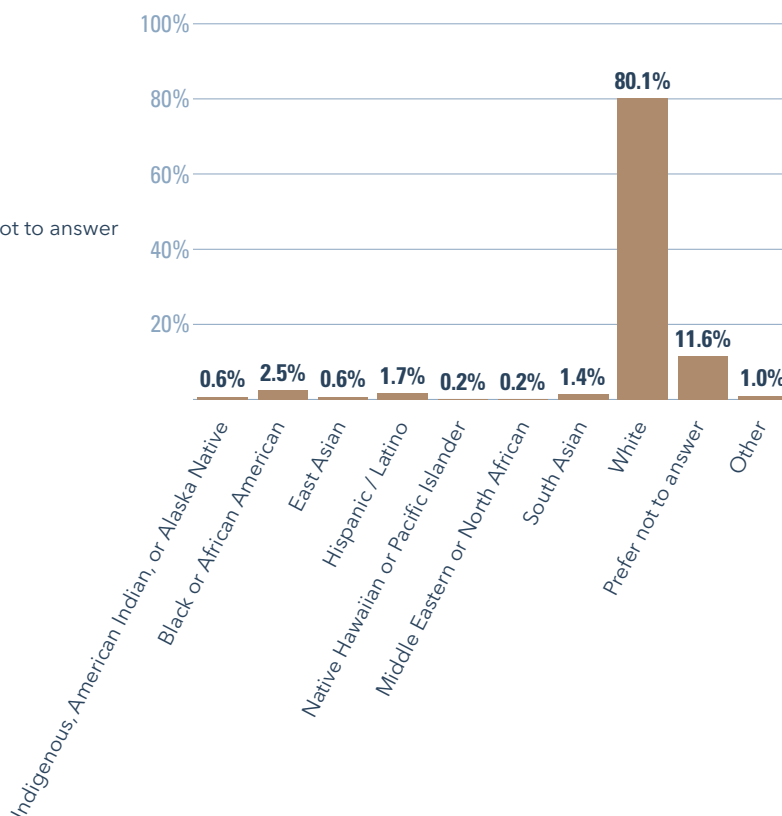
14. What is your age? (n=496)



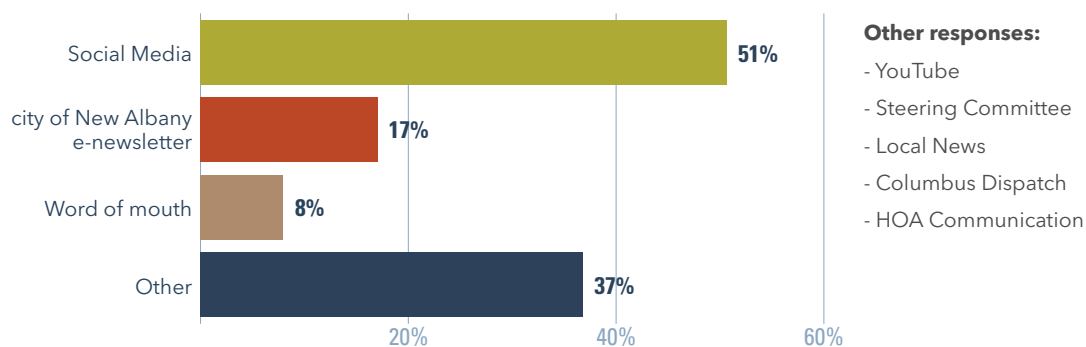
15. How would you describe your gender? (n=495)



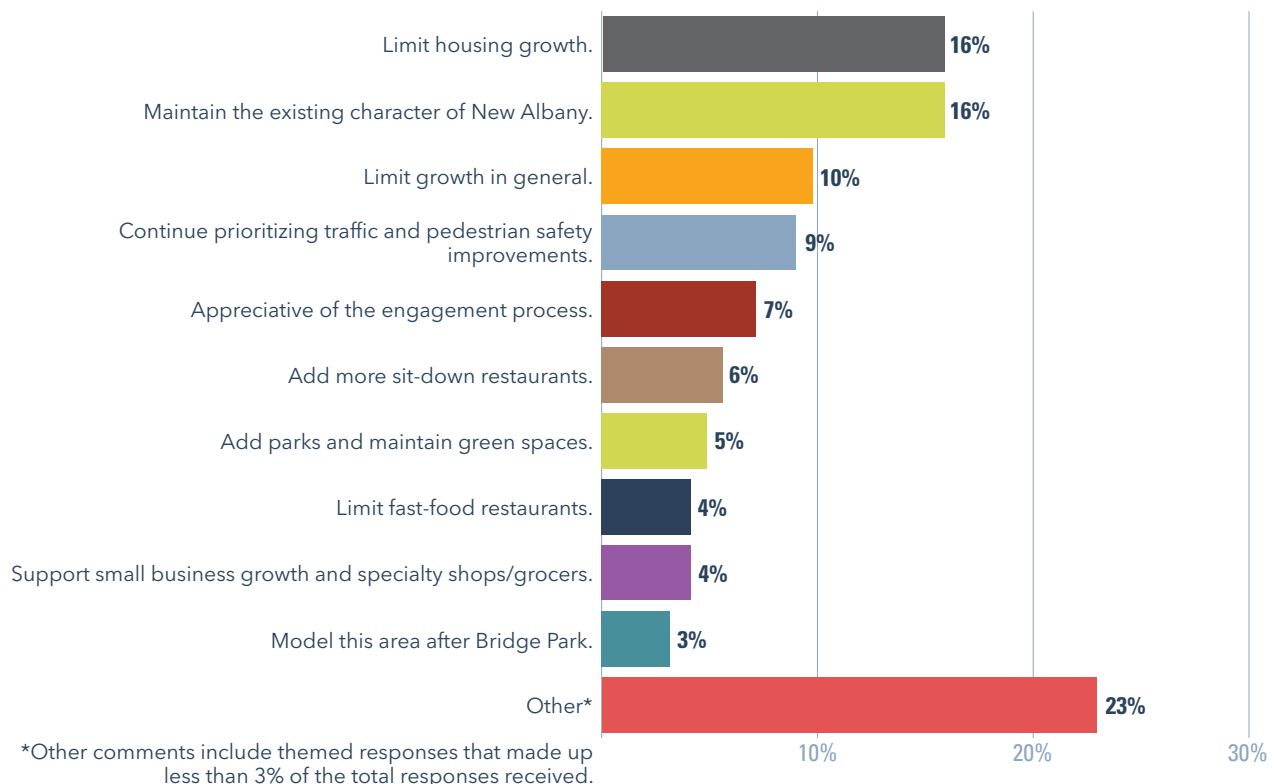
16. Which of the following best describes your race / ethnicity? Select all that apply. (n=483)



17. How did you hear about this survey? (n=505)



18. Is there anything else you'd like to share with us? (n=266)



Other responses:

- Not satisfied with previous public engagement efforts.
- Add leisure trails and bike paths.
- Looking forward to this planning process.
- Prioritize the natural environment.
- Leave residential areas alone.

COMMUNITY ENGAGEMENT THEMES

Based on the first phase of community engagement, including the first steering committee meeting, Community Workshop, and survey, the planning team summarized the input into five themes. The themes are summarized below:



Residents desire responsible growth and development in the Focus Area.

Commercial and retail growth in the Focus Area has received mixed reviews. Residents expressed that the fast-food and auto-oriented uses recently developed along US-62 do not align with the community's aspirations for the Focus Area. Residents also have mixed feelings about housing, with some desiring more variety in housing options, and many who oppose any additional housing in the Focus Area.



Traffic, congestion, and roadway improvements remain a top priority. Daily commuters traveling through the focus area are experiencing higher volumes of congestion along arterial and collector roads. This is also felt by local residents, who have experienced higher traffic volumes along Bevelhymer, Kitzmiller, and Central College Roads. Additionally, the "five-points" intersection at Central College Road and US-62 is increasingly becoming a safety issue as more commuters travel through the Focus Area to reach nearby employment areas.



Residents prefer different kinds of restaurants, services, and retail than what is offered in the Focus Area. Community members would like to see more commercial uses tailored toward local residents, rather than regional commuters or visitors seeking quick food options and auto-oriented services. However, residents are concerned that any new retail in this area might end up competing with and harming the Village Center.



Pedestrian infrastructure, safety, and connectivity are critical, especially along Central College, Kitzmiller, and Bevelhymer Roads. Residents living within and adjacent to the Focus Area noted that there are limited options for children and family members to access recreational amenities and popular restaurants safely on foot. This is primarily due to gaps in the city's leisure trail network along Kitzmiller Road, Smith's Mill Road, and Bevelhymer Road.



Kitzmiller and Tidewater Parks are potentially catalytic investments to spark a healthier, more active, recreationally oriented district. Residents are excited about the potential for additional passive park space within and near the Focus Area, which can inspire further investment in leisure trail connections, intersection improvements, and pedestrian safety.

COMMUNITY WORKSHOP #2

Around 100 residents attended the second community workshop held on December 2, 2024. This self-paced open house showcased essential elements of the plan, including background information, key findings, constraints, and the direction of the plan. This meeting followed four Steering Committee sessions, during which members engaged in focused discussions and activities that contributed to shaping the plan's direction. Members of the Steering Committee and city Council were also present to help facilitate and answer questions from community members. This workshop was crucial for sharing vital findings and recommendations with the public, including the following:

- **Community engagement results.** Residents analyzed and discussed data from the first phase of community engagement, including survey and Social Pinpoint results.
- **Constraints and market realities.** The planning team shared key findings from the market study, which revealed several constraints related to future retail demand in the Focus Area. Residents were able to discuss these findings with the Planning Team and members of the Steering Committee, including ways to influence better market outcomes.
- **Future land use and public realm investments.** Residents reviewed and discussed the first draft of the Future Land Use Character Map and prioritized key public investments that may serve as catalytic projects for the Focus Area.



COMMUNITY WORKSHOP #3

Community Workshop #3 was a critical milestone in preparation for plan adoption. Around 75 residents attended the final community workshop on February 11, 2025. The meeting was an open house format with exhibits displaying draft content and recommendations from the plan. Draft plan documents were provided for residents to review and provide feedback. Several Steering Committee members were also in attendance to provide additional support and share their experience participating in the planning process. The overall goal of the third workshop was to test final concepts and draft recommendations to ensure alignment with community feedback. As a result of comments received at this event, several revisions were made to the plan in accordance with community concerns.





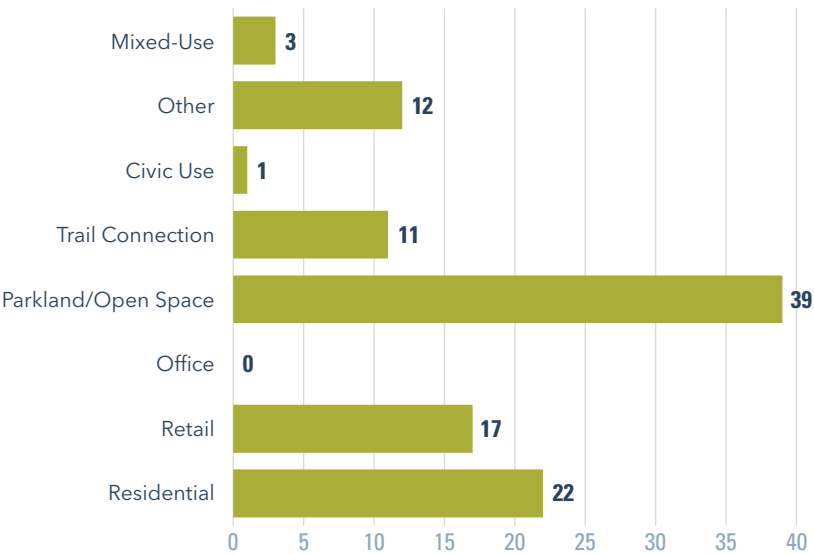
SOCIAL PINPOINT

Social Pinpoint is an online engagement platform that uses interactive mapping tools to collect feedback from residents. The website was launched in September 2024 and included an Interactive Land Use Strategy Map to gather input on desired land uses, roadway/intersection improvements, trail connections, and other opportunities to be considered as part of the Focus Area’s land use and thoroughfare plan. In addition to the Steering Committee’s insights about specific sites in the Focus Area, the feedback collected from Social Pinpoint helped shape the plan’s framework for future land use and thoroughfares.

Parkland/Open Space was the most frequently placed marker on the map, primarily clustered west of US-62 around existing open spaces, undeveloped parcels, and tree canopy areas.

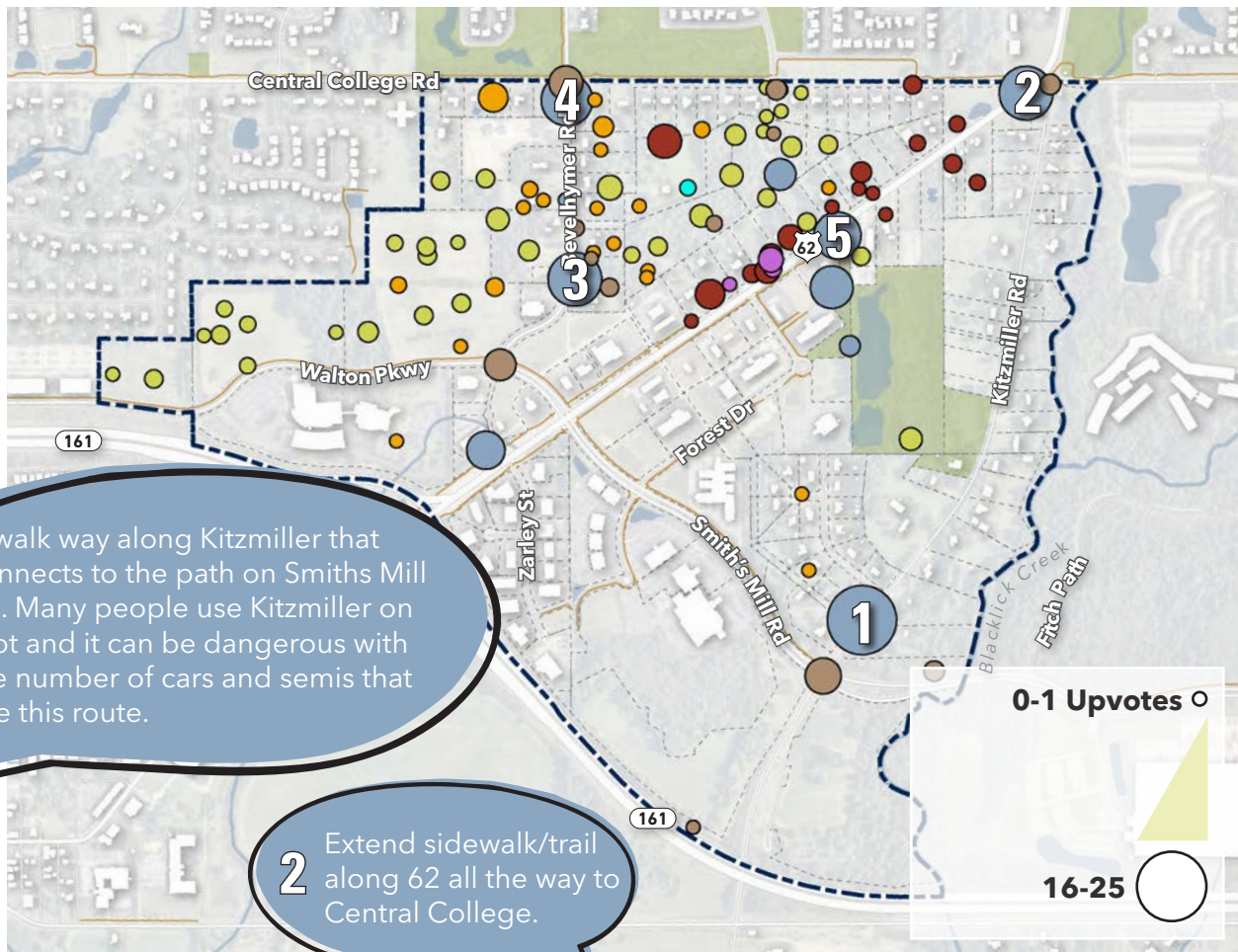
Residents were able to ‘upvote’ markers placed on the map if they agreed with the placement or related comment. The top five upvoted comments were all related to potential bike and leisure trail connections that would improve pedestrian safety throughout the Focus Area.

Number of Land Use Markers Placed, n=105



Top 5 Comments, Ranked by Upvotes

#	Comment	Votes
1.	A walk way along Kitzmiller that connects to the path on Smiths Mill Rd. Many people use Kitzmiller on foot and it can be dangerous with the number of cars and semis that use this route.	25
2.	Extend sidewalk/trail along 62 all the way to Central College.	18
3.	A bike path connecting the links to US-62 from The Links	18
4.	There needs to be trail connectivity to retail on Rt 62 from bevelhymer rd.	17
5.	this trail ends here. It would be great to extend all the way up to Central College Rd	15



Legend

Plan Boundary	
Existing Leisure Trails	
Parks/Open Space	
Parcel Boundaries	
Parkland/Open Space	
Residential	
Retail	
Mixed-Use	
Trail Connections	
Other	
Civic	

MARKET STUDY

MARKET ANALYSIS

Development Strategies, a consulting firm specializing in economic/market research and community development, conducted a market study to evaluate the local and regional retail landscape within the Focus Area, citywide, and the surrounding area. Early feedback gathered during the planning process, where residents and Steering Committee members expressed a desire for more high-quality, place-based retail and amenities, necessitated a focused look into how these desires align with the current real estate market. Given that the

existing commercial and retail environment in the Focus Area conflicts with community aspirations, the market study provides insights into unmet demand, competitive pressures, and strategic interventions that may help generate a shift in the retail market over time. Additionally, the city, Planning Team, and Steering Committee utilized these market findings throughout the planning process to inform research, discussions around development, and considerations for future land uses in the Focus Area.

MARKET STUDY OVERVIEW

The market study consists of two components:

Market Analysis

Identifies current conditions and quantifies opportunities.



PEOPLE (WHO)

Identifies demand



PRODUCT (WHAT)

Identifies supply



PLACE (WHERE)

Identifies location

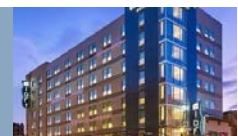
Market Strategy

Focused on how to change market conditions and capitalize on opportunities.

HOW DO WE GET THEM THERE?



WHAT DO WE BUILD?



WHERE DO WE CREATE IT?

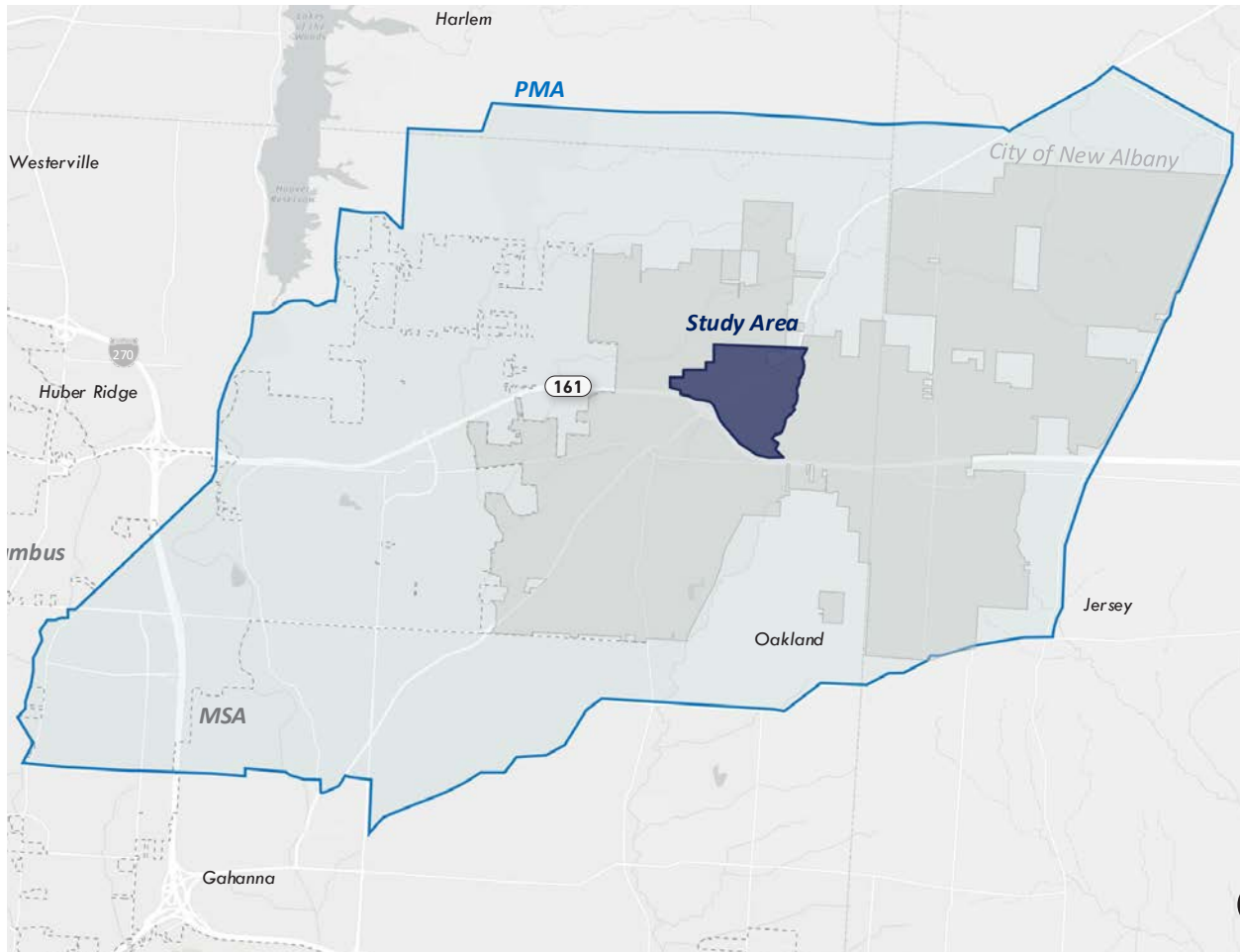


KEY FINDINGS

The following key findings summarize the main takeaways and considerations from the Market Analysis.

- **The Focus Area is in a competitive real estate market.** Several highly-visited commercial centers are located within the Primary Market Area (PMA), including New Albany Village Center, Hamilton Quarter, retail areas north and south of the New Albany Road East interchange, Easton Town Center, and Hampsted Village Center.
- **Retail demand greatly exceeds supply in the PMA.** While there are plenty of commercial options, higher-than-average consumer purchasing power in the PMA is creating a larger market for a wide range of commercial establishments, especially auto-oriented or suburban-scaled uses.
- **Substantially higher growth in jobs than residents.** While both figures steadily increased over the last decade, New Albany's business park added over 10,000 jobs. This has significant impacts on the types of commercial and retail uses that follow to support these industries and their employment base.
- **Positive retail visitation trends in the Focus Area indicate support for the current mix of uses.** Workers and commuters are the primary drivers of retail demand in the Focus Area, which has led to an increase in auto-oriented land uses like drive-throughs, quick service, car wash, auto shops, and grocery stores.
- **The Focus Area may capture 25,000 to 30,000 additional square feet of retail in the next 10-15 years.** This assumes that the Focus Area absorbs 50% of the net demand in New Albany, which accounts for demand after pipeline projects are built. If the future growth along US-62 reflects the current development pattern, 25,000 to 30,000 additional square feet roughly translates into three to five out-lot buildings.
- **Office vacancy in the Focus Area remains low, with limited office development energy since the pandemic.** Data indicates that offices in New Albany have recovered stronger than the PMA, partially due to building age and office workers living closer than average to their workplace. Market research indicates that office real estate has a higher chance of remaining stable post-Covid if located near walkable, mixed-use retail areas, restaurants, and civic amenities.



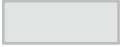

Primary Market Area (PMA)



PRIMARY MARKET AREA (PMA)

The market findings are based on the Primary Market Area (PMA), which captures areas generally within a 10-minute commute from the Focus Area to areas with similar socio-economic conditions. This includes retail centers like Hampsted Village Center, Hamilton Quarter, and Easton Town Center. The market study also uses the Columbus Metropolitan Statistical Area (MSA) for regional analysis and context where appropriate.

Legend

- Plan Boundary 
- Primary Market Area (PMA) 
- New Albany Boundary 
- Metropolitan Statistical Area (MSA) 

86,000
Population, PMA

36%
Population Change, 2010-2023, PMA

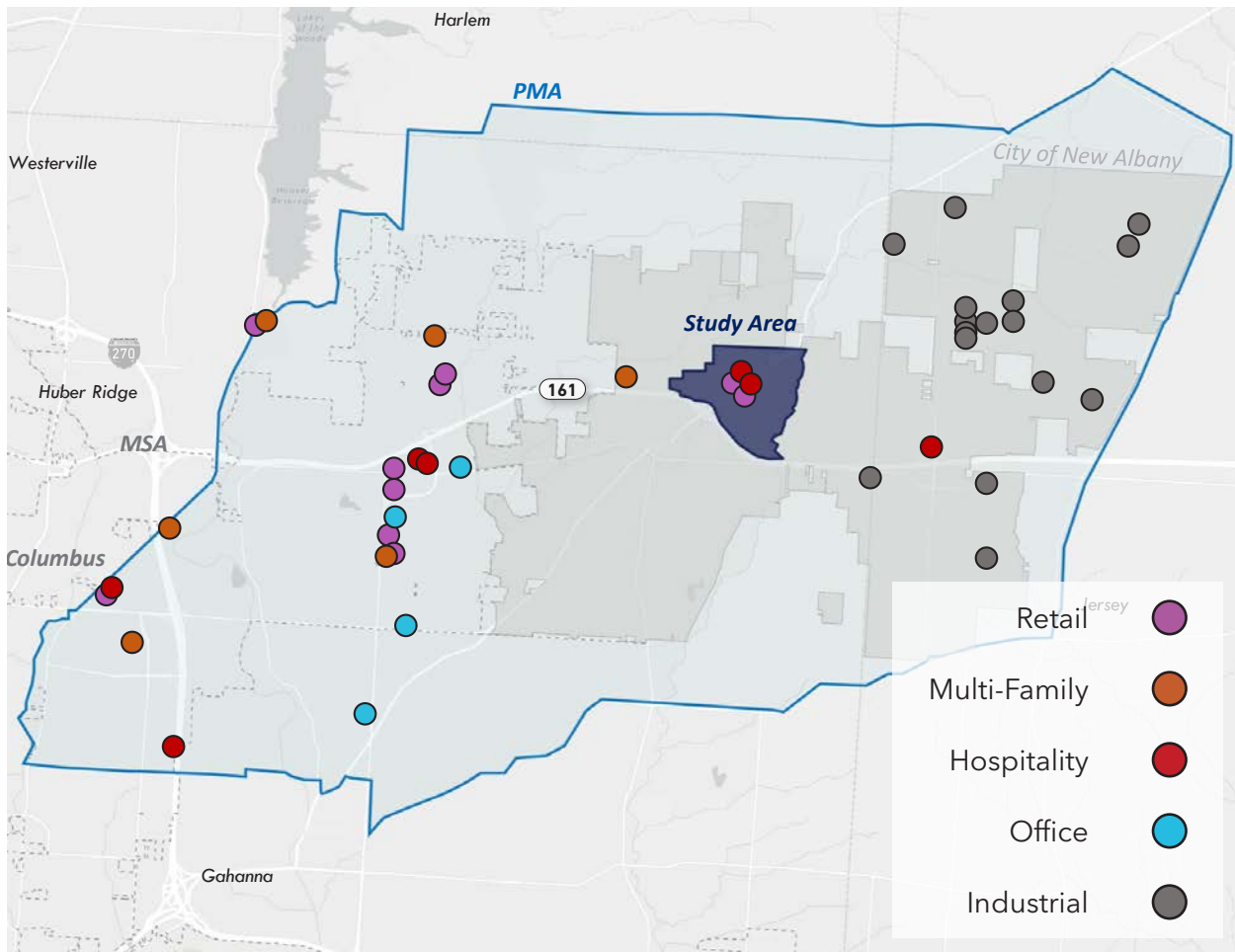
69,400
Jobs, PMA

\$101,000
Median Household Income, PMA

\$317,000
Median Home Value, PMA

+19,800
Jobs Added Since 2010, PMA

Recent Development, Proposed and Under Construction in PMA



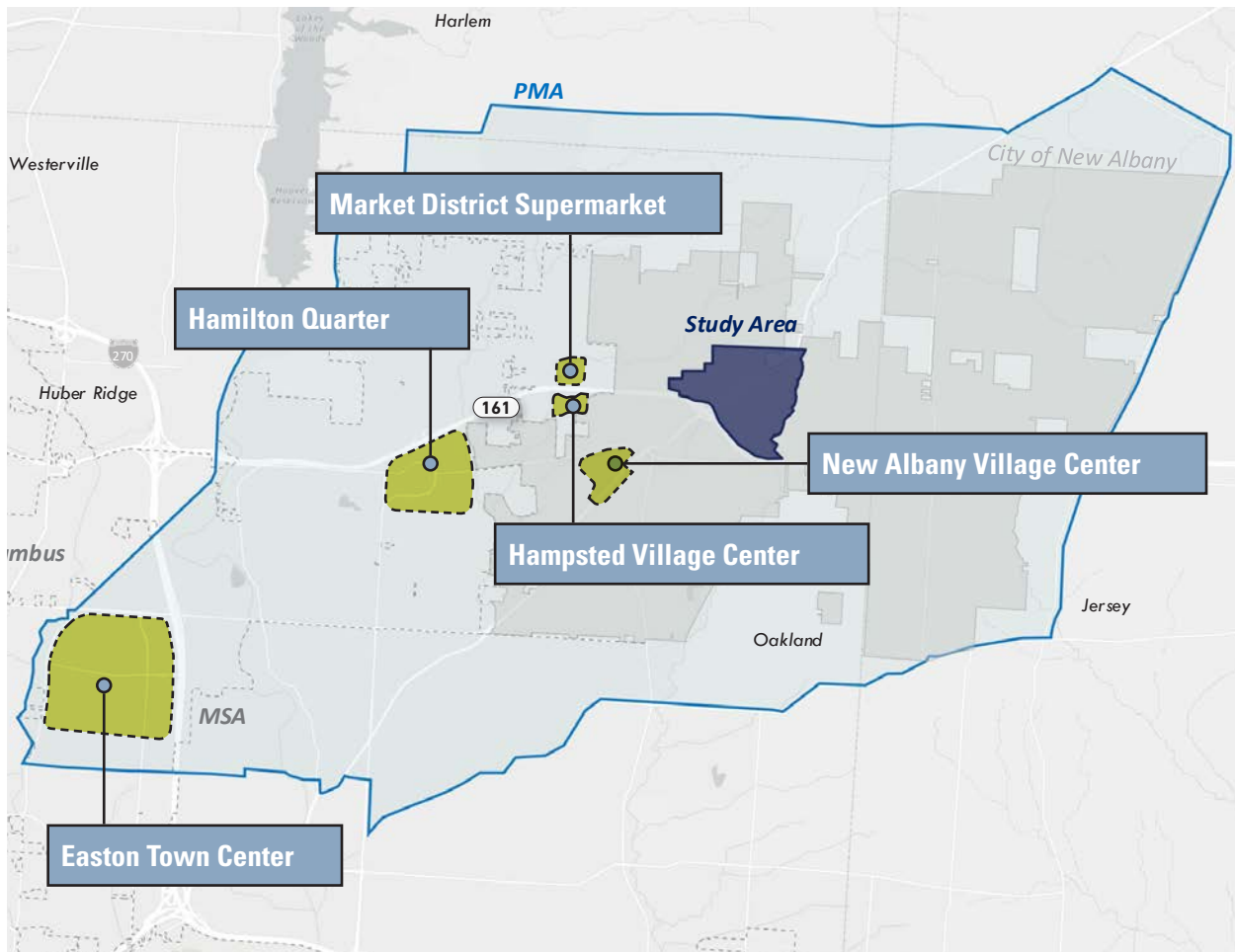
RECENT DEVELOPMENTS IN THE PMA

The map above shows recently proposed or constructed retail, multi-family, hospitality, office, and industrial uses in the PMA. Recent retail proposals or built projects occurred primarily along Hamilton Road near higher-density residential areas. Four new hotels were recently constructed in New Albany, two located within the Focus Area and two near New Albany's business park, where over 15 new employment uses were proposed or are under construction.

Legend	
Plan Boundary	
Primary Market Area (PMA)	
New Albany Boundary	
Metropolitan Statistical Area (MSA)	

Recent Developments, PMA



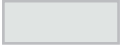

	Multi-Family	Retail	Hospitality	Office	Industrial
Under Construction	193 units	25.7k sqft.	319 rooms	0 sqft.	1.92M sqft.
Proposed	992 units	81.8k sqft.	317 rooms	68k sqft.	3.83M sqft.



RETAIL DESTINATIONS IN THE PMA

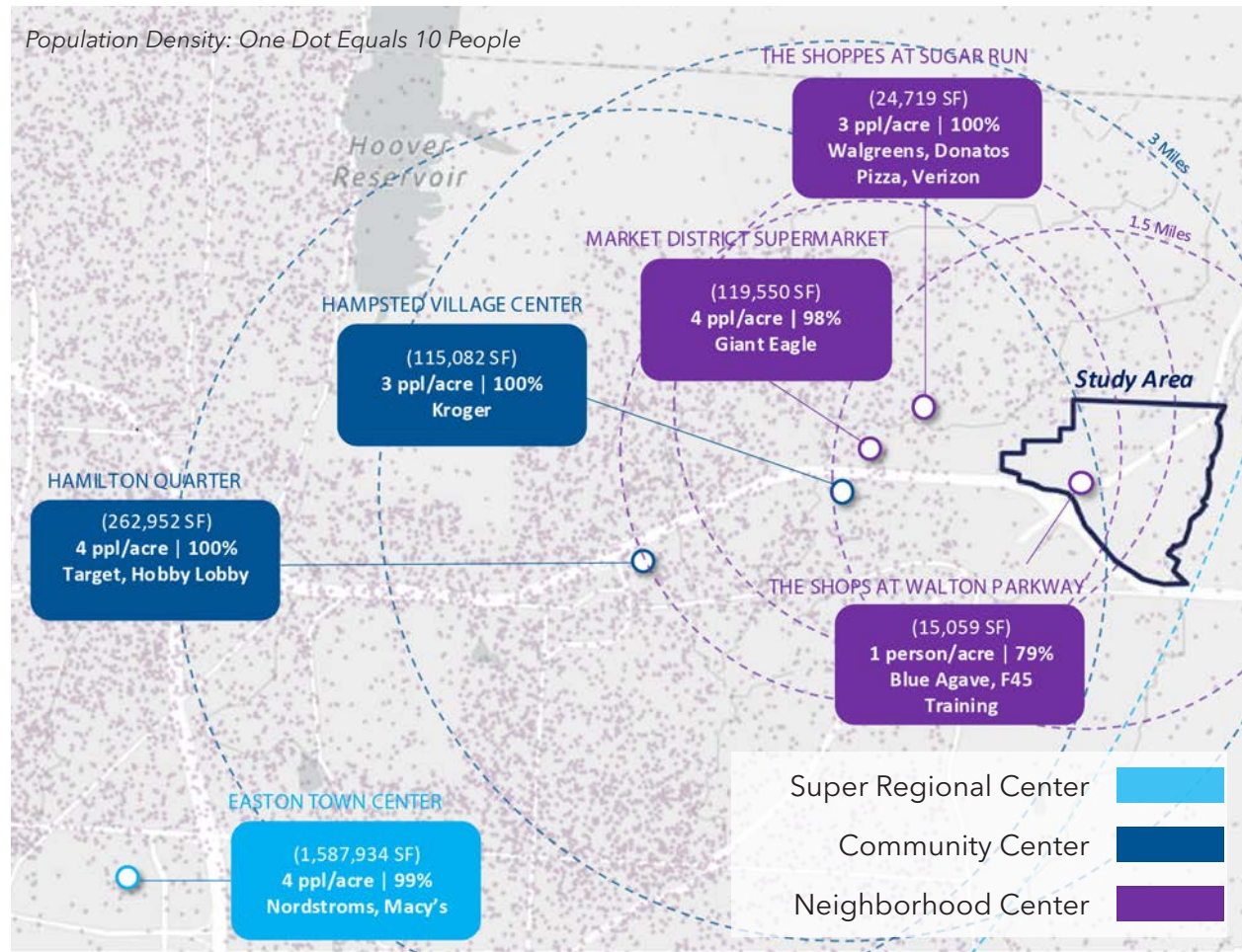
The Focus Area is situated in a competitive retail environment, with several commercial destinations like Easton Town Center and Hamilton Quarter serving a regional market. However, total retail demand within a two-mile radius of the Focus Area exceeds supply by \$75 million, indicating strong consumer purchasing power within the immediate surrounding area.

Legend

- Plan Boundary 
- Primary Market Area (PMA) 
- New Albany Boundary 
- Metropolitan Statistical Area (MSA) 

Retail Characteristics, 2-Mile Radius and PMA

	Total Retail Demand	Total Retail Supply	Median Household (HH) Income	Avg. Annual Retail Spending per HH
2-Mile Radius	\$113M	\$38M	\$154,000	\$25,000
PMA	\$1.7B	\$1.5B	\$106,000	\$17,600



TYPES OF RETAIL CENTERS

The demand for different types of retail centers is greatly influenced by population density. Super Regional Centers, such as Easton Town Center, are designed to cater to entire metropolitan areas with over 500,000 residents. On the other hand, Neighborhood Centers are designed to serve 3,000 to 40,000 residents, typically found in suburban areas like New Albany. These retail centers have varying leasable space, which determines the types of retail users that typically occupy them.



The Shops at Walton Parkway

Neighborhood Center

Typically anchored by a grocery store, with smaller shops like pharmacies, dry cleaners, and convenience stores.

TYPICAL ANCHOR
Drugstore/Supermarket

TRADE AREA RADIUS
1.5 miles

GROSS LEASABLE AREA
30k - 100k square feet

DRIVE TIME
5-10 minutes

POPULATION SUPPORT
3k - 40k



Hamilton Quarter

Community Center

Includes larger retailers like department stores, discount stores, and more diverse food options than a neighborhood center.

TYPICAL ANCHOR
Junior Dept. Store

TRADE AREA RADIUS
3 - 5 miles

GROSS LEASABLE AREA
100k - 450k square feet

DRIVE TIME
10 - 20 minutes

POPULATION SUPPORT
40k - 150k



Polaris Towne Center

Regional Center

Features major department stores, a large variety of retailers, and often draws from customers from multiple communities.

TYPICAL ANCHOR
1 - 2 full-line dept. stores

TRADE AREA RADIUS
8 miles

GROSS LEASABLE AREA
300k - 900k square feet

DRIVE TIME
20 minutes

POPULATION SUPPORT
>150k



Easton Town Center

Super Regional Center

Considered the largest retail type, with a vast selection of retailers, multiple major department stores, and a large catchment area.

TYPICAL ANCHOR
3 - 4 full-line dept. stores

TRADE AREA RADIUS
12 miles

GROSS LEASABLE AREA
500k - 2M square feet

DRIVE TIME
30 minutes

POPULATION SUPPORT
>300k

RETAIL SUPPLY/DEMAND IN THE PMA

As a baseline, current retail supply and demand provide insight on the most immediate development energy based on market needs. This data is then used as one of the key inputs for projecting future demand in a given area.

Current retail demand in the PMA exceeds supply by 240,300 square feet. New Albany would potentially capture 24,000 square feet of this demand gap, assuming a market capture of 10%. Additionally, the Focus Area may potentially capture 3,600 square feet, which is generally equal to one additional freestanding building.

The higher-than-average purchasing power of residents significantly impact the retail demand in the PMA. The types of under-supplied retail uses identified generally have larger building footprints and are typical for suburban locations, such as building materials/garden equipment, grocery stores, and general merchandise. Additionally, several auto-oriented uses are currently under-supplied, including auto parts/tire stores and gas stations. The data align with recent retail development trends within and surrounding the Focus Area.

Current Retail Demand, PMA, New Albany, Focus Area

CURRENT PMA DEMAND GAP
240,300 SF

**NEW ALBANY POTENTIAL
CAPTURE (10%)**
24,000 SF

STUDY AREA CAPTURE (15%)
3,600 SF

Over-Supplied in PMA

Clothing and Clothing Accessories (-471,000 sqft.)
Furniture and Home Furnishings (-51,000 sqft.)*
Restaurants and Other Eating Places (-59,000 sqft.)*
Pharmacies, Health, and Personal Care (-35,000 sqft.)
Sporting Goods, Hobby, Book & Music (-5,000 sqft.)

*Use type found within Focus Area

Retail centers like Easton Town Center contribute to an over-supply of clothing, restaurant, and sporting goods stores in the PMA.

This column shows how many additional stores the PMA can accommodate based on the under-supplied square footage.

Under-Supplied in PMA

of Stores Based on Typical Building Footprint

Building Materials, Garden Equipment (265,000 sqft.)	2 Department/Home Improvement Stores
General Merchandise (241,000 sqft.)	2 General Superstores
Grocery Store (154,000 sqft.)*	2 Grocery Supermarkets
Auto Parts/Tire Stores (56,000 sqft.)*	7 Auto Parts Stores
Specialty Food and Liquor Stores (51,000 sqft.)	5 Specialty Grocers
Gasoline Stations (48,000 sqft.)*	7 Gas Stations
Miscellaneous Store Retailers (29,000 sqft.)	3 Convenience Stores
Drinking Establishments (12,000 sqft.)	1 Brew-pub
Electronics and Appliance Stores (4,000 sqft.)	1 Small-Scale Electronics Store

*Use type found within Focus Area

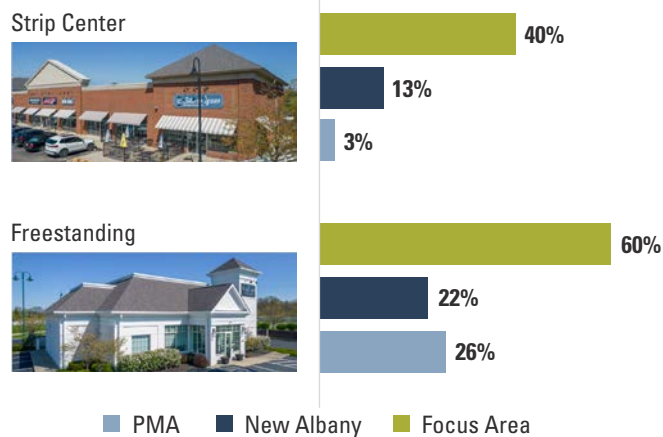
FOCUS AREA MARKET CONDITIONS & SUPPLY TRENDS

Since 2010, the Focus Area's retail inventory more than doubled, adding over 76,000 square feet of new buildings. This accounted for 5% of the 1.67 million square feet of retail space delivered in the PMA since 2010. The two retail types introduced in the Focus Area are classified generally as Strip Center and Freestanding buildings. The Focus Area disproportionately accommodates these types of establishments in New Albany given its proximity to the US-62/OH-161 interchange and the city's employment centers.

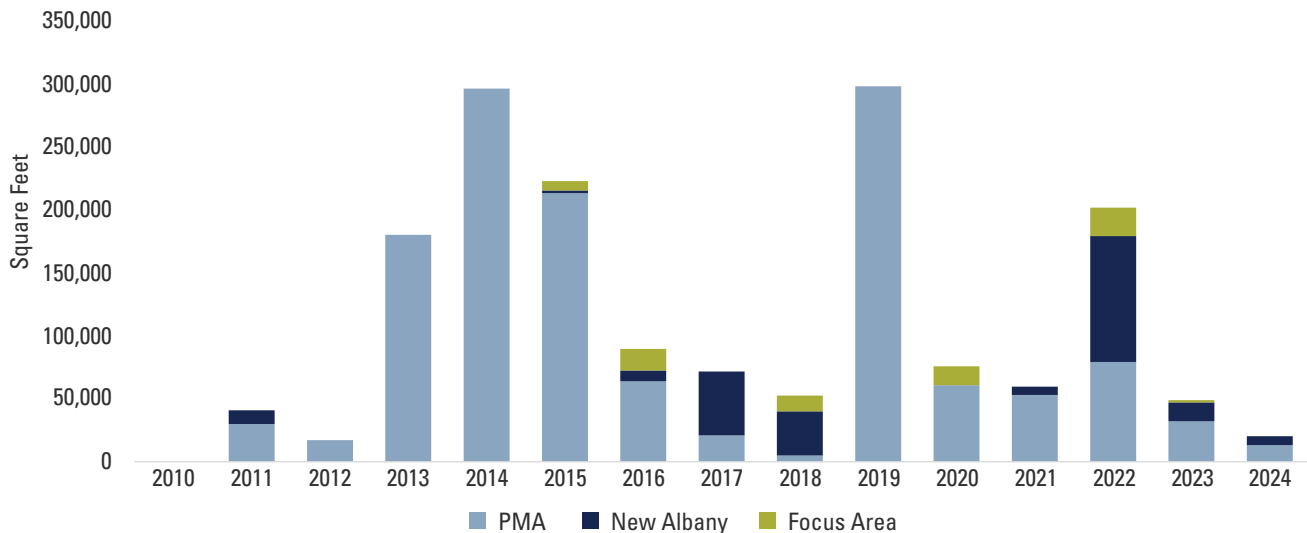
The Focus Area remained relatively underdeveloped until 2010. Significant retail investment in the area did not

begin until this point, over five years after Mt. Carmel Hospital was built and over 15 years after Zarley Industrial Park was introduced. Predating the construction of OH-161, Zarley Industrial Park was one of the first employment areas within a mostly rural residential portion of Plain Township. By 2010, the retail market began to pick up throughout the city, only escalating over the past decade following housing and employment growth. Over time, the Focus Area became a prominent location for commercial uses, with its convenient access to OH-161 and largely undeveloped land areas left over after years of market fluctuation and development energy in other areas of the city.

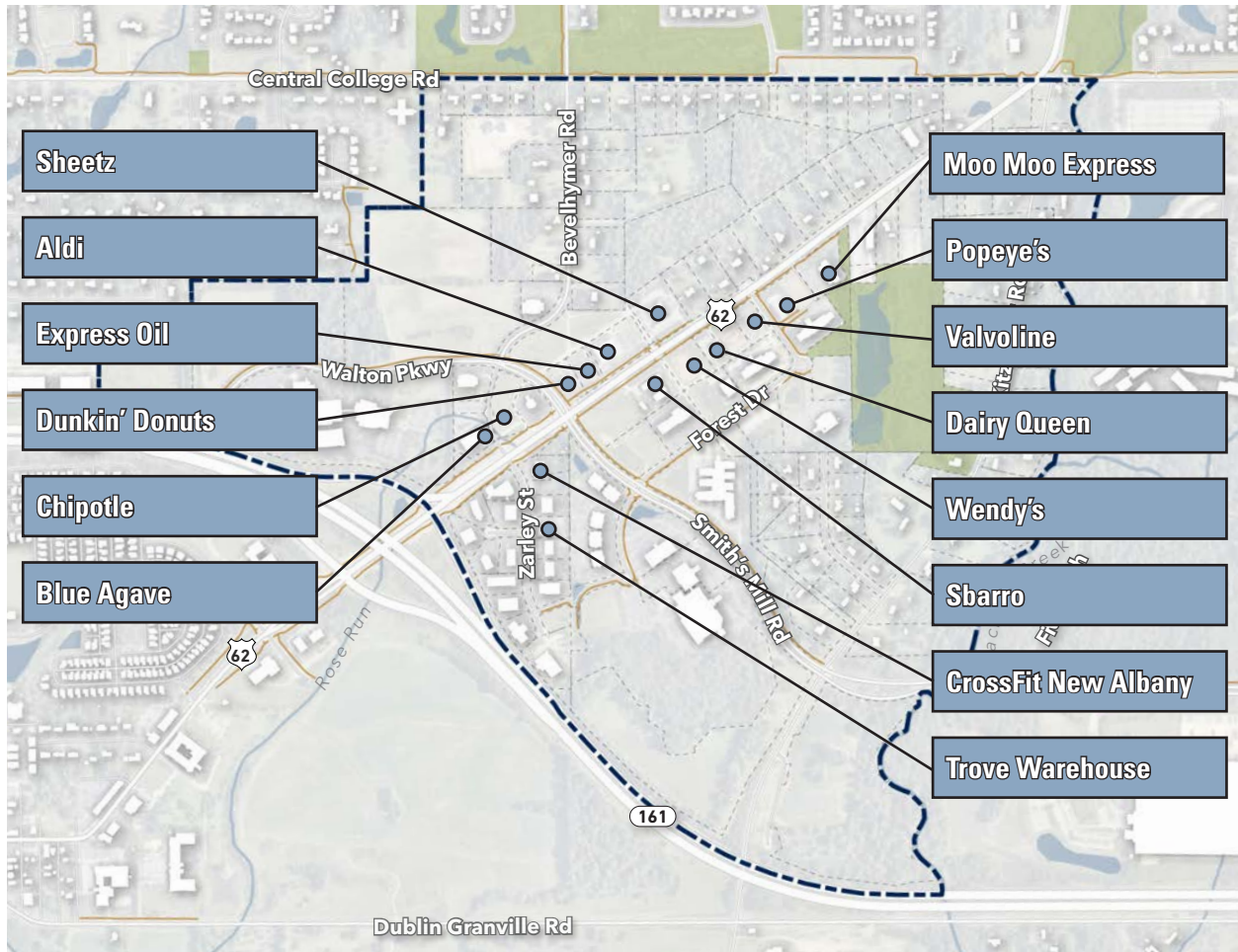
Supply by Retail Center Type



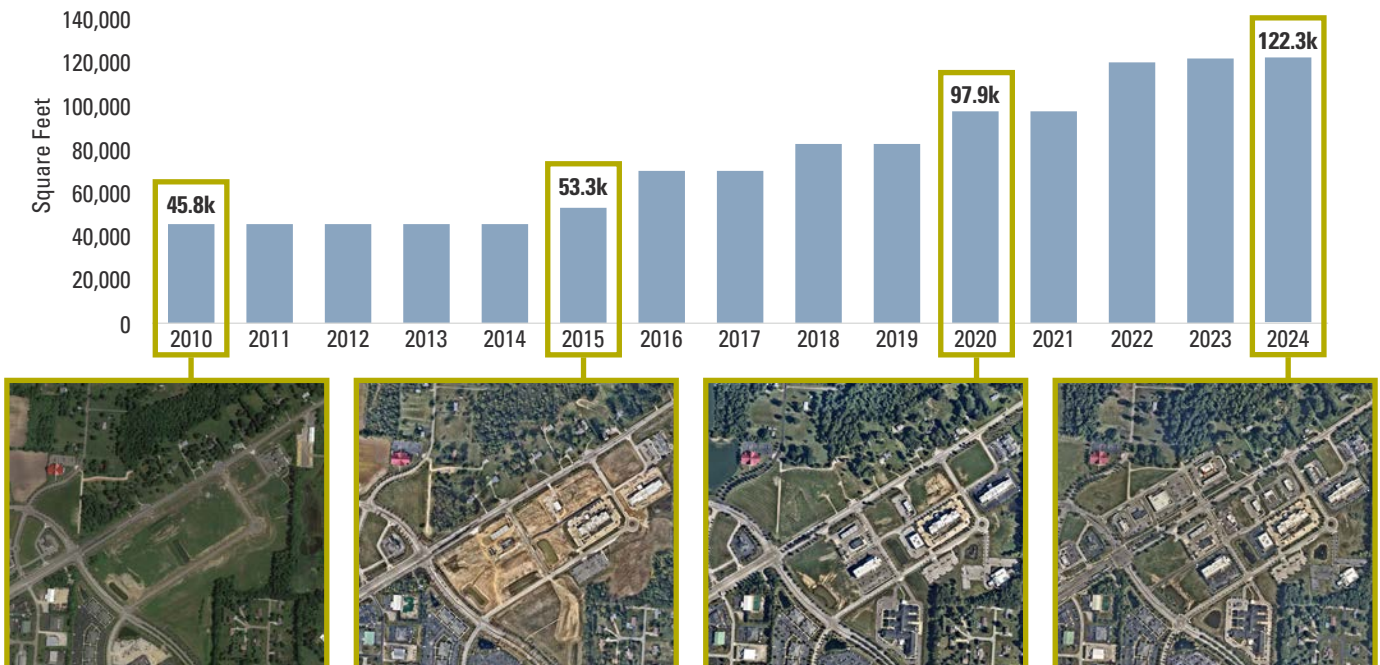
Retail Deliveries in the PMA, New Albany, and the Focus Area, 2010-2024



Retail Establishments in the Focus Area



Retail Inventory in the Focus Area, 2010-2024

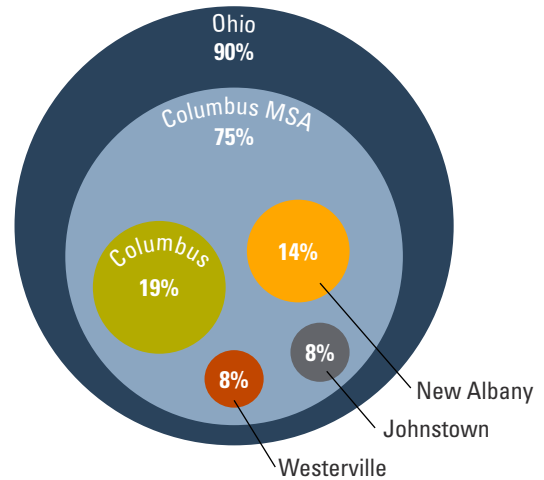


VISITATION

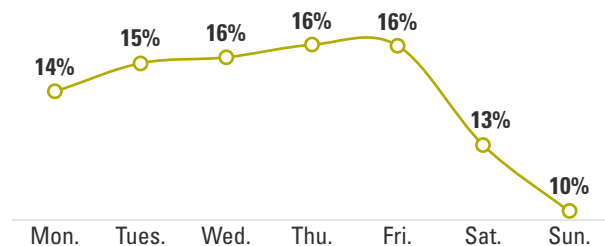
The Focus Area's average monthly visitation quickly recovered post-Covid and surpassed pre-pandemic levels by the end of 2021, indicating a resilient retail market.

Visitation data suggest that the Focus Area's retail supply primarily serves employees and commuters. The Focus Area has high visitation during weekdays, mainly from the Columbus MSA, with nearly half (46%) of visits from visitors living within 10 miles. Overall visitation drops significantly during the weekends as commutes through the Focus Area decrease to and from employment areas.

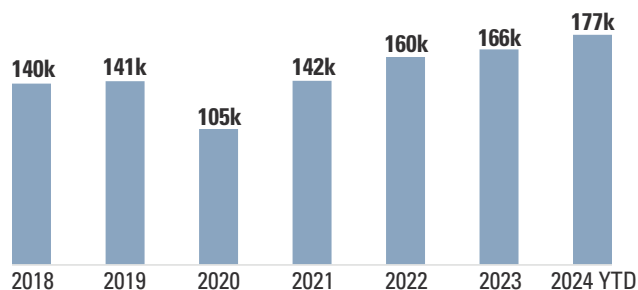
Visitation Rates by Home Location



Visits by Day of Week



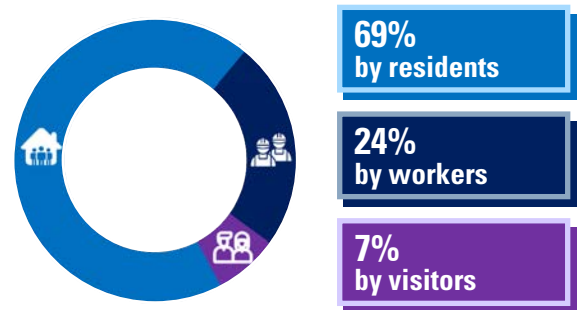
Average Monthly Visitation, 2018-2024 YTD



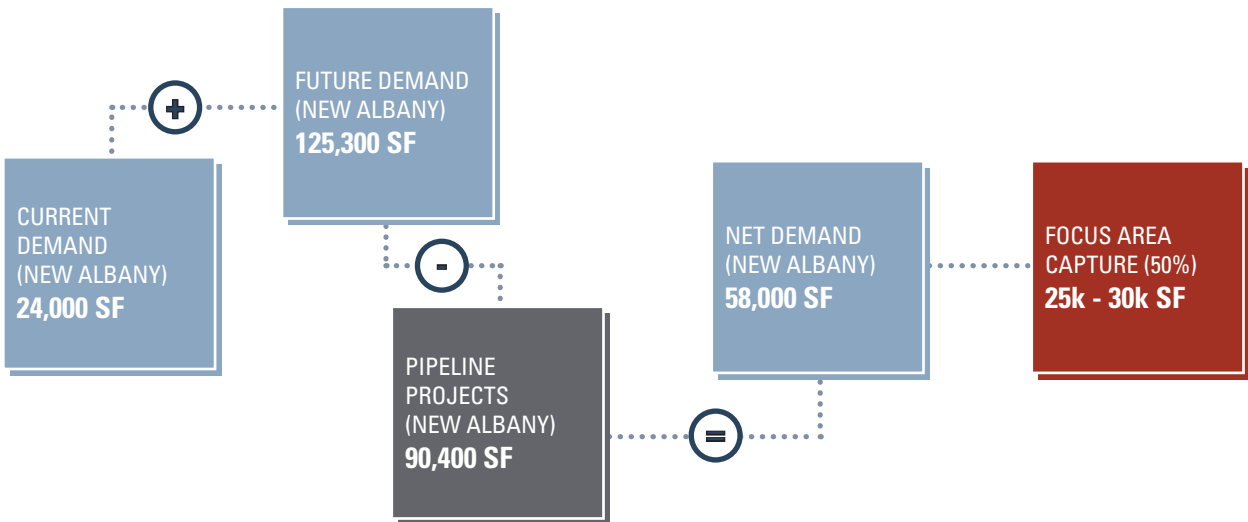
FOCUS AREA RETAIL DEMAND

The Focus Area is projected to capture 25,000 to 30,000 square feet of New Albany’s future retail demand. The three main drivers of retail demand are the number of residents, workers, and visitors within a given area, each affecting the types and locations of future retail deliveries. While residents are generally the primary drivers of retail demand, workers and visitors often significantly impact demand for auto-oriented retail/commercial and hospitality establishments. Based on current visitation data, the market for future retail in the Focus Area is likely to continue focusing on meeting the daily needs of workers and commuters.

Average Annual Retail Spending



Focus Area Future Retail Demand Summary



OFFICE TRENDS

The office market in the Columbus MSA has struggled to recover post-Covid.

Office vacancy remains high in the Columbus MSA at 24.8%, which is up from 23.4% in Q1 2023. Net absorption has remained negative every year since 2019, which quantifies the difference between the amount of space leased and the amount of space vacated in a commercial real estate market over a specific period of time. However, lease rates have remained steady since 2022, with a median office rent of \$21.45 per square foot.

Offices are doing better when located in or near high-quality, mixed-use destinations. Office markets are healthier in areas like Bridge Park and Grandview Yard due to their dense, walkable, and mixed-use oriented design, where intentional efforts were put into placemaking.

Office competition is growing around the Focus Area. While the Focus Area currently supplies 440,000 square feet of office space, no new office space was added since 2010 as the PMA added 2.6 million square feet since 2010. An additional 76,000 square feet of office space was proposed in 2023, none of which is in New Albany.

The Focus Area's office vacancy rates are rising, but remain significantly lower than the PMA. Average vacancy has increased from 1.5% in 2019 to 3.1% in 2024. Although the current average vacancy in the PMA is far higher at 15%.

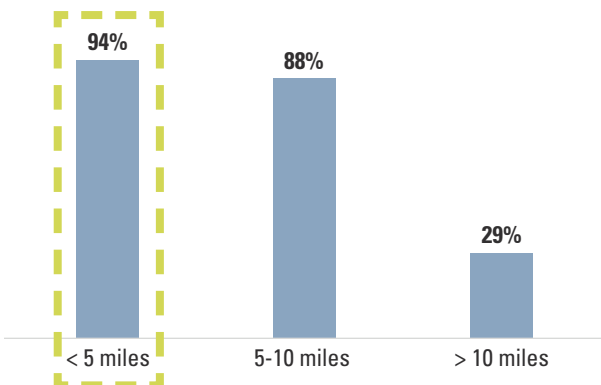
Office rents in the Focus Area have stagnated since 2019, but remain higher than the PMA and regional average. Average office rent in the Focus Area is \$18 per square feet, slightly higher than the PMA (\$17) and the region (\$16).

440,000
Sqft. of Office Space, Focus Area

Office Vacancy Rates, MSA, PMA, Focus Area

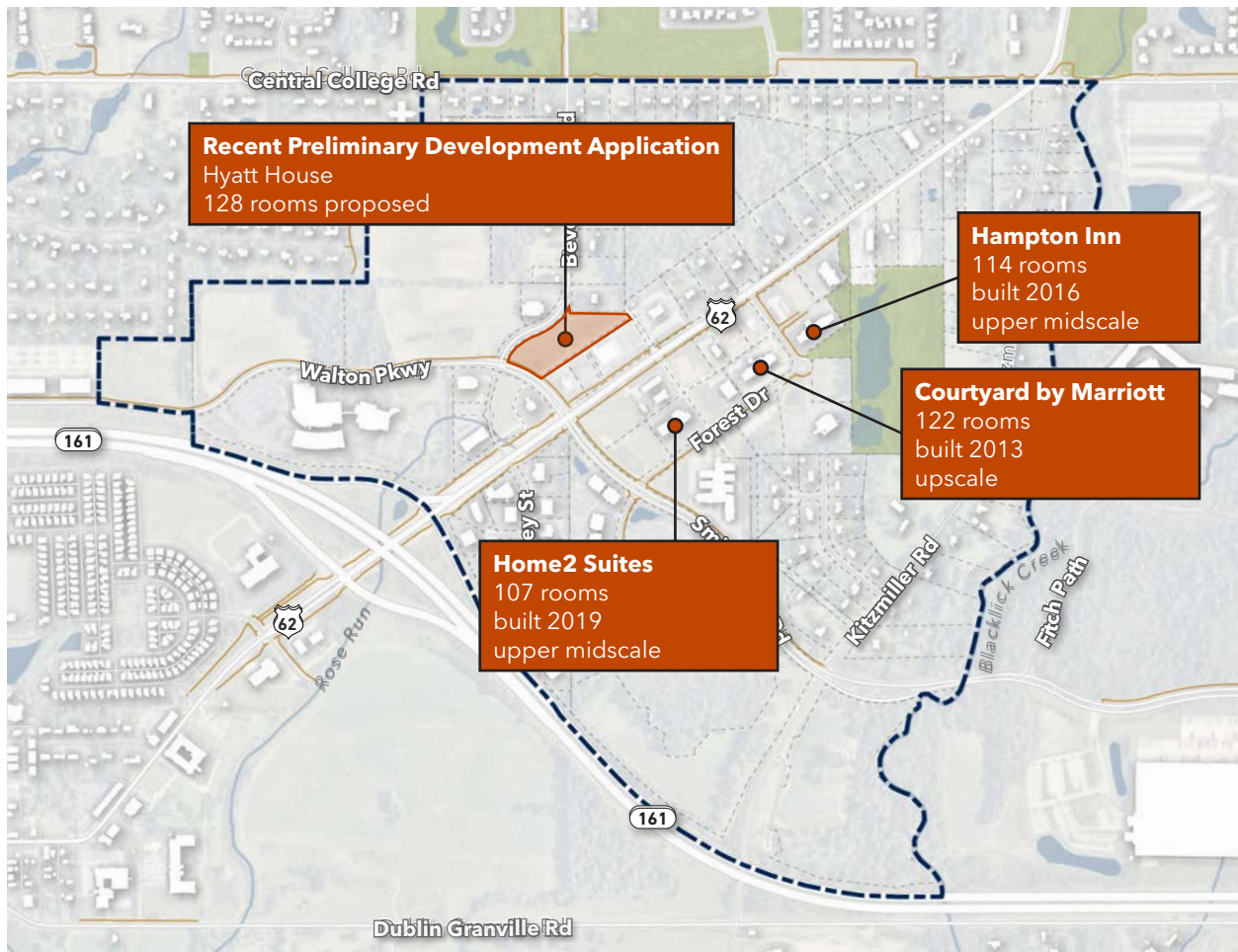
Vacancy Rate, 2024	
Columbus MSA	24.8%
PMA	15%
Focus Area	3.1%

Percentage Return to Pre-COVID Workplace Attendance by Home Location, 2023 share of total annual office attendance over 2018



When workers live close, they prefer to work on-site.

People like coming to work in an office if it's convenient to do so.



HOSPITALITY

The hospitality market in the Focus Area remains competitive due to its proximity to New Albany's business park. The clustering of hotels impacts the demand for supporting services in the area for visitors to enjoy. Hotel visitation generates daytime and overnight revenues primarily in retail, food and beverage, and entertainment sectors. These three sectors comprised two-thirds of 2023 Columbus overnight and day-trip visitor spending.

Legend

- Plan Boundary 
- Existing Leisure Trails 
- Parks/Open Space 
- Parcel Boundaries 

MARKET STRATEGY

BACKGROUND

The goal of a market analysis is to provide answers to questions about aspects of the community that impact the real estate market and its opportunities for future growth. Market analysis provides the foundation for market strategy—the creation of a formal plan for building up the existing assets, overcoming the challenges, and augmenting the unique characteristics of the community identified by the market analysis to achieve and sustain desired development outcomes. The following strategies are potential options for the city of New Albany to pursue in concert with community preferences and stakeholder feedback.

Resident Growth in the PMA

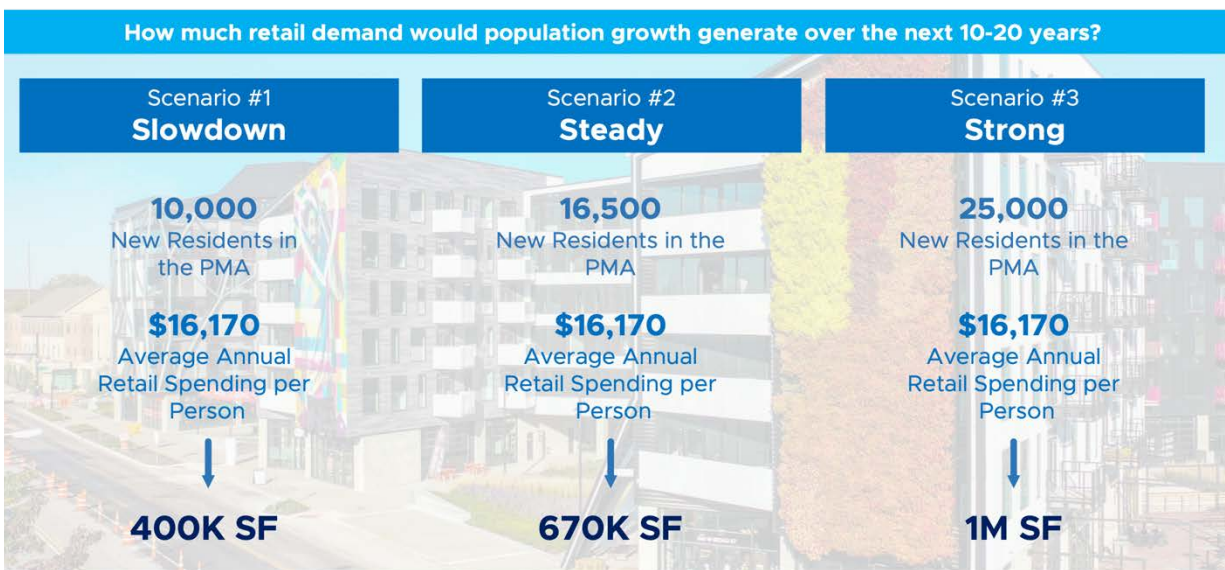
New Albany is an affluent community and an emerging employment hub in the growing Columbus MSA. Given these conditions and an abundance of developable land, the community is an attractive locality for future commercial, industrial, and residential development. The city's potential for future investment can be enhanced by using the opportunity to grow its residential population, which would, in turn, support the development of a vibrant, walkable, and diverse retail district. While New Albany's Village Center provides this type of desired environment, it requires continued subsidy due to the restricted size and spending capacity of the community. New workers in the city will also grow retail support. Still, the resident-to-worker spending ratio is approximately seven to one—that is, it would take seven new workers to provide the retail support of a single new resident.

In the “post-COVID” environment, low office demand is another key reason to focus on resident growth. The local and national office market is rapidly changing. New Albany's office typologies – large-scale, auto-oriented, suburban office parks – are becoming more challenging to market as demand has shifted toward smaller, higher-quality office spaces in dense, walkable environments. This trend poses a vulnerability for New Albany and is illustrated by continued low rates of office utilization—among six properties in and near the Focus Area, only one property (offices at Walton Parkway) has seen attendance restored to 2018 figures, and by far the largest decline in attendance was among workers living more than 10 miles away. Thus, adding new residents to areas around the Focus Area is necessary to support the community's goal of providing continuous organic support for more extensive office development.

Growth Scenarios

The three growth scenarios outlined below highlight the various levels of potential residential growth and its impact on future retail development in the Primary Market Area (PMA). Given the strong population growth expected in the PMA over the next decade, retail demand is anticipated to increase even in the most conservative scenario.

Primary Market Area (PMA) encompasses more than just New Albany. The PMA includes areas generally within a 10-minute commute from the Focus Area to areas with similar socio-economic conditions.



The more residents captured within the PMA, the more retail space it can support; hence, New Albany's (and the Focus Area's) capture of growing demand under any of these scenarios depends on an increase of residential units over the forthcoming decade.

How does residential growth in the PMA affect retail demand in the Focus Area?

More residential creates more community-centered retail. The types of retail demand captured by the Focus Area greatly depends on how much of the projected residential growth within the PMA can occur within or nearby the Focus Area. As outlined in the Market Study, supporting a neighborhood retail center generally requires a population

support of 3,000 to 40,000 residents within a 1.5-mile trade radius. The closer residents can locate in walking or bikeable proximity to retail areas within the Focus Area, the better chance that retail demand will begin to shift from auto-centric uses to neighborhood-oriented establishments that align with the community's aspirations for the area.

MINI NODES

The Focus Area is well-positioned for retail development that supports daily needs, as the local market (two-mile radius) is underserved while many retail categories in the PMA are oversupplied. A viable strategy for retail development is to focus on this local market via the creation of a “mini node”—a hyper-local destination for services and retail curated for the specific desires and needs of the community. Mini nodes are typically multi-tenant strip centers that contain small, leasable spaces (e.g., less than 1,000 sqft.) tenanted by services that cater toward the immediate resident base, such as salons, coffee shops, counter-service restaurants, fitness studios, and urgent cares. Examples of three “mini nodes” in the Columbus MSA include Plaza at Hamilton Quarter, The Shoppes at Olentangy Crossing, and Linworth Crossing Retail – all strip centers developed within the last decade that vary from 15,000 to 43,000 sqft. and source nearly all customers from within a 10-mile radius, but rely typically on much smaller trade areas, such as within a one-mile radius, for repeat traffic. Implementing a “mini node” at the Study Area allows adding desired community services that are otherwise absent in New Albany in a central, highly accessible destination.

Mini Nodes: Case Studies



Plaza at Hamilton Quarter

- Strip Center
- 15,000 SF
- 78% of visits within less than 10 miles
- Built 2021



The Shoppes at Olentangy Crossing

- Strip Center
- 15,400 SF
- 72% of visits within less than 10 miles
- Built 2022



Linworth Crossing Retail

- Strip Center
- 43,000 SF
- 82% of visits within less than 10 miles
- Built 2016





TENANT CURATION

The Focus Area is projected to be able to accommodate an additional 30,000 to 35,000 sqft. of retail space over the next decade. For this space to succeed in both the short and long term, creating a tenancing strategy will ensure that the specific services and goods provided will meet the wants and needs of existing and prospective community members.

One method of identifying retailers for future tenancing is by evaluating the psychographic profile of New Albany's current residents. In this context, psychographics refers to sets of demographic characteristics and consumer preferences shared by groups of residents within a given geography. Commonly used in marketing, psychographic profiles indicate common interests, values, and behaviors within a community that, when combined with demand gap analysis, can direct to potential desirable tenants within the retail environment.

Esri Tapestry Segmentation data provides profiles of U.S. communities divided up into 67 distinct segments, the following of which are most common among households in the city of New Albany:

- **Professional Pride:** Upscale suburban families that tend to prefer new homes in growing outer-ring suburbs and follow the latest lifestyle, health and fitness, tech, and parenting trends. They comprise 58 percent of the city households.
- **Top Tier:** Established families and retirees who represent the highest earners in the country and have a taste for luxury goods, international travel, and fine dining. They comprise 17 percent of the city households.
- **Workday Drive:** Stable upper-middle class households that tend to value convenience, familiarity, and safe, family-oriented communities. They comprise 11 percent of the city households.
- **Boomburbs:** High-earning and well-educated young families that are attracted to outdoor adventures and keep up to date with the newest trends in tech, interior design, and apparel. They comprise six percent of the city households.

Professional Pride	Top Tier	Workday Drive	Boomburbs
			
3.13 HH Size	2.84 HH Size	2.97 HH Size	3.25 HH Size
\$140k Income	\$200k Income	\$100k Income	\$120k Income
92% Homeowners	90% Homeowners	85% Homeowners	84% Homeowners
Whole Foods BMW Dealership CYCLEBAR Farm-to-table cafes Gastropub Sephora Salons and spas Apple Store Ralph Lauren	Boutique grocer Ferrari Dealership Equinox Wine bars Fine dining Performing Arts Center Dry-cleaning COACH Private financial management	Costco Chrysler Dealership 24 Hour Fitness Chipotle Family restaurants Drive-thru car wash Children's Place Sheetz Target	Kroger EVs Apple Store REI Barre Bed Bath & Beyond Starbucks Private preschools Nordstroms

PLAN DIRECTION

INTRODUCTION

The following chapter presents the primary recommendations for the US-62 Interchange Focus Area. These recommendations are the result of a thorough public engagement process, where community members reviewed and iterated regularly on the plan direction and future outcomes. The plan direction is organized into four components:

- **Constraints and Realities** - a high-level review of findings gathered throughout the engagement process and market study, which informs key ideas presented throughout this chapter.
- **Future Land Use Character** - establishing where and what types of development should occur in the future, including specific land uses, building scale, roadway character, parks, and open space considerations.
- **Future Thoroughfare Plan** - identifying the functional and character classifications for all existing and future roadways in the Focus Area, including considerations for streetscape elements and right-of-way dimensions.
- **Public Realm Improvements** - recognizing key public investments that will enhance the character and function of the Focus Area, including intersection improvements, planned parks, and leisure trail connections.



Blue Agave



Bevelhymer Road



Kitzmiller Wetland Park - Existing Condition

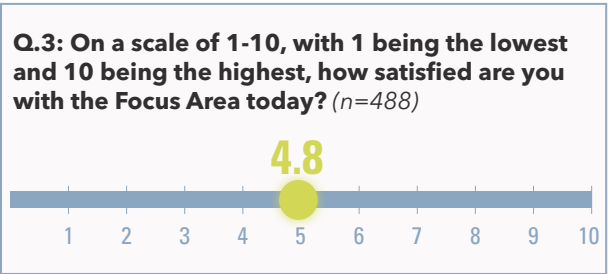
CONSTRAINTS & REALITIES

The following takeaways summarize important concepts and lessons learned during the planning process, including the market study, survey data, and conversations with community members and the Steering Committee. These statements describe current realities that face the Focus Area and build the framework for the plan’s direction and process. They describe the current conditions in the Focus Area and aim to justify the recommendations made throughout this plan, signaling the community’s desired direction and providing guidance for public investments (roads, parks, trails) and new private development (permitted forms and character).



Residents prefer a different development pattern than what is offered in the Focus Area. The community input highlights an overall dissatisfaction of how the area developed over time compared to other areas in New Albany.

Public Survey Results



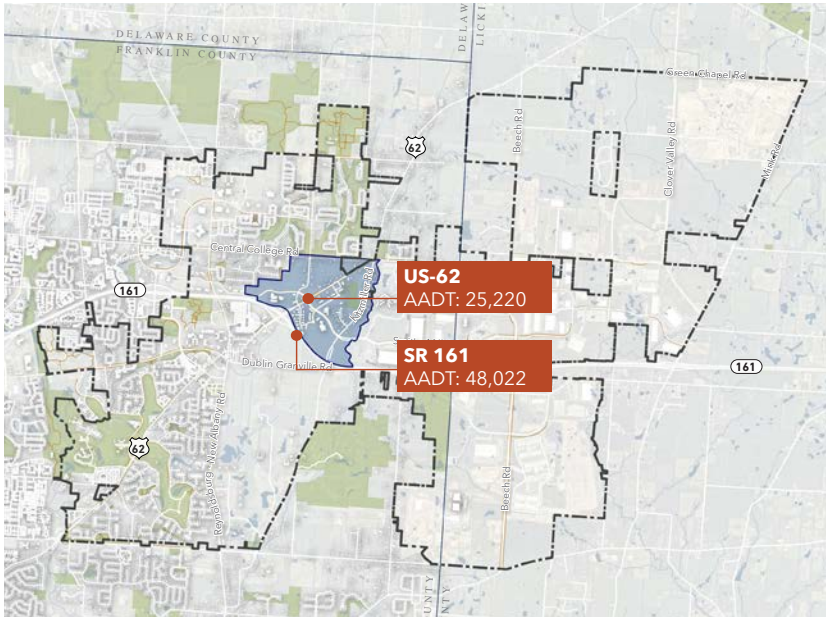
Q.4: Tell us more about your answer to question 3: (n=419)	
Don't want more fast-food restaurants.	33%
Traffic, congestion along roadways and at intersections, access management, and traffic signals are a major concern.	26%
Don't want more auto-oriented uses (Oil Change, Gas Station, Car Wash)	16%

Q.6: What would you change or improve in the Focus Area? (n=411)	
More sit-down, upscale, or casual restaurants.	33%
Continue prioritizing traffic and roadway improvements.	20%
More high-quality, small shops/specialty retail/grocers.	14%

2

The US-62 Interchange will continue to be an important regional vehicular connection. US-62 is an important regional state route and carries significant traffic. This will likely not change.

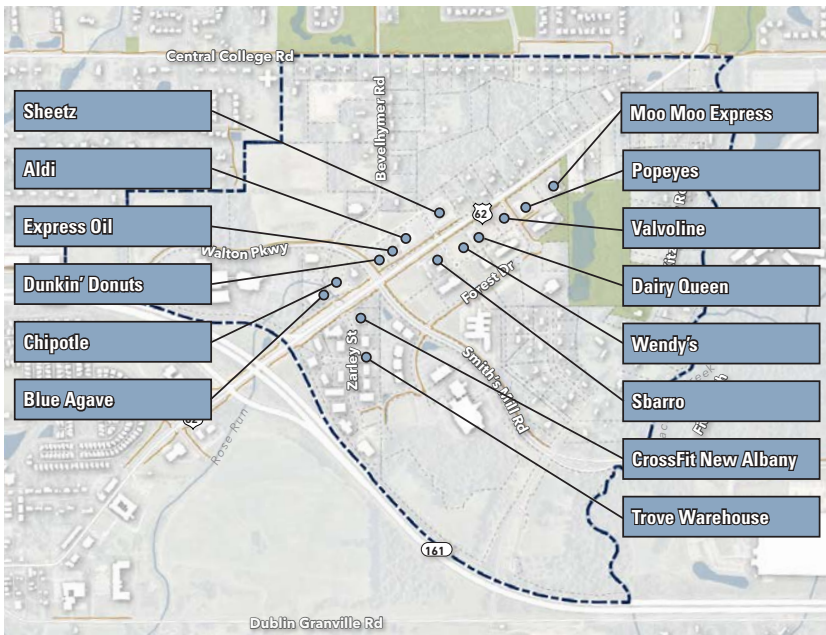
AADT: US-62, SR 161



3

Data points to a supportive market for the current retail offerings in the Focus Area. The current and foreseeable market pressure in the Focus Area favors quick service retail, drive-through uses, and multi-unit residential development. Visitation rates suggest that this demand is primarily driven by commuters and employees.

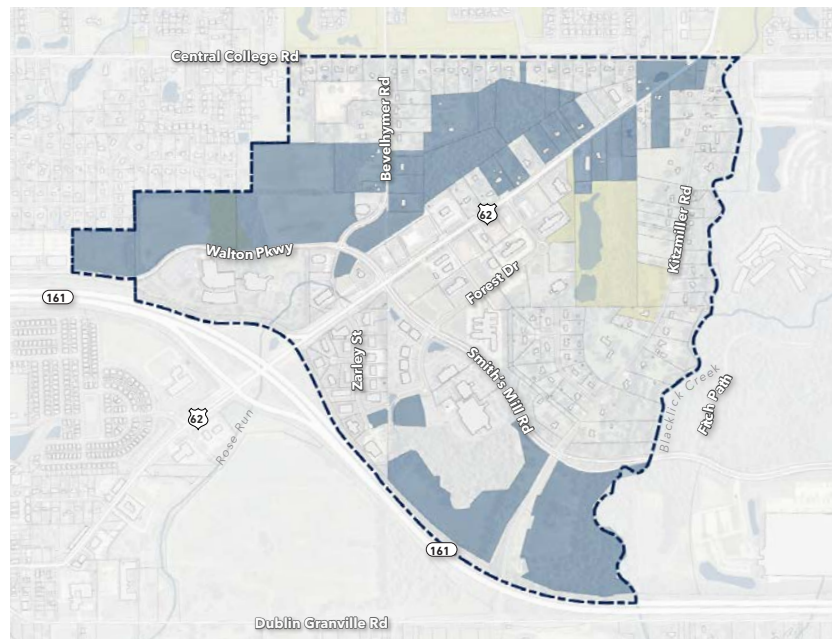
Retail Establishments in the Focus Area







4

Development will likely occur incrementally. Most of the land in the Focus Area is privately owned, and changes will occur when landowners are ready to sell or develop their properties. The areas identified above as most susceptible to change may evolve more quickly than others; however, the scattered ownership in these areas makes it challenging to create a cohesive development pattern.

Areas Susceptible to Change



Legend

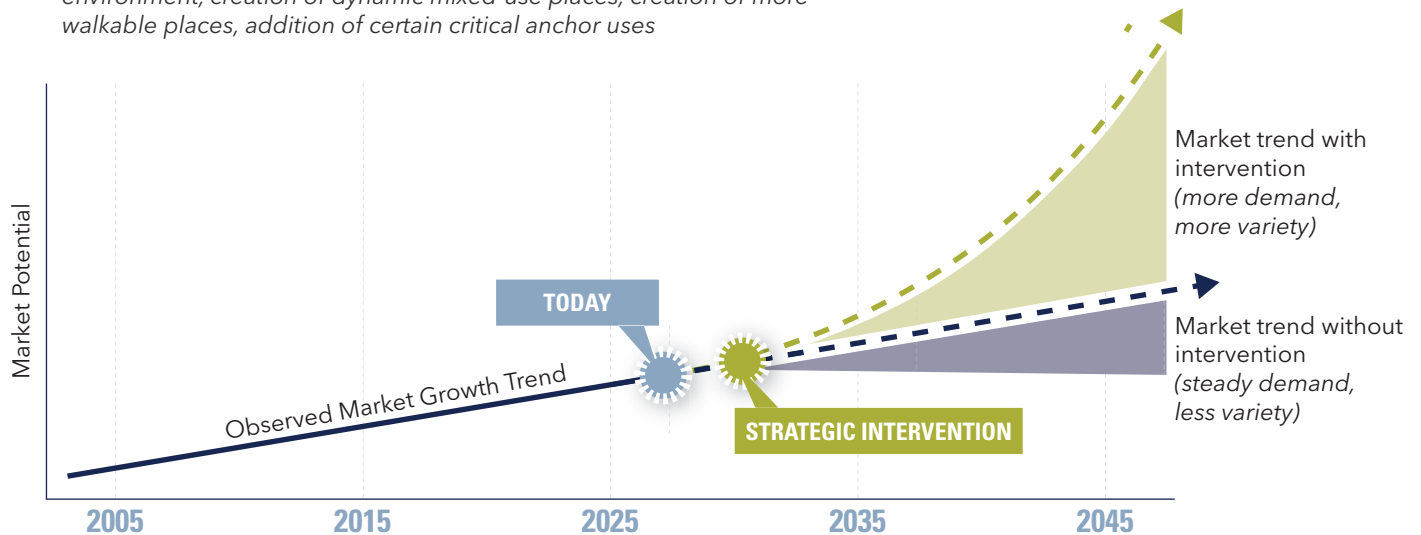
- Plan Boundary 
- Parcels Susceptible to Change 
- Parks/Open Space 
- Parcel Boundaries 

5

Changing the real estate market will require time and innovative ideas. To manage market pressure and foster suitable private development as expressed by the community and the Steering Committee, it will be essential to introduce various strategic interventions to spark a shift in the retail environment.

Impact to Market Demand With Strategic Placemaking Intervention in a Slow Growth Market

Examples of strategic interventions: placemaking, shift in real estate environment, creation of dynamic mixed-use places, creation of more walkable places, addition of certain critical anchor uses



Bridge Park and Grandview Yard exemplify two strategic investments that impacted the surrounding real estate market. Steering Committee members visited both sites to identify qualities that may be applicable to the Focus Area. They expressed overall interest in similar development patterns, while ensuring that they respect New Albany's character, scale, and context. Bridge Park exemplifies a contemporary approach to density that supports a vibrant market for retail, offices, and restaurant real estate. This is accomplished by developing a walkable street network, designing highly-activated building frontages, and creating distinctive public spaces. Most importantly, Bridge Park fosters cultural and civic connections by linking Riverside Crossing Park to historic downtown with a pedestrian bridge.



FUTURE LAND USE CHARACTER

BACKGROUND

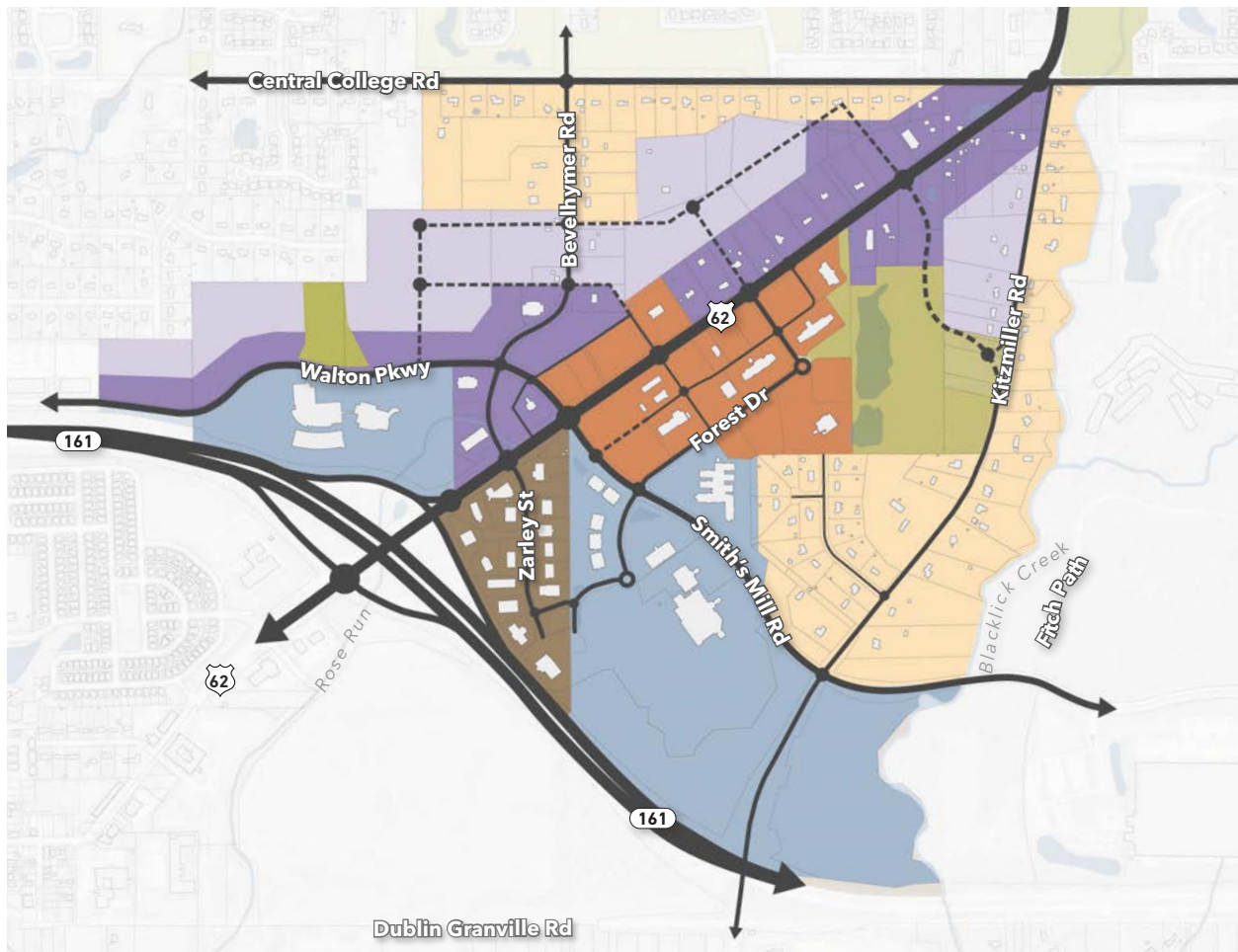
This section serves as a guide for future land use and development within the Focus Area. It is intended to assist city officials in shaping development discussions, pursuing future updates to codified ordinances, and communicating development priorities and preferred character expressed by the community. For private development, this section establishes clear expectations regarding the intent and preferred development pattern for parcels within the Focus Area.

FUTURE LAND USE CHARACTER MAP

The Future Land Use Character Map identifies four distinct land use character typologies and their specific locations within the Focus Area. Each typology is detailed in the following pages, providing descriptions, purpose statements, development considerations, and any relevant supplemental materials. The four typologies are:

- **North Mill Gateway District** - A vibrant, pedestrian oriented district characterized by diverse building types and context-sensitive development patterns that integrate seamlessly with their surroundings. This district is divided into three subdistricts to emphasize the gradient of development that is desired here.
- **Zarley Industrial Park** - a unique, historic pocket of flex office/industry and small business, strategically positioned to support New Albany's evolving employment needs, including continued support for small businesses.
- **US-62 Commercial Core** - a robust corridor of auto-oriented retail, commercial, civic, and accommodation uses, situated along one of the city's primary thoroughfares.
- **Office Campus** - an important hub for medical, healthcare, continuing care, and office headquarters.





Legend

Zarley Industrial Park



US-62 Commercial Core



Office Campus



North Mill Gateway District

Subdistrict A



Subdistrict B



Subdistrict C



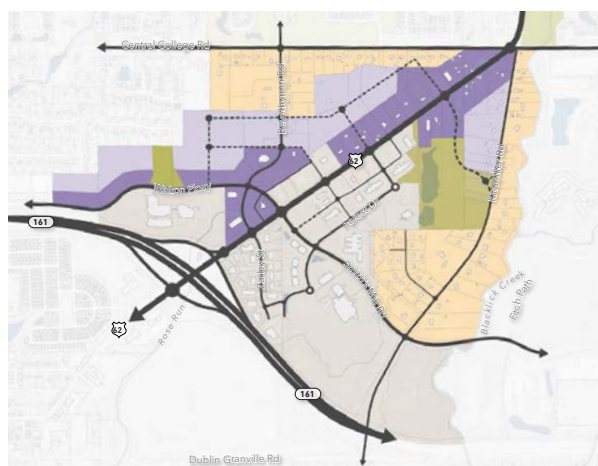
NORTH MILL GATEWAY DISTRICT

The North Mill Gateway District is a land use character typology that promotes specific building forms to encourage cohesive and thoughtful development over a long period of time. Given that certain parcels within this typology may be more susceptible to change quicker than others, future development will likely unfold incrementally over the span of 15-25 years. As the area changes, this new approach ensures that new development remains cohesive and abides by the same set of guidelines.

Purpose

- Encourage a multi-functional, compact neighborhood pattern consistent with the quality and character of New Albany.
- Promote neighborhood-scaled development, including a walkable mix of uses, recreation areas, and civic spaces.
- Introduce a variety of housing types at different scales and strategic locations, while remaining sensitive to the surrounding context.
- Emphasize distinctive, high quality, and four-sided architecture that is aligned with the guidelines established in New Albany's Design Guidelines & Requirements (DGRs). A preference toward vernacular, rectangular form houses, and barn-inspired architecture is appropriate for this district to blend with existing buildings yet distinguish it from the architectural styles in the nearby Village Center.
- Activate more civic and green spaces that provide connectivity, preservation, and recreation.
- Emphasize a cohesive identity or brand for future development that signals/reinforces best-in-class design and upholds community aspirations.
- Implement development typologies and standards based on community feedback

North Mill Gateway District Designated Area



that encourage a shift in the real estate market towards preferred commercial and retail offerings.

- Provide additional multi-modal connectivity throughout the Focus Area.

Subdistricts

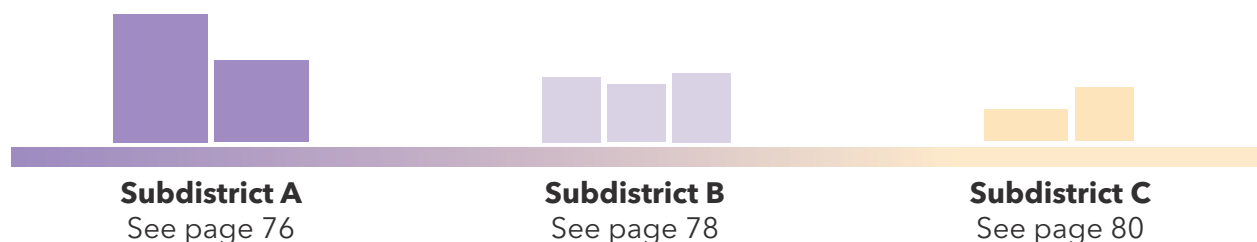
The North Mill Gateway District covers a variety of areas that differentiate in existing land uses and adjacency to either commercial or residential areas. The three identified subdistricts promote a development gradient that introduces various building typologies appropriate for each subdistrict. Certain building typologies identified in this section have varying lot size and building scale expectations depending on which subdistrict they are permitted in.

Subdistrict A includes areas fronting major arterial or collector streets that are appropriate for larger building scales, a mix of commercial and residential uses, increased pedestrian activity, centralized green spaces, and civic amenities.

Subdistrict B is a “transitional zone,” which identifies areas where future developments have to demonstrate a gradual decrease in land use intensity and building scale as they approach existing residential areas within and surrounding the Focus Area. This may involve, but is not limited to, stepping-down building heights, increasing landscape buffers and lot setbacks, and positioning development so that smaller-scaled buildings are situated between the highest intensity of proposed buildings and Subdistrict C and/or existing residential areas.

Subdistrict C includes existing residential lots and areas where more traditional, single-unit or attached residential structures are appropriate.

In the case where land is not currently designated within the North Mill Gateway District, landowners may elect to become part of one or multiple subdistricts, subject to review by city staff based on the location and surrounding context of the subject parcel(s).



NORTH MILL GATEWAY DISTRICT - SUBDISTRICT A

Purpose

This subdistrict is characterized by mixed-use, pedestrian-oriented development with primary access to arterial roads, creating a highly visible and walkable destination. Distinctive gateway elements, like linear parks and civic spaces, more prominent building scale, and uses oriented around community gathering establishes this area as a key focal point and destination in the Focus Area, while anchoring supporting uses that surround it.

Roadway Character

Typology A Street, see pg. 110

Building Height

Maximum 4 stories

Open Space

10-20% of gross development acreage, with at least 60% of total open space dedicated as publicly accessible parkland

Parking

Parking areas and/or structures located behind principle buildings

Permitted Building Typologies

Urban Commercial, see pg. 84

Flex Office, see pg. 85

Multi-Unit, see pg. 87

Lined, see pg. 88

Classic Commercial, see pg. 89

Courtyard, see pg. 90

Townhome, see pg. 91

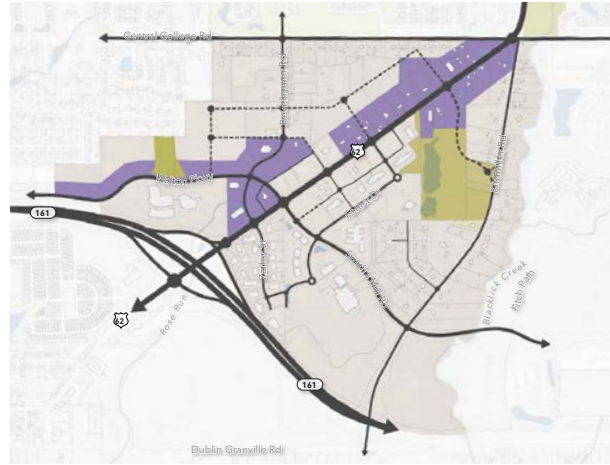
Campus, see pg. 86



Development Considerations

- A** Encourage higher-intensity, active uses near major roadways. This allows for quick vehicular access and visibility, while allowing for more pedestrian-scaled activities to occur further away from main entry points.
- B** Frame buildings and blocks perpendicular to major roadways and around central public greenspaces to allow for safer pedestrian activity throughout the site and less noise generated from major roads.
- C** Integrate a variety of second and third-floor housing types, encouraging walkable access to retail and recreation amenities nearby. Where applicable, ground-floor residential is also encouraged, allowing for variable street frontages and increased pedestrian activity.
- D** Provide on-street, parallel parking facilities fronting commercial and retail uses.

Subdistrict A Designated Area



Conceptual Rendering

NORTH MILL GATEWAY DISTRICT - SUBDISTRICT B

Purpose

This subdistrict includes areas suitable for high-quality, integrated neighborhoods, designed around pedestrian connectivity and proximity to community amenities. These areas include a variety of residential building types, with opportunities for local pockets of **commercial**. Streets in this subarea are quiet, compact, and serve primarily local residents and surrounding bicycle traffic.

Roadway Character

Typology B Street, see pg. 111

Building Height

Maximum 3 stories

Open Space

15-25% of gross development acreage, with at least 60% of total open space dedicated as publicly accessible parkland

Parking

Parking areas located behind principle buildings

Permitted Building Typologies

Flex Office, see pg. 85

Multi-Unit, see pg. 87

Lined, see pg. 88

Classic Commercial, see pg. 89

Campus, see pg. 86

Courtyard, see pg. 90

Townhome, see pg. 91

Two Family House, see pg. 92

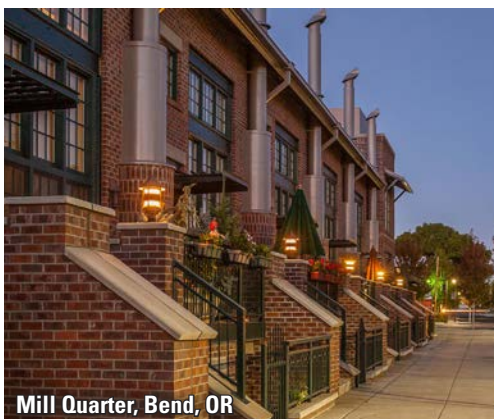
House, see pg. 93



Orleans Landing, Detroit, MI



Franklin, TN

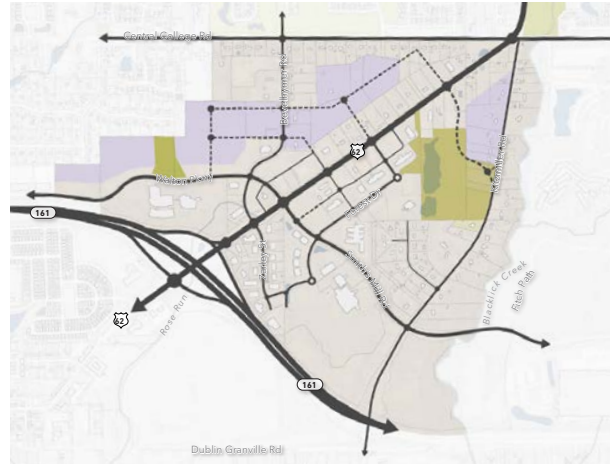


Mill Quarter, Bend, OR



Van Aken District, Shaker Heights, OH

Subdistrict B Designated Area



Development Considerations

- A** Screen uses from major roadways through linear park spaces that provide sidewalk and leisure trail connections, seating areas, and shade.
- B** Promote a variety of housing types and densities framed around an internal network of gridded streets and sidewalks.
- C** Provide a horse fence and other street edge conditions close to major roadways to signal a change in the development pattern as vehicles enter into the Focus Area.
- D** Frame residential buildings around public green spaces, providing accessible and safe recreation opportunities.



NORTH MILL GATEWAY DISTRICT - SUBDISTRICT C

Purpose

This subdistrict includes residential areas situated on lots with larger setbacks from rural or residential roads. These areas are quiet, but connected to surrounding amenities and civic uses through trails and improved streetscape infrastructure. This typology also incorporates natural features, open space, and passive recreation to maintain existing rural character.

Roadway Character

Neighborhood, see pg. 114

Building Height

Maximum 2.5 stories

Open Space

25-40% of gross development acreage -
Neighborhood parks, preserves, greenways, trails, open space

Parking

Detached/attached garages located behind or to the side of principle buildings

Permitted Building Typologies

Two Family House, see pg. 92

House, see pg. 93

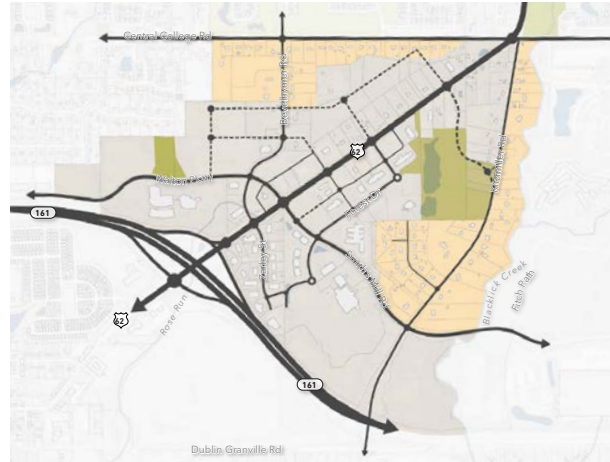
Rural Residential, see pg. 94



Development Considerations

- A** Lot types with larger setbacks/front yards are encouraged in transitional areas to integrate new housing with surrounding rural lots.
- B** Single-unit residential building typologies should blend well with the existing residential character, while still encouraging modern accents.
- C** Gradually step down the density of housing types by introducing “middle-housing,” softening edges between rural residential and mixed-use areas along corridors.
- D** Provide pocket parks nearby residential areas for convenient, walkable access.

Subdistrict C Designated Area



Conceptual Rendering

NORTH MILL GATEWAY DISTRICT - BUILDING TYPOLOGIES

Building typologies outlined below describe the types of structures permitted within each subdistrict. This approach ensures that proposed land uses uphold an expectation of high-quality architectural standards and appropriate scale. It also deters an auto-oriented development pattern that may conflict with the community's aspirations for this area.

Each building typology is accompanied by a set of lot and building expectations that provide general guidance for future development. These include appropriate ranges for lot size, lot width, lot coverage, yard setbacks, building height, and anticipated residential dwelling unit densities, where applicable.

General Building, Lot, and Site Considerations

General

- Gross dwelling unit density in the North Mill Gateway District should not exceed 7 dwelling units per acre. This can be achieved by promoting a balance of residential development, including a mix of higher and lower net density areas across development sites.
- If lot and building expectations found herein cannot be reasonably achieved on a site, the city should consider alternative dimensional requirements, so long as the applicant provides justification that a deviation from the range of expectations are beneficial to achieving the purpose and intent of the North Mill Gateway District.

Open Space

- Integrate civic green space with development to soften the frontage along primary traffic corridors. They can also serve to square off buildings and internal road connections.

Permitted Building Typologies			
Building Typology	Subdistrict		
	A	B	C
Urban Commercial			
Flex Office			
Campus			
Multi-Unit			
Lined			
Classic Commercial			
Courtyard			
Townhome			
Two Family House			
House			
Rural Residential			

- For parcels under five acres that are not part of a larger development plan, open space requirements should be waived or modified so long as all other applicable engineering and stormwater permits/standards are met.

Lot Access

- Build an internal **public** street network to facilitate site access. New development should be accessed through a “gridded” street network, providing multiple connections throughout a site to mitigate congestion at entrances or at key intersections.
- Where an alley or service drive is present, off-street parking and service must be accessed through the alley.
- Where an alley is not present, any off-street parking and services must be accessed by a driveway, with specific width limitations.
- On corner lots without alley access, driveways should be off the street not facing the primary entrance.

Parking

- Off-street parking should be located in the rear yard for all building typologies, except for House and Rural Residential building typologies, where certain structures may have attached garages accessed from the street.
- In addition to general parking recommendations for each building typology, it is encouraged to utilize accepted standards, such as those published by the Urban Land Institute (ULI) and/or the Institute of Transportation Engineers (ITE) to evaluate parking requirements for future mixed-use developments.
- Off-street surface parking should be set back at least five feet from any street right-of-way.

Service & Utility Standards

- Any above-ground mechanical devices must be located in the side or rear yard and fully screened from view.
- Any above-ground utility structures should be in alleys, side yards, or rear yards and must be screened.
- Dumpsters must be stored out of public view and screened from adjacent properties.

Building Frontage

- At least one functioning entrance must be provided from the street or side yard.
- Residential entrances must incorporate a frontage element, such as a stoop, porch, or gallery.
- Buildings on corner lots must have windows and at least one vertical plane break on the second street-facing elevation.

Landscape Standards

- All street and side yards must be landscaped with trees, shrubs, grass, ground covers, or a combination of these elements.

Architecture

- Building typologies may vary in architectural style, as long as they meet the New Albany Design Guidelines and Requirements.

Urban Commercial

A larger scale structure designed to accommodate a mix of uses. Buildings share common walls to create an entire block.

Permitted Subdistricts

Subdistrict A

Subdistrict B

Parking

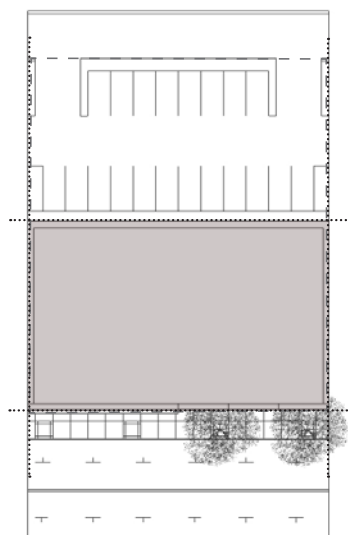
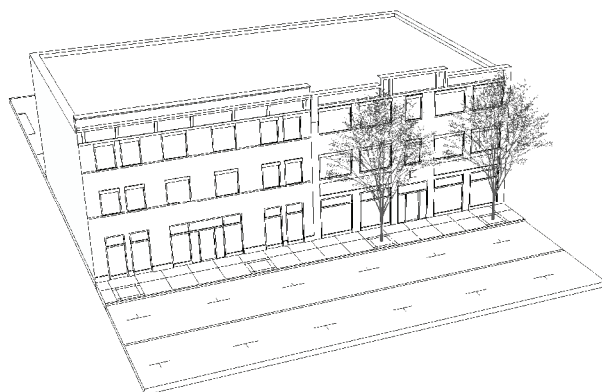
Residential: Min. 1 off-street space per unit.

Max. 1 off-street space per unit plus 1/2

space for each additional bedroom.

Non-Residential: Max. 1 off-street space per 350 sf

Typical Lot Configuration



Lot and Building Expectations

	Subdistrict A		Subdistrict B	
	Min.	Max.	Min.	Max.
Lot Area	-	-	-	-
Lot Width	-	-	-	-
Lot Coverage	-	100%	-	100%
Street Yard	-	20 ft	-	20 ft
Side Yard	-	20 ft	-	20 ft
Rear Yard	15 ft	-	15 ft	-
Stories	-	4	-	3
Height		55 ft	-	45 ft

Flex Office

Typical Lot Configuration

Structures with larger footprints and often designed with open concepts to accommodate a variety of modern office and associated warehousing, research and development, or indoor storage related to the business.

Permitted Subdistricts

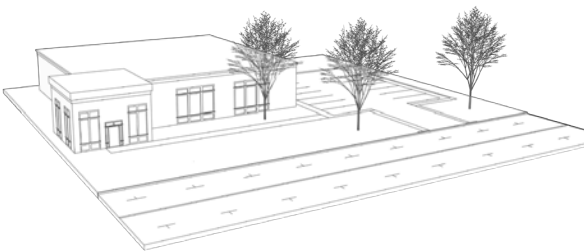
- Subdistrict A
- Subdistrict B

Parking

Parking shall be provided as needed and supported by evidence-based standards.

Other

Flex Office structures are encouraged to front along major collector roads only. Refer to the Roadway Functional Classifications on page 101.



Lot and Building Expectations				
	Subdistrict A		Subdistrict B	
	Min.	Max.	Min.	Max.
Lot Area	1.5 acres	-	1.5 acres	-
Lot Width	-	-	-	-
Lot Coverage	-	40%	-	40%
Street Yard	25 ft	50 ft	25 ft	50 ft
Side Yard	25 ft	-	50 ft	-
Rear Yard	25 ft	-	50 ft	-
Stories	-	4	-	3
Height		55 ft	-	45 ft

Campus

A structure or grouping of structures placed amongst open space in a semi-formal arrangement.

Permitted Subdistricts

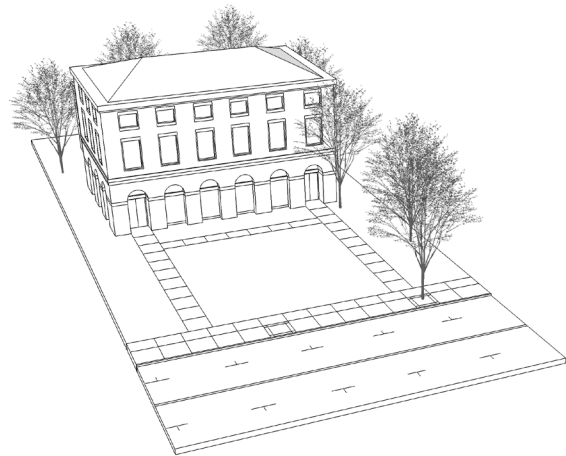
Subdistrict A

Subdistrict B

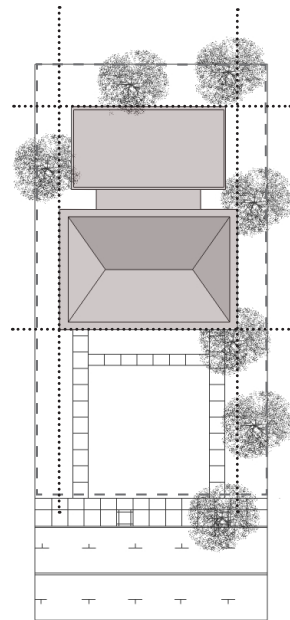
Parking

Parking shall be provided as needed and supported by evidence-based standards.

Typical Lot Configuration



New Albany Middle School



Lot and Building Expectations

	Subdistrict A		Subdistrict B	
	Min.	Max.	Min.	Max.
Lot Area	-	-	-	-
Lot Width	-	-	-	-
Lot Coverage	-	100%	-	100%
Street Yard	30 ft	-	30 ft	20 ft
Side Yard	20 ft	-	20 ft	-
Rear Yard	20 ft	-	30 ft	-
Stories	-	4	-	3
Height		55 ft	-	45 ft

Multi-Unit Building

A structure designed to accommodate multiple units above or beside each other.

Permitted Subdistricts

Subdistrict A

Subdistrict B

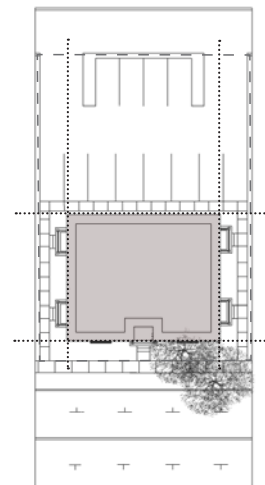
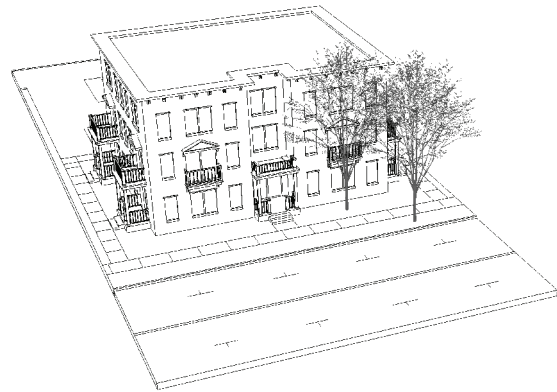
Parking

Min. 1 off street space per unit. Max. 1 off-street space per unit plus 1/2 space for each additional bedroom.

Building Frontage

- Entrances to units may be individual entrances, a foyer entrance, or internal corridor entrance from a street yard.
- Stairways to upper story units must be enclosed and internal to the building footprint

Typical Lot Configuration



Lot and Building Expectations

	Subdistrict A		Subdistrict B	
	Min.	Max.	Min.	Max.
Lot Area	10,000 sf	-	10,000 sf	-
Lot Width	90 ft	200 ft	90 ft	200 ft
Lot Coverage	-	75%	-	75%
Street Yard	5 ft	15 ft	5 ft	15 ft
Side Yard	5 ft	-	5 ft	-
Rear Yard	15 ft	-	15 ft	-
Stories	-	4	-	3
Height		55 ft	-	45 ft

Lined

A mixed-use structure that fronts a parking deck to screen it from view.

Permitted Subdistricts

Subdistrict A

Subdistrict B

Parking

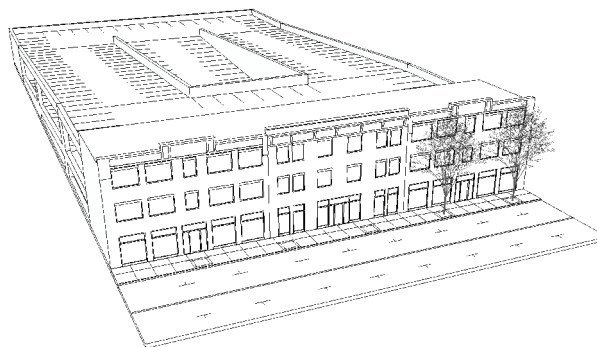
Residential: Min. 1 off-street space per unit.

Max. 1 off-street space per unit plus 1/2 space for each additional bedroom.

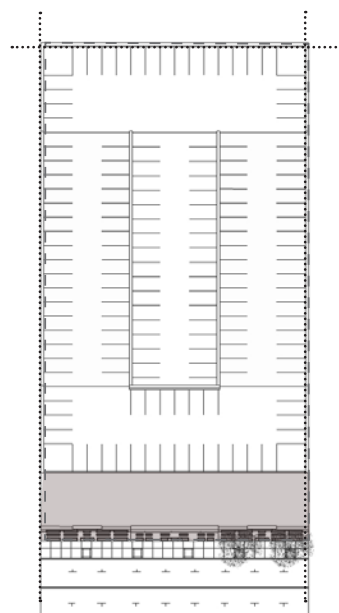
Non-Residential: Min. 2 off-street spaces.

Max. 1 off-street space per 500 sf

Typical Lot Configuration



Columbus, OH



Lot and Building Expectations

	Subdistrict A		Subdistrict B	
	Min.	Max.	Min.	Max.
Lot Area	-	-	-	-
Lot Width	-	-	-	-
Lot Coverage	-	100%	-	100%
Street Yard	-	20 ft	-	20 ft
Side Yard	0 ft	-	0 ft	-
Rear Yard	10 ft	-	15 ft	-
Stories	-	4	-	3
Height		55 ft	-	45 ft

Classic Commercial

A small scale structure designed to accommodate one or two users with small side and street yards.

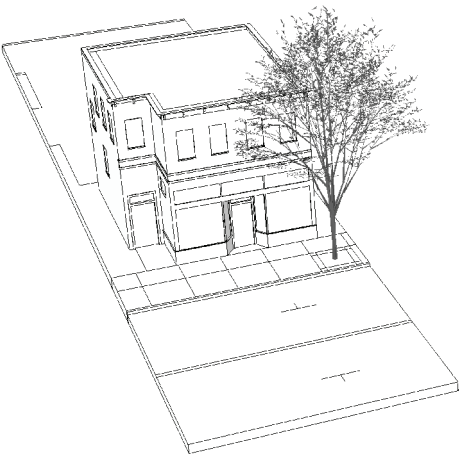
Permitted Subdistricts

- Subdistrict A
- Subdistrict B

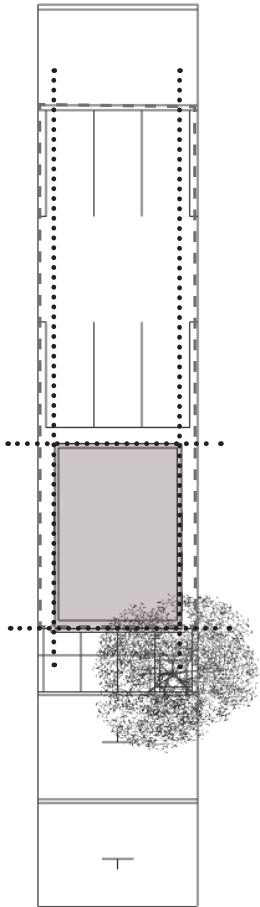
Parking

- Min. 1 off-street space
- Max. 1 off-street space per 500 sf

Typical Lot Configuration



Lot and Building Expectations				
	Subdistrict A		Subdistrict B	
	Min.	Max.	Min.	Max.
Lot Area	4,000 sf	-	4,000 sf	-
Lot Width	50 ft	150 ft	50 ft	150 ft
Lot Coverage	-	95%	-	90%
Street Yard	-	15 ft	-	15 ft
Side Yard	3 ft	16 ft	3 ft	16 ft
Rear Yard	10 ft	-	15 ft	-
Stories	-	4	-	3
Height		55 ft	-	45 ft



Courtyard

A set of structures containing multiple dwelling units (attached or detached) arranged around a private green or plaza space.

Permitted Subdistricts

Subdistrict A

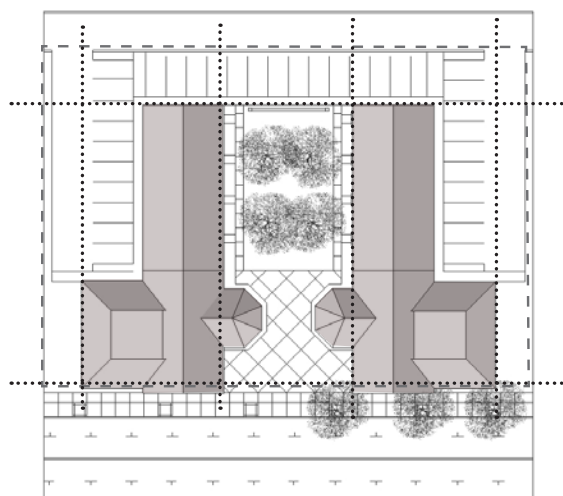
Subdistrict B

Parking

Residential: Max. 1.5 off-street space per unit

Non Residential: Min. 1 off street space, Max. 1 off-street space per 400 sf

Typical Lot Configuration



Lot and Building Expectations

	Subdistrict A		Subdistrict B	
	Min.	Max.	Min.	Max.
Lot Area	-	-	-	-
Lot Width	-	400 ft	-	400 ft
Lot Coverage	-	90%	-	75%
Street Yard	-	20 ft	-	20 ft
Side Yard	-	20 ft	-	20 ft
Rear Yard	15 ft	-	15 ft	-
Stories	-	4	-	3
Height		55 ft	-	45 ft

Townhome

A structure on one lot containing one dwelling unit with common walls on at least one side.

Permitted Subdistricts

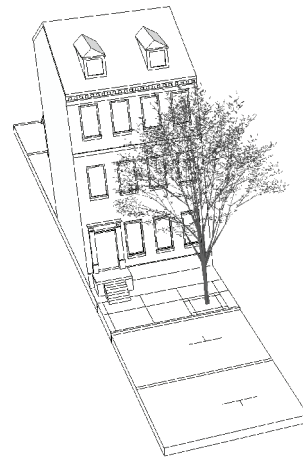
Subdistrict A

Subdistrict B

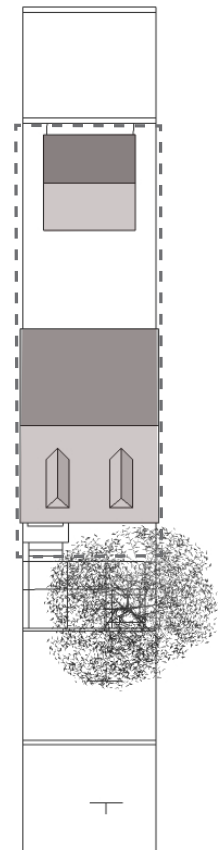
Parking

Min. 1 space per dwelling unit (on/off-street)

Typical Lot Configuration



Lot and Building Expectations				
	Subdistrict A		Subdistrict B	
	Min.	Max.	Min.	Max.
Lot Area	1,000 sf	3,000 sf	1,500 sf	4,500 sf
Lot Width	16 ft	35 ft	20 ft	45 ft
Lot Coverage	-	100%	-	85%
Street Yard	-	10 ft	-	15 ft
Side Yard	-	-	-	-
Rear Yard	-	-	-	-
Stories	-	3	-	2
Height		45 ft	-	35 ft



Two-Family Building

A detached structure with two dwelling units that is massed as a single structure on one lot. Yards can range from small to large and units can be side-by-side or stacked.

Permitted Subdistricts

Subdistrict B

Subdistrict C

Parking

Minimum 2 spaces per Two-Family Building
(on/off-street)

Typical Lot Configuration



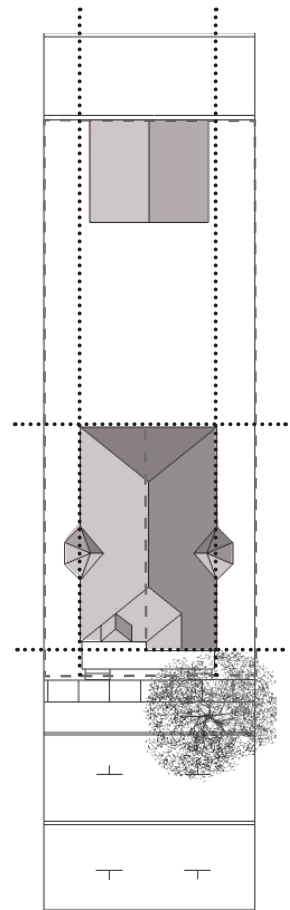
Anacortes, WA



Seattle, WA

Lot and Building Expectations

	Subdistrict B		Subdistrict C	
	Min.	Max.	Min.	Max.
Lot Area	3,000 sf	10,000 sf	7,000 sf	15,000 sf
Lot Width	30 ft	75 ft	70 ft	100 ft
Lot Coverage	-	50%	-	50%
Street Yard	5 ft	15 ft	10 ft	25 ft
Side Yard	10 ft	25 ft	10 ft	25 ft
Rear Yard	-	-	-	-
Stories	-	2.5	-	2.5
Height	-	35 ft	-	35 ft



House

A large detached structure that is typically two stories and contains one dwelling unit per lot.

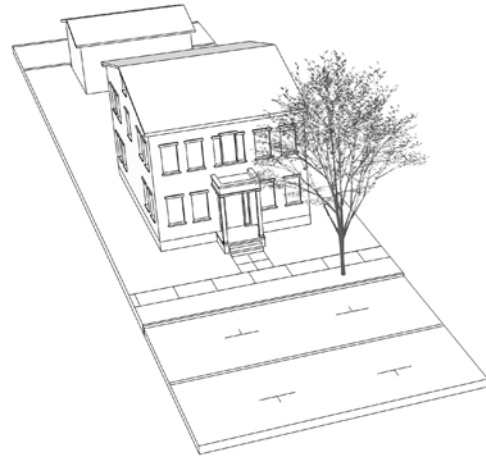
Permitted Subdistricts

Subdistrict C

Parking

Minimum 2 spaces per House (on/off-street)

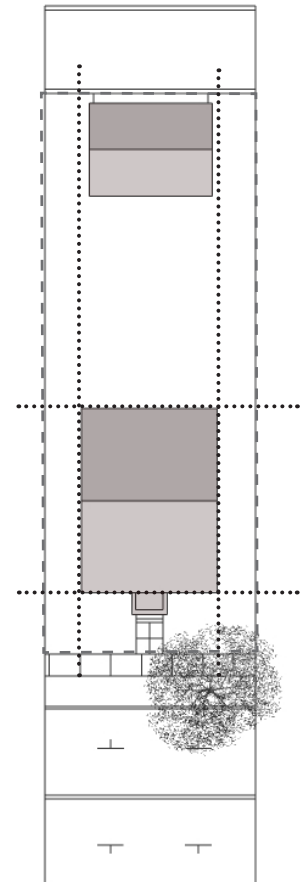
Typical Lot Configuration



Grandview Yard, Grandview, OH



Berry Farms, Franklin, TN



Lot and Building Expectations

	Subdistrict B		Subdistrict C	
	Min.	Max.	Min.	Max.
Lot Area	2,500 sf	4,000 sf	4,000 sf	15,000 sf
Lot Width	25 ft	40 ft	70 ft	125 ft
Lot Coverage	-	75%	-	40%
Street Yard	10 ft	20 ft	15 ft	30 ft
Side Yard	5 ft	10 ft	10 ft	25 ft
Rear Yard	-	-	-	-
Stories	2.5	3	-	2.5
Height	-	40 ft	-	35 ft

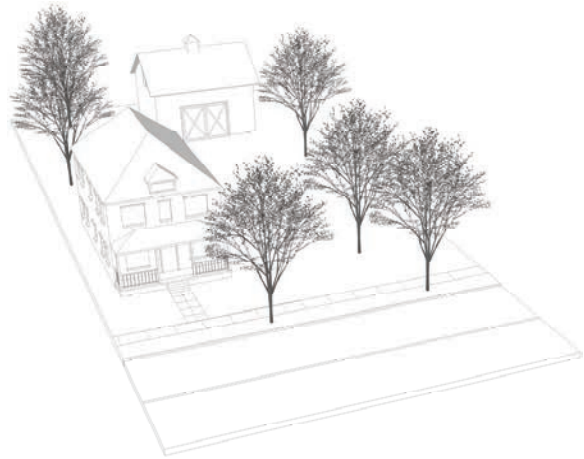
Rural Residential

A large detached structure placed on a generous lot and larger permitted accessory structures.

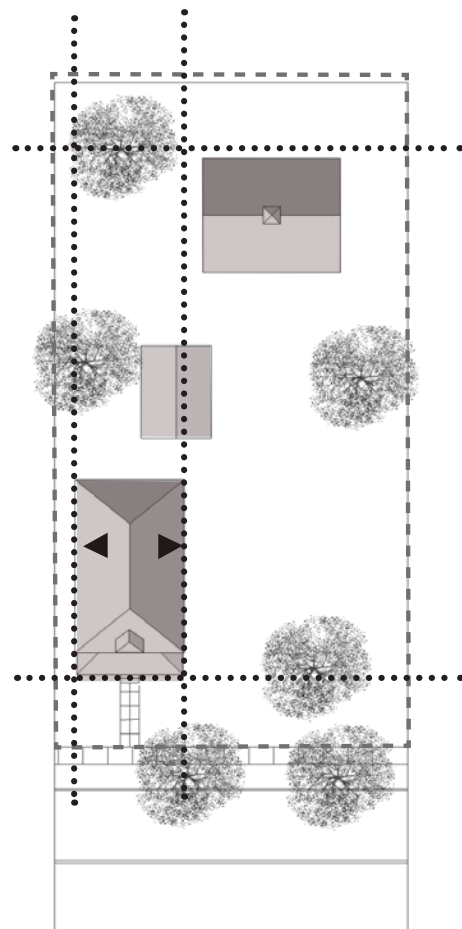
Permitted Subdistricts

Subdistrict C

Typical Lot Configuration



Lot and Building Expectations		
	Subdistrict B	
	Min.	Max.
Lot Area	15,000 sf	2 acres
Lot Width	100 ft	-
Lot Coverage	-	50%
Street Yard	20 ft	-
Side Yard	15 ft	-
Rear Yard	20 ft	-
Stories	1	2.5
Height	-	35 ft



ZARLEY INDUSTRIAL PARK

Zarley Industrial Park, established before the construction of State Route 161, is one of the first employment centers in what was primarily a rural area of Plain Township at the time. Today, it has successfully evolved into a thriving hub for small businesses and startups, which should be leveraged and protected to continue complementing the significant investments in technology, data, and medical sciences in New Albany. As demand for small-scaled, flex office/fabrication spaces continue to rise in Central Ohio, now is the time to leverage New Albany's existing assets to spark the next generation of small business growth in the city.

Purpose

- Encourage and procure a small-business, start-up, and incubator environment in the heart of New Albany.
- Invest in streetscape improvements that encourage pedestrian connectivity and appropriate landscaping and fencing.

Development Considerations

Roadway Character

Commercial Center, see pg. 109

Building Height

Maximum 2 stories

Open Space

Preserves, woodlots

Parking

Parking lots, located along the side or rear of principle buildings

Other

- Retain the area's established materiality and architectural style by incorporating features like metal siding, natural color palettes, and simple geometric forms that reflect the current aesthetic.

Zarley Industrial Park Designated Area



- Align with contemporary flex office/ industrial design trends, including modern updates such as expanded fenestration for natural light, energy-efficient materials, and dynamic facades and roof-lines.
- Ensure that new developments harmonize with the existing character while introducing thoughtful updates that enhance functionality and aesthetic appeal without overwhelming the area's established identity.
- Encourage, where appropriate, shared access and parking facilities for two or more uses.

Strategic Plan Alignment

Several key recommendations and considerations from the strategic plan are reiterated in this Focus Area Plan, including streetscape improvements, a proposed roadway connection, and revised considerations for general land use areas within the Zarley Industrial Park.

Streetscape Improvements

The Engage New Albany Strategic Plan recommends streetscape improvements to align Zarley Street with the character of surrounding New Albany streets. This includes curb and gutter, lighting, planted buffers, sidewalks, and horse fencing.

Zarley Street - Existing Condition



Zarley Street - Proposed Improvements



OFFICE CAMPUS

The Office Campus is anchored by institutions like Mount Carmel Surgical Hospital and includes a wide range of other office, medical, dental, and care-related uses all clustered along Smith's Mill Road and Walton Parkway. Future improvements should be focused on bolstering a "campus-like" built environment, achieved through internal and roadside leisure trail connections and crosswalks at strategic locations.

Purpose

- Encourage potential expansion of complementary medical and care-related uses.
- Promote medical, office, flex office/warehouse, and research & development uses.
- Fill in leisure trail gaps along Smith's Mill Road to increase connectivity.
- Explore other pedestrian mobility improvements along Smith's Mill Road.
- Protect woodlots, wetlands, and consider future passive recreation opportunities.
- Explore additional signage and wayfinding improvements throughout the Office Campus.

Development Considerations

Roadway Character

Business Park, see pg. 107

Building Height

Maximum 6 stories

Open Space

Large civic areas

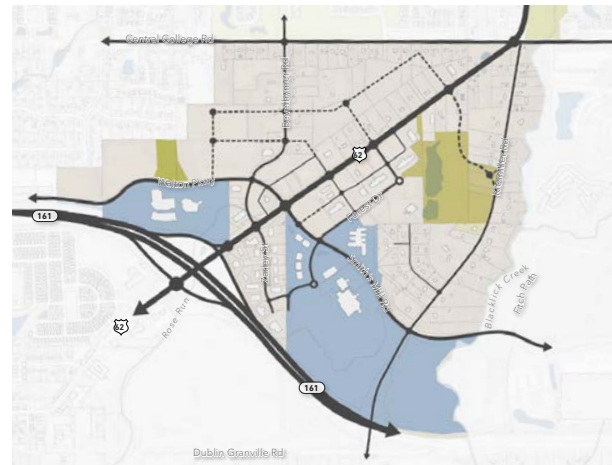
Parking

Parking lots, located along the side or rear of principle buildings

Other

Generally align future development with existing building scale, architectural style, building setbacks, and landscaping.

Office Campus Designated Area



US-62 COMMERCIAL CORE

The US-62 Commercial Core identifies land areas along US-62 and Forest Drive that are primarily commercial and serve a regional market. Significant changes to this area are unlikely in the near term, as much of it has been developed within the past decade. The steady flow of through-traffic on US-62 and the growing employment base in the city's business park continue to drive demand in this corridor. However, if change does occur, landowners should consider how to align future development with the community's preference for less auto-oriented development pattern.

Purpose

- Encourage vibrant, pedestrian-friendly commercial and retail uses that reflect the community's preference for less auto-oriented development along the corridor.
- Identify strategies for tenant curation in collaboration with landowners to attract retail, commercial, and accommodation uses that align with the community's aspirations for this corridor.
- Incorporate community input throughout the development process to ensure future land uses align with local values and aspirations.
- Promote uses that complement and support existing anchor uses. Existing anchor uses include hotels, medical facilities, and civic/event spaces.

US-62 Commercial Core



Development Considerations

Roadway Character

Business Park, see pg. 107

Commercial Center, see pg. 109

Alley / Service Drive, see pg. 115

Building Height

Maximum 4 stories

Open Space

Preserves, woodlots

Parking

Parking lots, located along the side or rear of principle buildings

Other

- Discourage uses that typically require drive-through facilities, in favor of development that encourages pedestrian accessibility. Where these uses are already entitled, promote strong site design and strategic building orientation to mitigate undesired visual and circulation impacts.
- Integrate high-quality pedestrian infrastructure that connects businesses to adjacent uses, neighborhoods, and future leisure trail systems.
- Maintain a grid-like internal street network to enhance connectivity, providing multiple vehicular access points to main arterial/collector streets.
- Align drive access points strategically to minimize congestion and unsafe turning movements, with a preference for shared access drives and cross-parcel connections.

Market Strategy Considerations

- **Establish a tenant curation process and strategy.** Using data-driven analytics, ongoing public input, and survey results gathered during previous planning efforts, the city can assist landowners and developers to procure tenants that meet market demands and align with the needs and aspirations of local residents.
- **Promote mini-nodes as a long-term redevelopment strategy.** While already reaching full build-out, the US-62 Commercial Core may present long-term redevelopment opportunities that focus on “hyper-local” destinations for services and retail establishments curated for the specific desires and needs of residents. Similar mini-nodes in the region include Plaza at Hamilton Quarter, The Shoppes at Olentangy Crossing, and Linworth Crossing. These strip centers vary from 15,000 to 43,000 square feet and source nearly all customers from within a 10-mile radius, but rely typically on much smaller trade areas within a one-mile radius that generate repeat traffic and customers.



Plaza at Hamilton Quarter



The Shoppes at Olentangy Crossing



Linworth Crossing Retail

FUTURE THOROUGHFARE PLAN

The Engage New Albany Strategic Plan includes the city's future thoroughfare plan, establishing the development of a complete roadway network throughout New Albany. This section provides an update to the previously adopted thoroughfare plan for streets within the Focus Area boundary.

The future thoroughfare plan map is made up of two components:

Functional Classification

All existing and future thoroughfares are first categorized by their functional classification, which are based on definitions provided by the Federal Highway Administration (FHWA) and the Ohio Department of Transportation (ODOT). Functional classifications define the role that roadways play in an entire network by assessing levels of service and capacity. Once defined, functional classifications provide a general understanding for the engineering requirements for a road to best serve its role in the transportation network. This would include elements like right-of-way width, utilities, and curbed vs. un-curbed improvements.

Functional Classification Components:

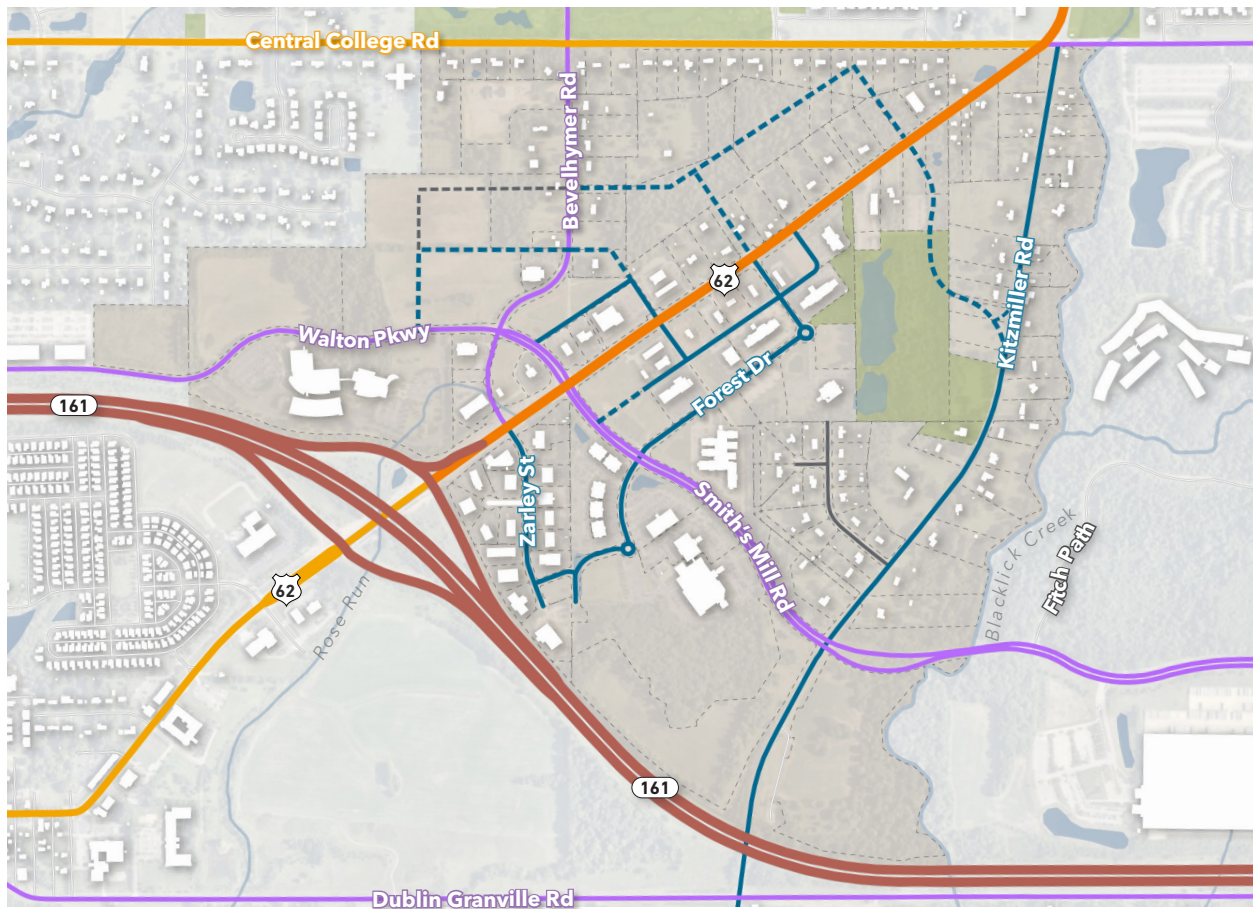
- Categorizes roadways based on their role in the transportation network.
- Evaluates service levels and capacity to define roadway functions.
- Determines engineering standards, such as right-of-way width and curbing design.
- Ensures roadways are engineered to fulfill their intended network function.

Character Classification

Once functional classifications are established, thoroughfares can then be further defined by their character classifications, which describe the design elements within and beyond the rights-of-way that contribute to achieving complete streets, such as on and off-street bike and pedestrian facilities, on-street parking, street edge conditions, landscape medians, and other amenity zones. Character classifications also define the appropriate setbacks, utility areas, and access management standards for future thoroughfares.

Character Classification Components:

- Defines design elements like bike and pedestrian facilities, parking, and landscape medians to achieve complete streets.
- Establishes street edge conditions, amenity zones, and other right-of-way features.
- Specifies appropriate setbacks, utility areas, and access management standards.



Legend		
	Functional Classification	Description
	Interstate	Highest classification of arterials and are designed and constructed with mobility and long-distance travel in mind
	Principal Arterial	These roadways usually serve cities and metropolitan areas, but also can provide a high degree of mobility to and throughout rural areas
	Minor Arterial	Provide service for trips of moderate length and offer connectivity to the higher Principal Arterial system
	Major Collector	Gathering and channeling traffic from local roads to the arterial network, with longer routes, fewer driveways, and higher speed limits than minor collectors
	Minor Collector	Gathering and channeling traffic from local roads to the arterial network
	Local Road	Not intended for use in long distance travel due to their provision of direct access to abutting land. They are often designed to discourage through traffic
	Proposed Minor Collector*	Proposed future alignment of a minor collector
	Proposed Local Road*	Proposed future alignment of a local road

*Proposed road alignments are illustrative and may change upon future engineering and feasibility studies.

CHARACTER CLASSIFICATIONS

The character classifications identified on the following page are intended to replace the classifications from the Engage New Albany Strategic Plan for this Focus Area.

In addition to defining design elements, street edge conditions, amenity zones, and other right-of-way features, character classifications also consider the underlying future land use character typologies that abut roadways. These typologies help inform the design elements and pedestrian infrastructure appropriate for the types of development that exist or are proposed to occur within each land use area.



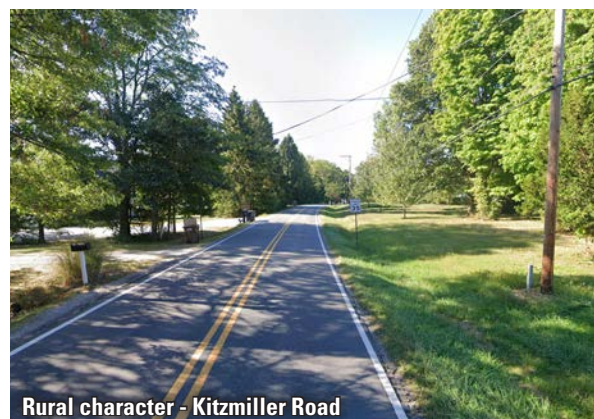
Business Park character - Walton Parkway



Rural Transitional character - Bevelhymer Road

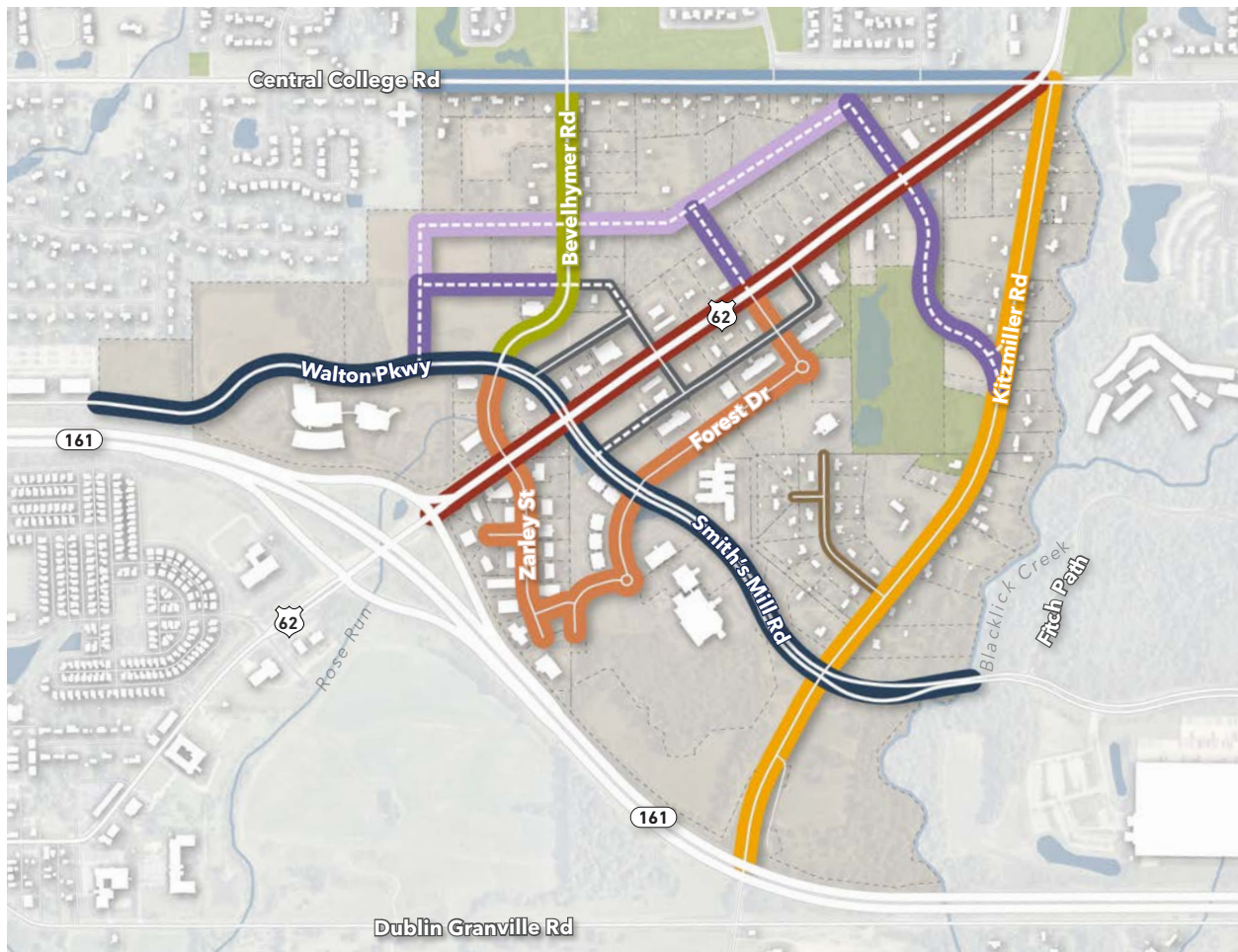


Commercial Street character - Forest Drive



Rural character - Kitzmiller Road

Roadway Character Classifications

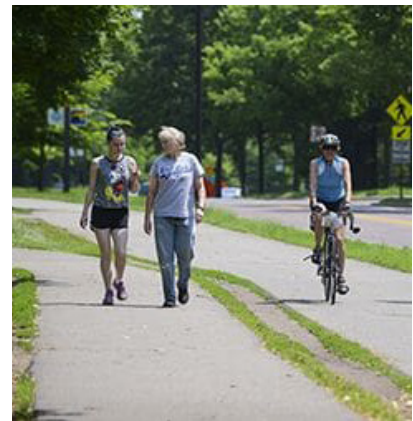
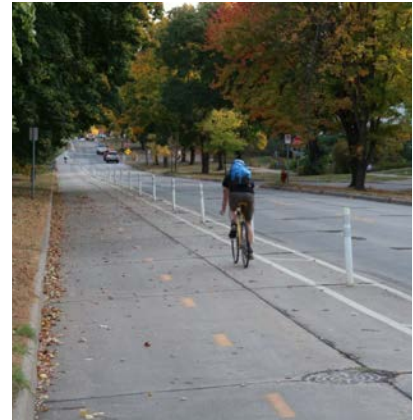
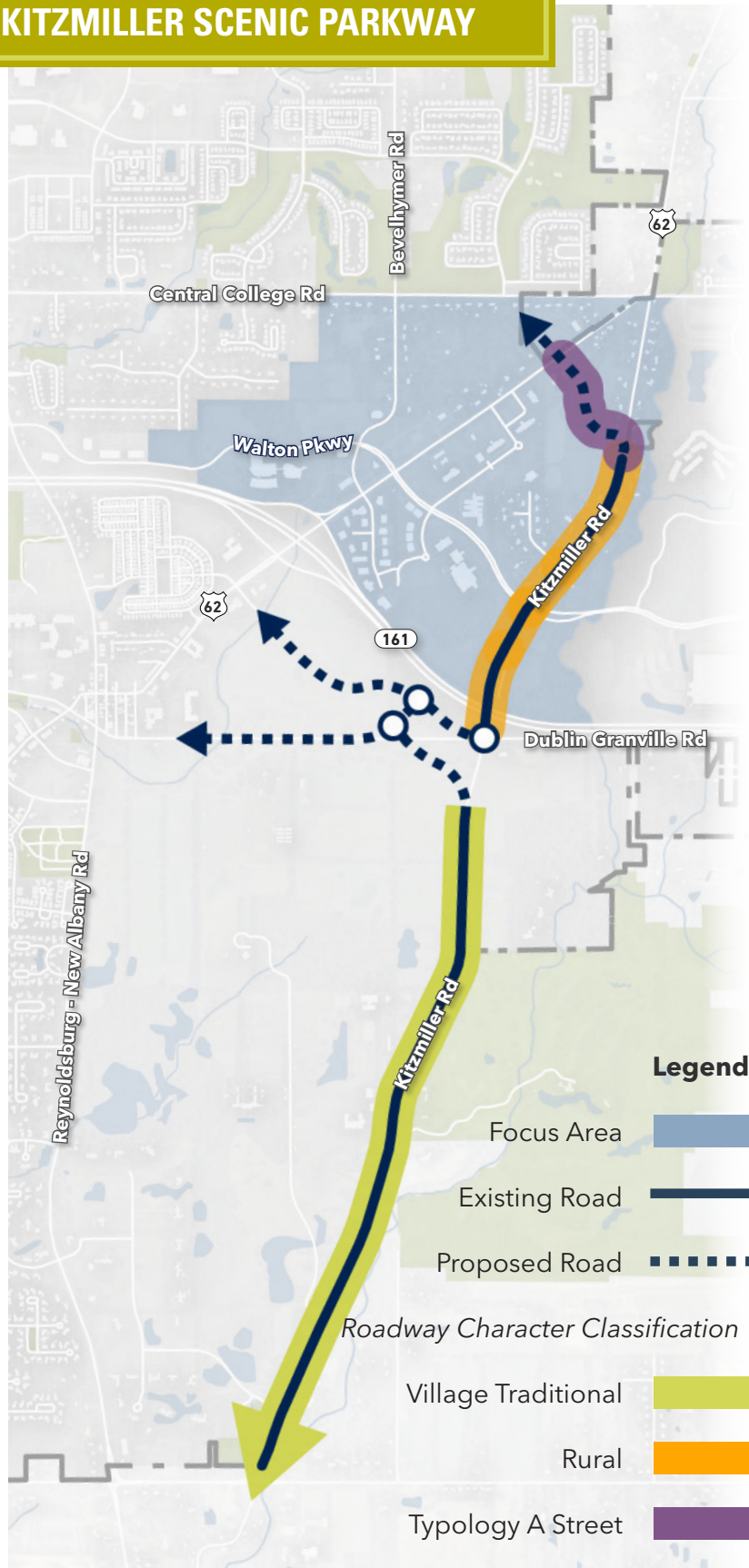


Legend

Character Classifications

Business Park	
Business Park Transitional	
Rural Transitional	
US-62 Corridor	
Typology A Street	
Typology B Street	
Alley / Service Drive	
Commercial Street	
Rural	
Neighborhood	

KITZMILLER SCENIC PARKWAY



KITZMILLER SCENIC PARKWAY

Kitzmillers Scenic Parkway is a quiet, family-friendly street designed to create pedestrian-friendly connections to future parks, trails, and other multi-modal networks. This is a bold idea to enhance Kitzmillers Road while preserving the character and legacy of traditional rural roads in New Albany.

The proposed alignment would disconnect Kitzmillers Road from the five-points intersection, effectively creating a localized cul-de-sac. By rerouting Kitzmillers Road to US-62, the city can address several significant traffic issues. First, this proposed alignment will allow northbound traffic on Kitzmillers Road to access US-62 directly, as left turns at the five-points intersection are currently prohibited. Second, disconnecting Kitzmillers Road from the five-points intersection will facilitate more efficient long-term improvements at that intersection. Additionally, this alignment provides additional frontage along the planned Kitzmillers Wetland Park, unlocking opportunities for future activation, parking areas, and pedestrian connectivity.

Plans to disconnect Kitzmillers Road south of the Focus Area at Ganton Parkway would transform Kitzmillers Road into a more local road than it currently is. This change would create a calm, safe and accessible pedestrian-focused parkway. The proposed infrastructure improvements for this one-mile segment would include on-road or separated bike lanes and leisure trails, pending future engineering and feasibility studies.

This segment can also serve as an “outer-belt” to the proposed Velo Loop, a dedicated bike loop proposed along Walton Parkway, Smith’s Mill Road, Kitzmillers Road south, and Dublin-Granville Road. Future developments north of US-62 would help link existing residential areas to the parkway through additional bike or pedestrian infrastructure.



Kitzmillers Road at Five-Points Intersection



US-62 Corridor

US-62 Corridor is a high-capacity street with up to four drive lanes, variable landscape medians or turn lanes, protected on-street bike lanes, tree lawns on both sides of the street, and an eight-foot wide leisure trail on both sides of the street. Total right-of-way width varies between 77-146 feet to accommodate areas where only two drive lanes exist, but may still be improved in the future to include tree lawns, bike lanes, and/or leisure trails.



US-62 Corridor Characteristics

Street Pavement	
Drive Lane Width	11' (12' outside lanes)
Turn Lane/Median Width	11'-30'
Total Number of Drive Lanes	2-4
On-Street Bike Facility	Bike Lanes (5'-7')
On-Street Bike Tree Lawn Buffer	5'
On-Street Parking	No
Access Management Priority	High
Transit Accommodations	Yes
Pavement Edge	Curb
Total Pavement Width	45' - 100'
Streetscape/Amenities	
Trail/Sidewalk Width	8' min. leisure trail, on both sides of street
Trail/Sidewalk Material	Asphalt
Amenity Zone	Tree lawn
Amenity Zone Width	8' - 15'
Streetlights	Yes
Underground Utility Priority	High
Tree Spacing	30' on center
Right-of-Way Fence	Yes
Setbacks Beyond ROW	50'
Total Right-of-Way	77' - 146'

Business Park

Business Park roadways typically have employment-based uses on both sides of the street and carry employee, truck, and delivery traffic. These two- to four-lane roads have curbs, decorative street lighting, and should have leisure trails on both sides of the road. Most of the landscape is turf grass and trees, with natural tree stands preserved where practical. Parking is not fully screened from view but includes plantings. Setbacks along these roads beyond the right-of-way should be at least 50'.



Business Park Characteristics

Street Pavement

Drive Lane Width	11' (12' outside lanes)
Turn Lane/Median Width	11'-12'
Total Number of Drive Lanes	2-4
On-Street Bike Facility	Bike Lanes (5')
On-Street Parking	No
Access Management Priority	Medium
Transit Accommodations	Yes
Pavement Edge	Curb
Total Pavement Width	35'-69'

Streetscape/Amenities

Trail/Sidewalk Width	8' trail min. on both sides of street
Trail/Sidewalk Material	Asphalt
Amenity Zone	Tree lawn
Amenity Zone Width	8'-15'
Streetlights	Yes
Underground Utility Priority	High
Tree Spacing	30' on center
Right-of-Way Fence	Yes
Setbacks Beyond ROW	50' min.
Total Right-of-Way	81'-115'

Business Park Transitional

Business Park Transitional roadways are those where the long-term condition on at least one side of the road is likely to remain rural or residential. These roads, however, do still carry some through-traffic and traffic for the business park. These roads will have more narrow road shoulders and a ditch street edge condition. They should also have a leisure trail on the business park side of the street, along with white horse fence and significant naturalized plantings, with some mounding if necessary. Any parking areas should be properly screened.



Business Park Transitional Characteristics

Street Pavement

Drive Lane Width	12'
Turn Lane Width	11'
Total Number of Drive Lanes	2
On-Street Bike Facility	Shared road
On-Street Parking	No
Access Management Priority	High
Transit Accommodations	Yes
Pavement Edge	Swale
Total Pavement Width	24'-35'

Streetscape/Amenities

Trail/Sidewalk Width	8' trail min. on both sides of street
Trail/Sidewalk Material	Asphalt
Amenity Zone	Natural buffer
Amenity Zone Width	15'-20' min.
Streetlights	No
Underground Utility Priority	Medium
Tree Spacing	Irregular, natural spacing
Right-of-Way Fence	Yes
Setbacks Beyond ROW	100'
Total Right-of-Way	70'-91'

Commercial Street

Commercial streets are located in neighborhood retail nodes with many auto-centric commercial uses. These roadway corridors will have reduced setbacks, parking lots that are more visible from the street, monument signage, leisure trails rather than sidewalks, no on-street parking, private driveways, and right turn lanes with curb cuts.



Commercial Street Characteristics

Street Pavement

Drive Lane Width	12'
Turn Lane Width	11'
Total Number of Drive Lanes	2
On-Street Bike Facility	Shared road
On-Street Parking	No
Access Management Priority	Medium
Transit Accommodations	Yes
Pavement Edge	Curb
Total Pavement Width	24' - 35'

Streetscape/Amenities

Trail/Sidewalk Width	8' trail min. on both sides of street
Trail/Sidewalk Material	Asphalt
Amenity Zone	Tree lawn
Amenity Zone Width	5' - 8'
Streetlights	Yes
Underground Utility Priority	Medium
Tree Spacing	30' on center
Right-of-Way Fence	Yes
Setbacks Beyond ROW	Varies
Total Right-of-Way	50'-56'

Typology A Street

Typology A Street is a collector street from a major roadway and is designed for pedestrian-oriented districts, with an emphasis on edge conditions that accommodate walkability and activation of ground-floor retail and dining. On-street parking facilities are included along the entire segment of Typology A Streets to support ground-floor uses and provide a natural buffer between pedestrian zones and the road.



Typology A Street Characteristics

Street Pavement	
Drive Lane Width	11'
Turn Lane Width	10'
Total Number of Drive Lanes	2
On-Street Bike Facility	Shared road
On-Street Parking	8'
Access Management Priority	Medium
Transit Accommodations	Yes
Pavement Edge	Curb
Total Pavement Width	38', 48' with center turn lane
Streetscape/Amenities	
Trail/Sidewalk Width	6' sidewalk min. on both sides of street
Trail/Sidewalk Material	Concrete
Amenity Zone	Pedestrian Zone*
Amenity Zone Width	6' - 12'
Streetlights	Yes
Underground Utility Priority	High
Tree Spacing	30' on center
Right-of-Way Fence	No
Setbacks Beyond ROW	0'
Total Right-of-Way	62'-84'

* Pedestrian zones must include tree planters, with additional ROW width set aside for pedestrian activity.

Typology B Street

Typology B Streets are internal, non-collector streets providing connections within planned developments that carry local traffic, typically for residential and small-scale retail uses. This street is intended to function as a neighborhood street, but with varied on-street parking facilities and landscape buffers depending on which uses front the road.



Typology B Street Characteristics

Street Pavement	
Drive Lane Width	11'
Turn Lane Width	10'
Total Number of Drive Lanes	2
On-Street Bike Facility	Shared road
On-Street Parking	8*
Access Management Priority	Low
Transit Accommodations	Yes
Pavement Edge	Curb
Total Pavement Width	38', 48' with center turn lane
Streetscape/Amenities	
Trail/Sidewalk Width	6' min. sidewalk on both sides of the street
Trail/Sidewalk Material	Concrete
Amenity Zone	N/A
Amenity Zone Width	N/A
Streetlights	Yes
Underground Utility Priority	High
Tree Spacing	30' on center
Right-of-Way Fence	No
Setbacks Beyond ROW	Max. 15'
Total Right-of-Way	50' - 60'

*Landscape islands must be placed every 3-5 spaces.

Rural Transitional

Rural Transitional is a traditional rural roadway with varying edge conditions depending on the land use fronting the roadway. In commercial areas, a horse fence, streetlights, and a center turn-lane, where appropriate, are recommended to align with other commercial areas in the Focus Area. As the road transitions into residential areas, horse fences and streetlights are discouraged, and amenity zones are varied to accommodate future feasibility and extent of leisure trail improvements.



Rural Transitional Characteristics

Street Pavement

Drive Lane Width	12'
Turn Lane Width	11'
Total Number of Drive Lanes	2
On-Street Bike Facility	Shared road
On-Street Parking	No
Access Management Priority	Medium
Transit Accommodations	Yes
Pavement Edge	Swale
Total Pavement Width	24' - 35'

Streetscape/Amenities

Trail/Sidewalk Width	8' min. leisure trail, on both sides of street
Trail/Sidewalk Material	Asphalt
Amenity Zone	Tree Lawn
Amenity Zone Width	8' - 20'
Streetlights	Varies*
Underground Utility Priority	Medium
Tree Spacing	Irregular, natural spacing
Right-of-Way Fence	Varies*
Setbacks Beyond ROW	Varies*
Total Right-of-Way	56' - 91'

*These conditions vary based on residential vs. commercial land uses fronting the roadway.

Rural

Rural roadway corridors are charming, original township corridors that contain two-lane roads, do not have a curbed street edge, and carry lower traffic volumes. These roadways will also typically have numerous curb cuts and shallower setbacks. Homes or buildings along rural corridors are often located close to the road, along with large open spaces, fields, pastures, or large rural lots with few structures. Vegetation along the roadway is generally more natural in form and irregular in spacing.



Rural Characteristics	
Street Pavement	
Drive Lane Width	12'
Turn Lane Width	11'
Total Number of Drive Lanes	2
On-Street Bike Facility	Shared road
On-Street Parking	No
Access Management Priority	Medium
Transit Accommodations	No
Pavement Edge	Swale
Total Pavement Width	24', 35' with center turn lane/median
Streetscape/Amenities	
Trail/Sidewalk Width	8' trail min. on both sides of street
Trail/Sidewalk Material	Asphalt
Amenity Zone	Natural buffer
Amenity Zone Width	20' min.
Streetlights	No
Underground Utility Priority	Low
Tree Spacing	Irregular, natural spacing
Right-of-Way Fence	Yes
Setbacks Beyond ROW	Varies
Total Right-of-Way	80'-91'

Neighborhood

Neighborhood roadways are two-lane roads within residential subdivisions with curbs, tree lawns, formal street trees, and curb cuts for driveways. These roads typically have sidewalks on both sides of the street unless a leisure trail is placed on one side.



Neighborhood Characteristics

Street Pavement	
Drive Lane Width	13'
Turn Lane Width	11'
Total Number of Drive Lanes	2
On-Street Bike Facility	Sharrows
On-Street Parking	No
Access Management Priority	Low
Transit Accommodations	No
Pavement Edge	Curb
Total Pavement Width	27', 38' w/center turn lane
Streetscape/Amenities	
Trail/Sidewalk Width	5' sidewalk min. on both sides of street*
Trail/Sidewalk Material	Concrete
Amenity Zone	Tree lawn
Amenity Zone Width	7' min.
Streetlights	Yes, at intersections
Underground Utility Priority	High
Tree Spacing	30' on center
Right-of-Way Fence	No
Setbacks Beyond ROW	Varies
Total Right-of-Way	51'-62'

Alley / Service Drive

Alley / Service roads are narrow streets used for internal connections/lot access to non-residential uses or for any rear-loaded garages as part of new proposed residential streets. This typology includes a minimum requirement for treelawns and sidewalks to ensure that public and private alley ways and service drives still conform to the character of its surrounding street network.













Alley / Service Drive Characteristics









Street Pavement	
Drive Lane Width	11'
Turn Lane Width	N/A
Total Number of Drive Lanes	2
On-Street Bike Facility	Shared road
On-Street Parking	No
Access Management Priority	Low
Transit Accommodations	No
Pavement Edge	Curb
Total Pavement Width	22'
Streetscape/Amenities	
Trail/Sidewalk Width	5' sidewalk min. on both sides of street*
Trail/Sidewalk Material	Concrete
Amenity Zone	Tree lawn
Amenity Zone Width	5' min.
Streetlights	No
Underground Utility Priority	Low
Tree Spacing	30' on center
Right-of-Way Fence	No
Setbacks Beyond ROW	Varies
Total Right-of-Way	42'

FUTURE THOROUGHFARE MAP

Character Classifications

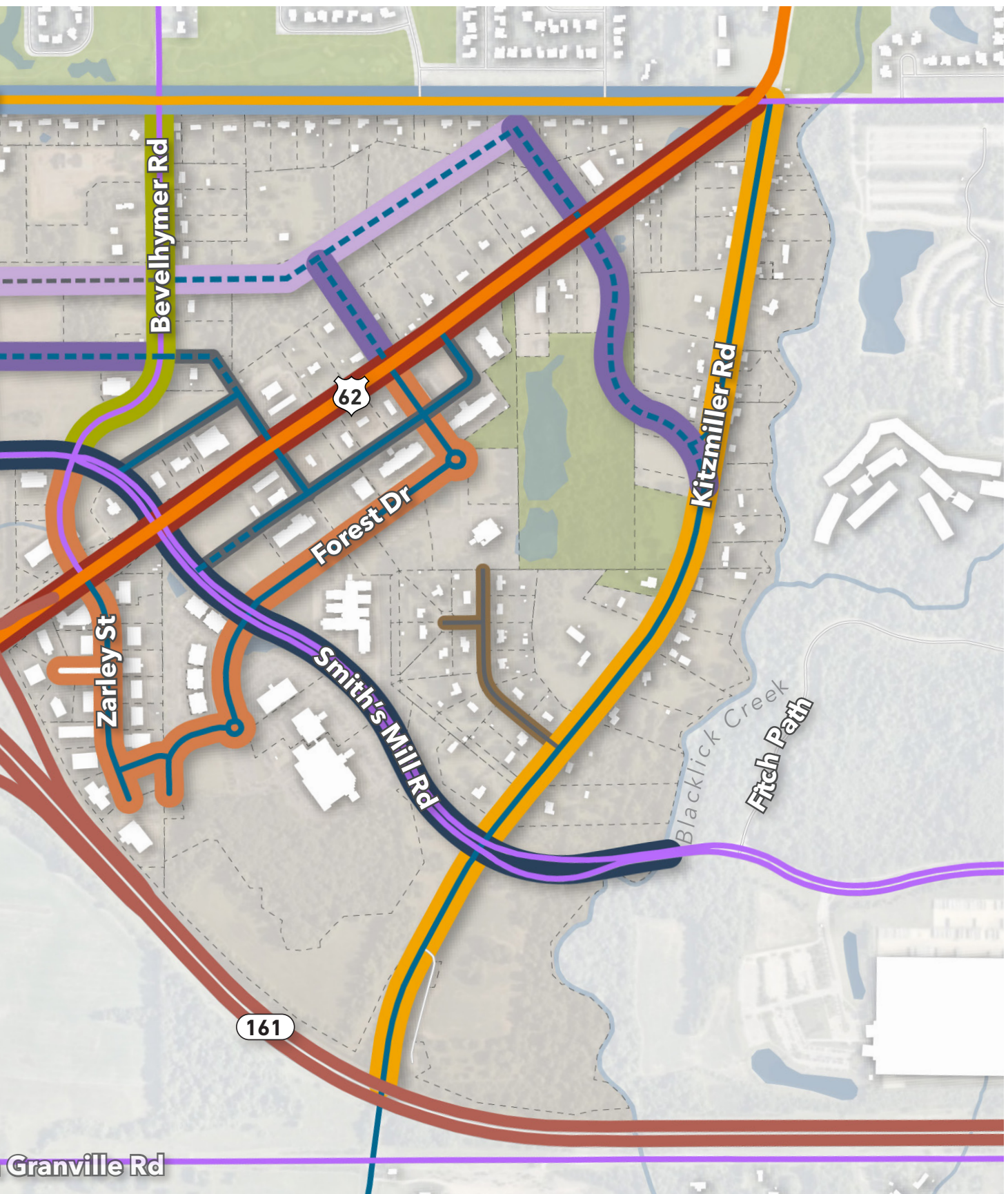
Business Park	
Business Park Transitional	
Rural Transitional	
US-62 Corridor	
Typology A Street	
Typology B Street	
Alley / Service Drive	
Commercial Street	
Rural	
Neighborhood	

Functional Classifications

Interstate	
Principle Arterial	
Minor Arterial	
Major Collector	
Minor Collector	
Local Road	
Proposed Minor Collector*	
Proposed Local Road*	

*Proposed road alignments are illustrative and may change upon future engineering and feasibility studies.





PUBLIC REALM IMPROVEMENTS

Public realm improvements refer to projects that the city can fully, or partially fund or implement to create a more connected mobility and recreation system. These investments can also serve as catalysts for future private investment in the Focus Area. These improvements include intersection, streetscape, parks, and leisure trail investments.

Intersection Improvements

Several high-priority intersections were identified for future study and analysis to improve pedestrian and vehicular connectivity in the Focus Area. The intersections listed below and their recommendations should be coordinated with ODOT to ensure they align with and/or guide ongoing planning efforts related to US-62 and future projects.

Central College / US-62

This intersection, commonly known as the “five-points intersection,” was the most frequently discussed area for future road improvements and remains a top priority for residents. Future engineering studies should focus on identifying suitable interventions for alignment and reconstruction, prioritizing the enhancement of safe pedestrian mobility both within and around the intersection. This also serves as a north-south gateway into New Albany. Improvements should consider elements that signify this intersection as a notable transition into the city.

Central College Road / Bevelhymer Road

Future enhancements are planned for Bevelhymer Park, located north of the Focus Area. As a result, Bevelhymer Road will continue to serve as a primary north/south thoroughfare. Central College Road is also an important east/west route, connecting various areas within New Albany’s business park and providing access to many residential neighborhoods. Future improvements, such as intersection widening,

signalization improvements, or a roundabout, should prioritize safe pedestrian connections, especially for north-south mobility options.

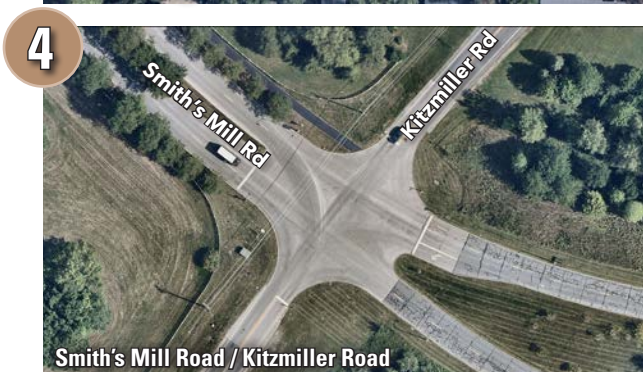
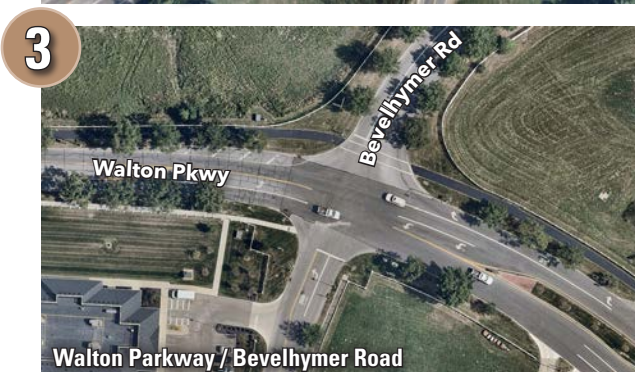
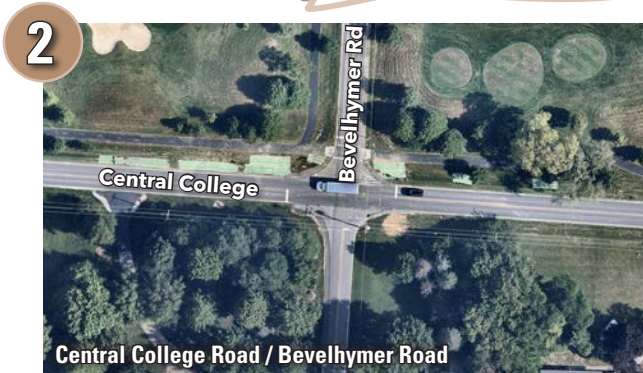
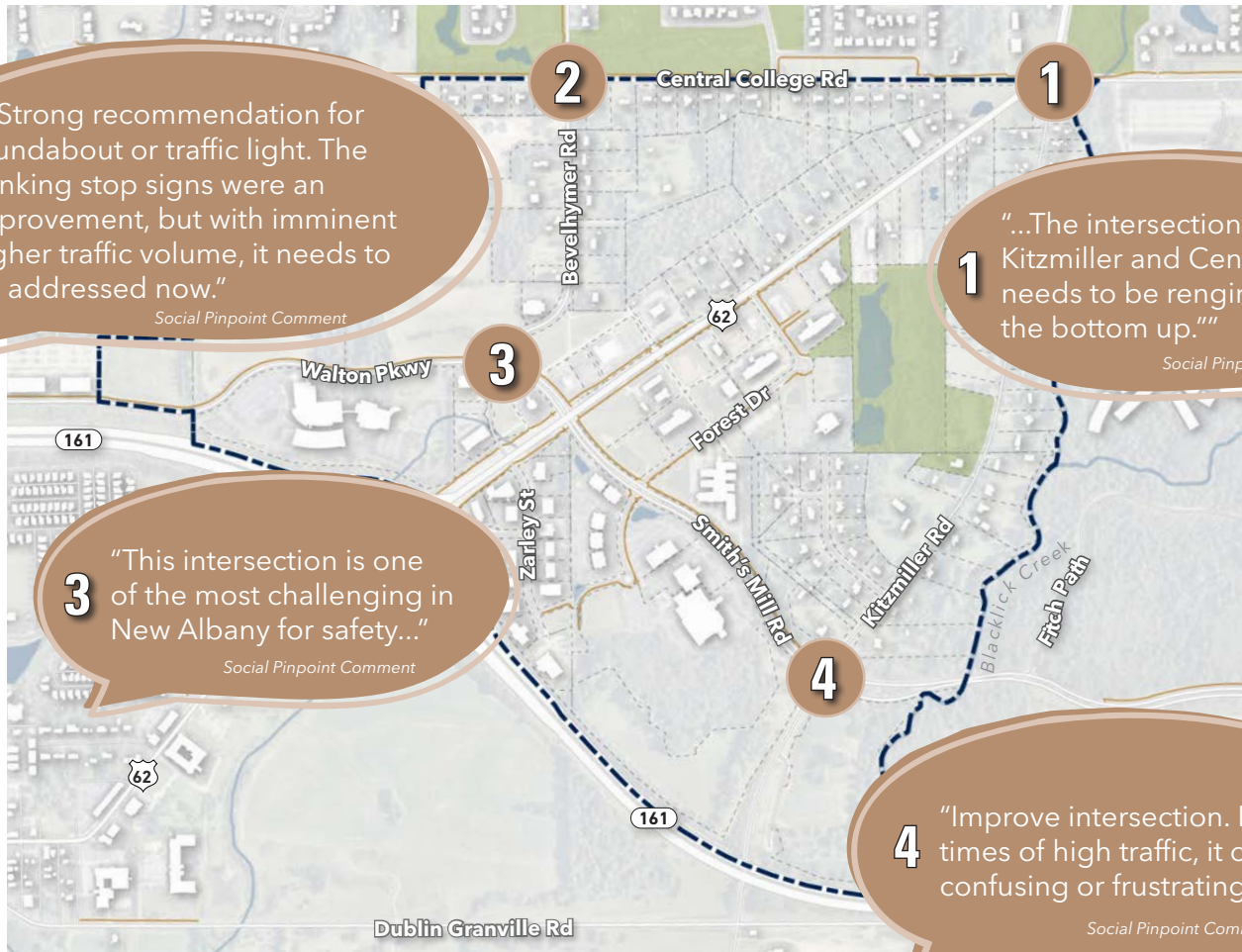
Walton Parkway / Bevelhymer Road

This intersection is currently non-signalized, including north-south stop signs along Bevelhymer Road, giving vehicular traffic along Walton Parkway the priority. This intersection is adjacent to land areas that are likely to be developed in the near future. Future improvements to this intersection need to be studied to assess appropriate interventions that respond to the scale and impacts of future development, while providing safe pedestrian circulation.

Smith’s Mill Road / Kitzmiller Road

This intersection is large and features four-way stop signs, which can be intimidating during periods of heavy traffic. This situation often leads to confusion and miscommunication among drivers. Additionally, pedestrian connectivity in the area is limited, with only one leisure trail connection located on the northwest side along Smith’s Mill Road. There are no sidewalks or pedestrian crosswalks to help access the leisure trail. Future improvements should be studied to identify improvements that minimize complexity, simplify driver decision-making, and improve pedestrian mobility infrastructure.

Intersection Improvements



Planned Parks

Survey results and conversations with residents highlighted the need to proceed with the planned parks in the Focus Area originally outlined in the 2022 Parks Framework Plan. Both Kitzmiller and Tidewater Parks are essential for enhancing park access in New Albany, filling a sizable gap in walking/bike-able access. The current environmental conditions at both parks offer opportunities for passive recreation and serve as key focal points for potential trail improvements along Kitzmiller Road.

Kitzmiller Wetland Park

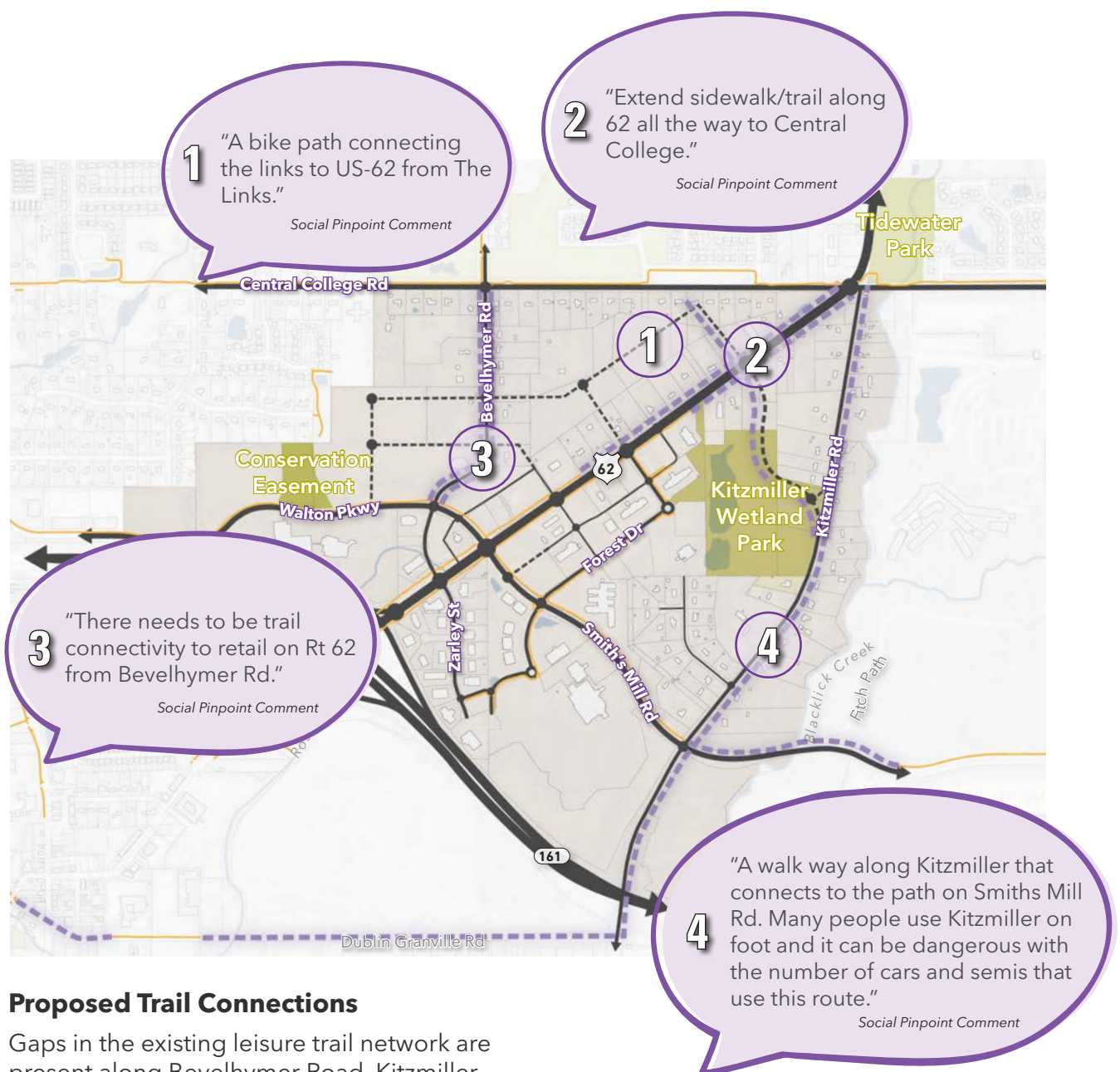
- Preserve the existing wetlands and provide access to the entire site through nature trails, boardwalks, and overlooks.
- Add a large destination playground that deviates in style from the more traditional playgrounds found in the pocket playgrounds and neighborhood parks.
- Build a modern shelter or pavilion with seating and restrooms.
- Build a community skatepark with unique ecological features like bioswales.
- Build an access drive and parking lot to provide vehicular access to the site. Appropriately screen the parking lot with landscaping.



Tidewater Park

- Incorporate signage to identify the site for those wanting to access it, as well as interpretive educational signage about the natural features and environmentally sustainable elements of the site.
- Add a small parking lot set back from the roadway and buffered with landscaping to provide vehicular access to the site.
- Add leisure trails, boardwalks and mown turf paths throughout the park.
- Add supplemental plantings around the cell tower to help screen views from the interior of the park.





Proposed Trail Connections

Gaps in the existing leisure trail network are present along Bevelhymmer Road, Kitzmiller Road, portions of US-62, and Smith's Mill Road. Addressing these gaps will improve safety and accessibility to retail areas and future amenities. While New Albany's zoning code requires developers to construct leisure trails along their property frontages, the city is actively exploring alternative strategies to accelerate leisure trail development beyond private projects. This ongoing effort is essential as the demand for pedestrian mobility connections in certain areas continues to grow faster than development can deliver. It is equally important for residents and affected landowners to remain engaged in these discussions to help shape the future of the leisure trail network.

Legend

Focus Area

Existing Leisure Trails

Proposed Leisure Trails

Parks/Open Space

Parcel Boundaries

IMPLEMENTATION

The land use, thoroughfare, and public realm recommendations found in this plan serve as catalysts for future policy and strategic interventions pursued by the city. The opportunities and challenges that exist in the Focus Area required coordinated public engagement, analysis, and planning to ensure that policy changes align with community aspirations and market realities. Thanks to the efforts of the community, which shaped the expectations for future development and public investment articulated in this plan, city officials and leaders are positioned to take immediate action steps that will increase the functionality, safety, and long-term prosperity of this important corridor in New Albany. Some of the most important initiatives are outlined below, including critical public investments, land acquisition and collaboration with private landowners, updates to zoning and development regulations, and maturing the city's approach to tenant curation and small-business development.

SHORT-TERM IMPLEMENTATION

Roadway Improvements and Leisure Trails

Public investments in roads and trails are essential for enhancing community well-being, safety, and attracting future private investment. The city regularly evaluates these improvements as part of its annual capital expenditure budget. Priority enhancements identified by the community during this planning process will guide the city's plans for future funding and projects. This includes improvements at key intersections such as the "Five Points," Bevelhymer Road/Central College Road, and Walton Parkway/Bevelhymer Road. Additionally, the community highlighted significant gaps in leisure trails. Addressing these gaps would provide safe pedestrian access to retail areas and future recreation destinations. Critical leisure trail connections identified include segments along Bevelhymer Road, US-62, and Kitzmiller Road.

Public Parks and Open Space

The planned Kitzmiller and Tidewater Parks represent key initial investments that address a critical gap in the city's parks network, adding over 30 acres of passive recreation opportunities for nearby residents.

Public engagement highlights strong community support for protecting additional open spaces within the Focus Area. In response, the city will continue to explore additional strategies, including methods to acquire private land, to expand open space dedication and support future civic investments.

Land Acquisition Strategy

As outlined above, the city is actively exploring strategies to acquire additional land for public improvements, including parks, open spaces, and leisure trails. However, addressing these needs in the Focus Area presents unique challenges. Gaps in the leisure trail network and the need for streetscape improvements are generally concentrated along rural residential roads, where short-term redevelopment is unlikely. Typically, the expansion of leisure trails and construction of streetscape improvements rely on cost-sharing with private development, who are required to build leisure trails along their property frontages. In the absence of new private development, the city must collaborate with residents to secure private land for these public improvements.

This may involve easements, voluntary agreements, grant funding, or philanthropic initiatives.

Tenant Curation

As outlined in the Market Study, the city can take proactive measures to influence areas within the Focus Area that are nearing full build-out. Future changes in tenancy or property ownership may create opportunities to better align land uses with the community's preferred retail and commercial offerings, as expressed during public engagement. The city is actively exploring analytical tools, business development partnerships, and procurement strategies to attract preferred tenants that contribute to the city's vibrant business community.

Zoning Updates

The next steps to implement the land use recommendations outlined in this plan may involve future updates to the city's codified ordinances, zoning map, and/or supplemental development standards to ensure alignment with the plan recommendations. These updates are essential to transform the intentions and guiding development principles detailed in this plan into legal requirements for future development in the city. As discussed with the Steering Committee and residents during the engagement process, the city will explore contemporary zoning and land development tools to fully achieve the goals identified in this plan. This may include options such as form-based zoning, overlays, additional architectural standards, or additional development review processes. In the interim, it is recommended that large developments utilize the PUD zoning process

to incorporate principles and standards found throughout this document. In addition, it is encouraged to incorporate the Focus Area into the existing Architecture Review Board (ARB) review process to ensure that the city's Design Guidelines & Requirements (DGRs) are upheld consistently across developments in the Focus Area.

Monitoring Development Impacts

The North Mill Gateway District calls for a development pattern that will introduce a variety of residential building typologies and densities. To ensure that gross density across the entire North Mill Gateway District does not exceed seven units per acre, the city could establish a density bank to monitor entitlements across multiple projects and site ownership over time.

LONG-TERM IMPLEMENTATION CONSIDERATIONS

Form-Based Code

One key challenge to ensuring consistent and quality development throughout the Focus Area is the scattered nature of parcel ownership and developable parcels. A form-based code is a long-term initiative that was discussed with leadership, the Steering Committee, and residents throughout the planning process. This approach ensures consistent expectations for new development across the entire focus area and promotes high-quality architectural styles and subsequent land uses. This is a possible long-term recommendation that would require additional public outreach, writing and adopting new code, and incorporating new review and approval processes into the city's existing regulatory framework.

North Mill Gateway Overlay District

A zoning overlay district could serve as an alternative to implementing a form-based district. An overlay could include design elements and standards proposed in this plan, while being less disruptive for existing landowners, since their base zoning district would remain unchanged. New developments would be required to utilize the overlay district to achieve design outcomes recommended in this plan. The potential overlay district could be extended across the Focus Area to any landowners who seek to redevelop under the guidance of the North Mill Gateway District standards.

Fee-In-Lieu / Formal Open Space Reimbursement Program

This program would involve the city actively acquiring open space in strategic locations to serve as “frontage” for future private development. The intent of such a program is to protect public access and pedestrian connectivity throughout the Focus Area and beyond, while incentivizing the type of development pattern desired from this plan. Developers would pay a fee-in-lieu of not providing any public open space to reimburse the city for its initial investment. This program could also encourage developers to provide additional public open space beyond what is required and what the city has already provided in return for density entitlements, where appropriate.

